

Design Review Panel

365 Highway 8,
Hamilton

**Official Plan & Zoning By-law
Amendment**

City File Nos.
FC-23-071

365 Highway 8
City of Hamilton

Prepared For
2752037 Ontario Inc.

January 11, 2024

 **BOUSFIELDS INC.**
PLANNING | DESIGN | ENGAGEMENT

 **OFFICE ARCHITECTURE**

 **BOUSFIELDS INC.**
Job Number
22376

 **BOUSFIELDS INC.**
PLANNING | DESIGN | ENGAGEMENT

 **OFFICE ARCHITECTURE**

www.bousfields.ca

Toronto Office

Hamilton Office

Urban Planning
Urban Design
Community Engagement

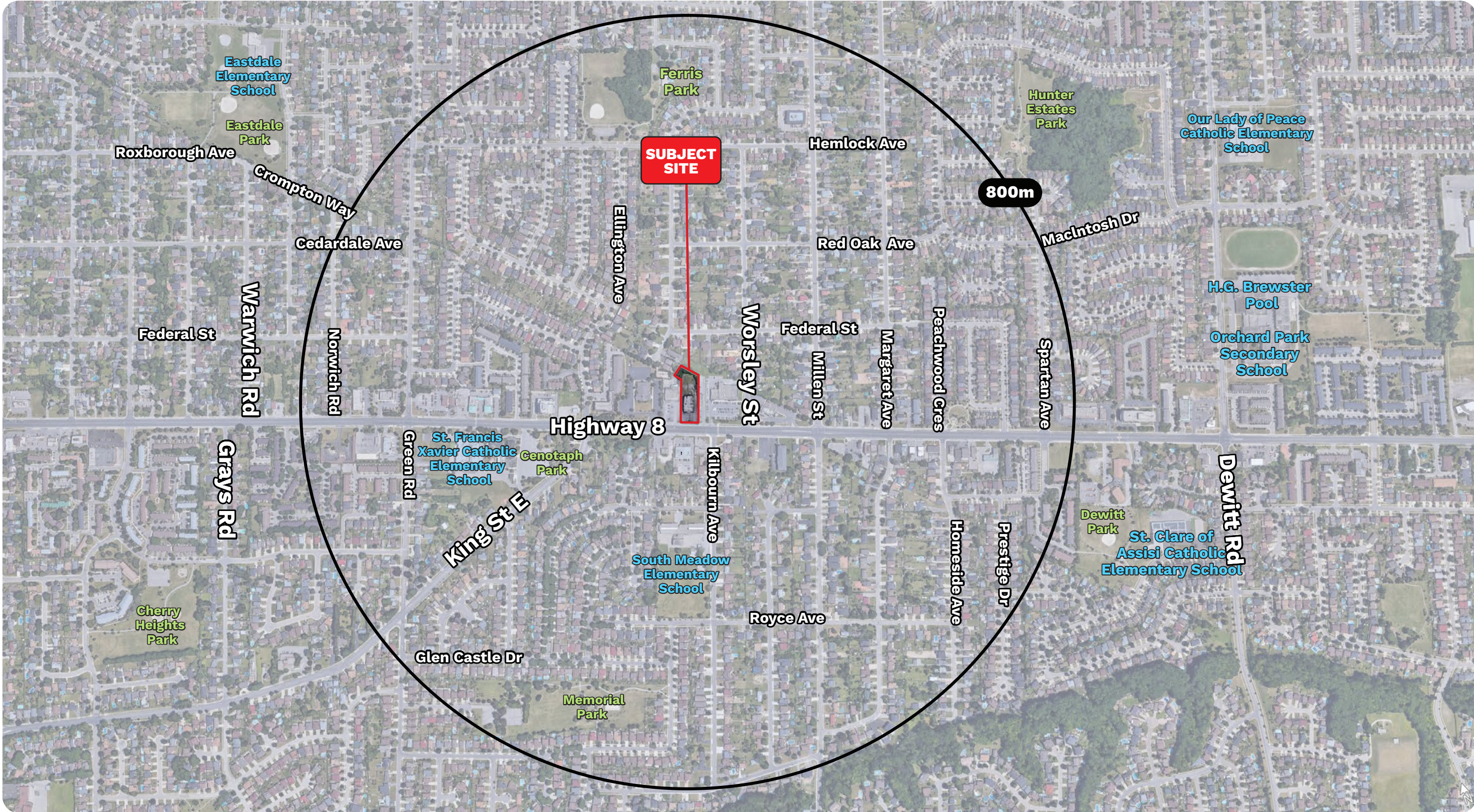
3 Church Street, Suite 200
Toronto, ON
M5E 1M2

1 Main Street East, Suite 200
Hamilton, ON
L8N 1E7

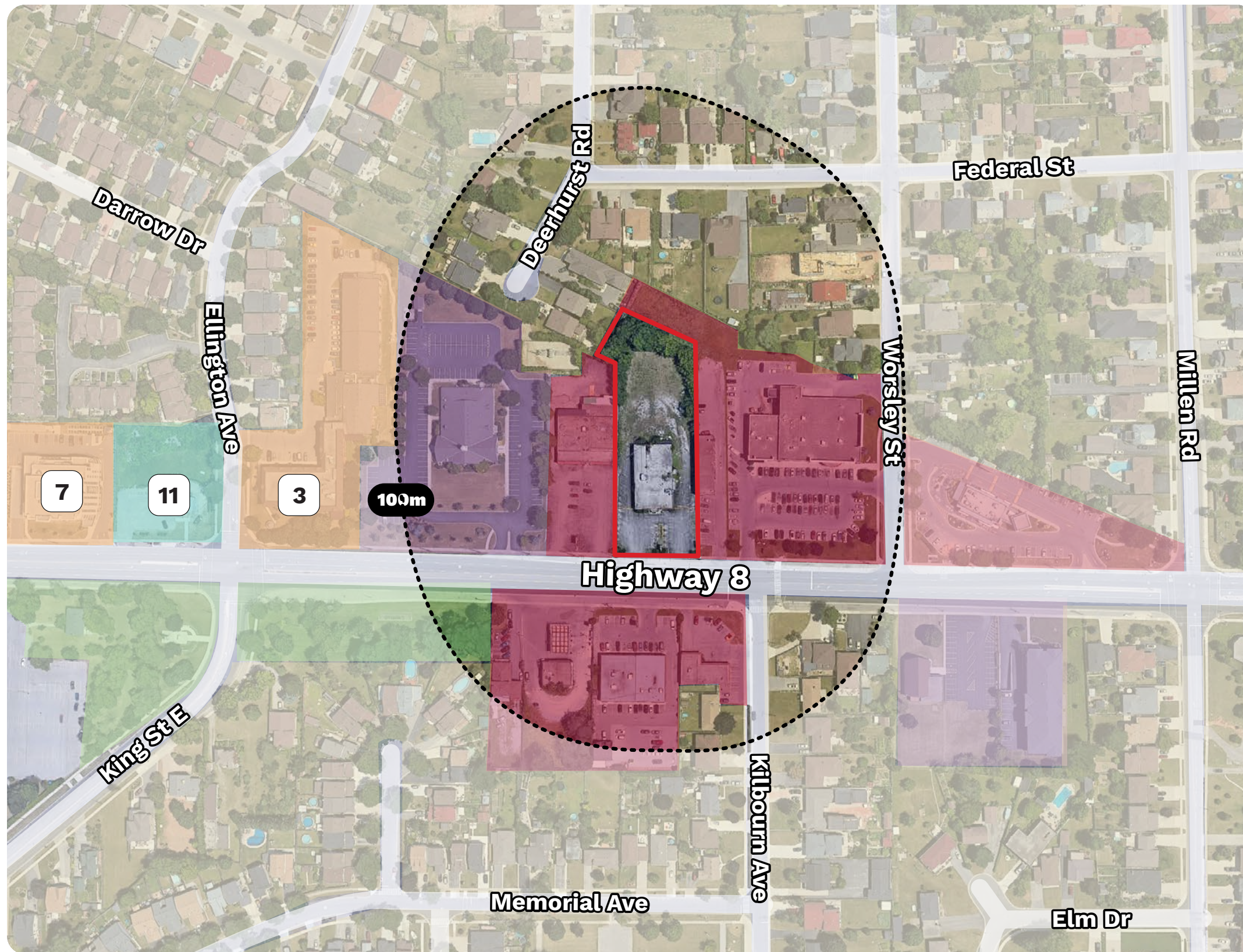
T. 416.947.9744
F. 416.947.0781

T. 905.549.3005
F. 416.947.0781

1.0 Surrounding Context Map



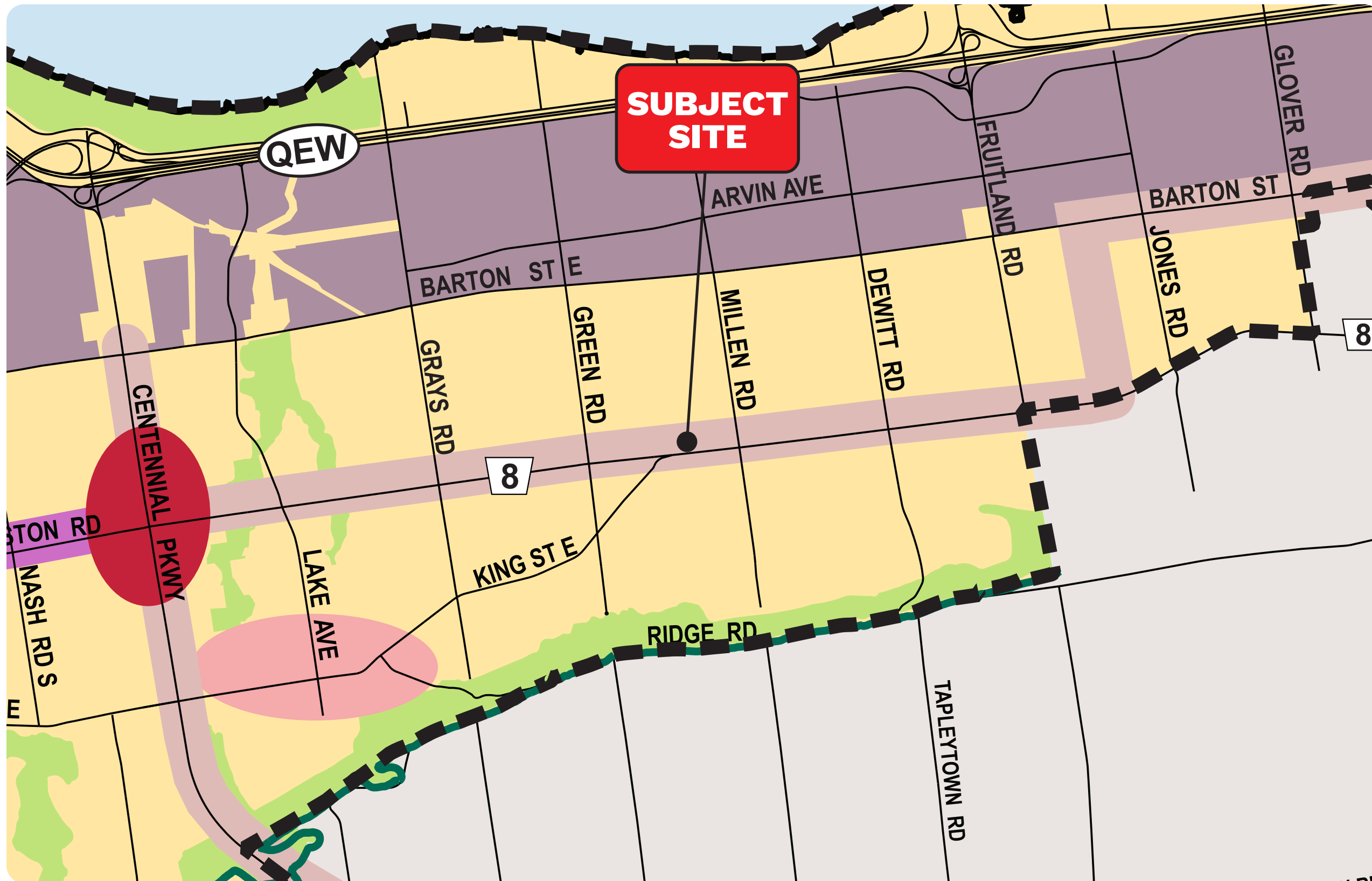
2.0 Neighbourhood Context



Legend

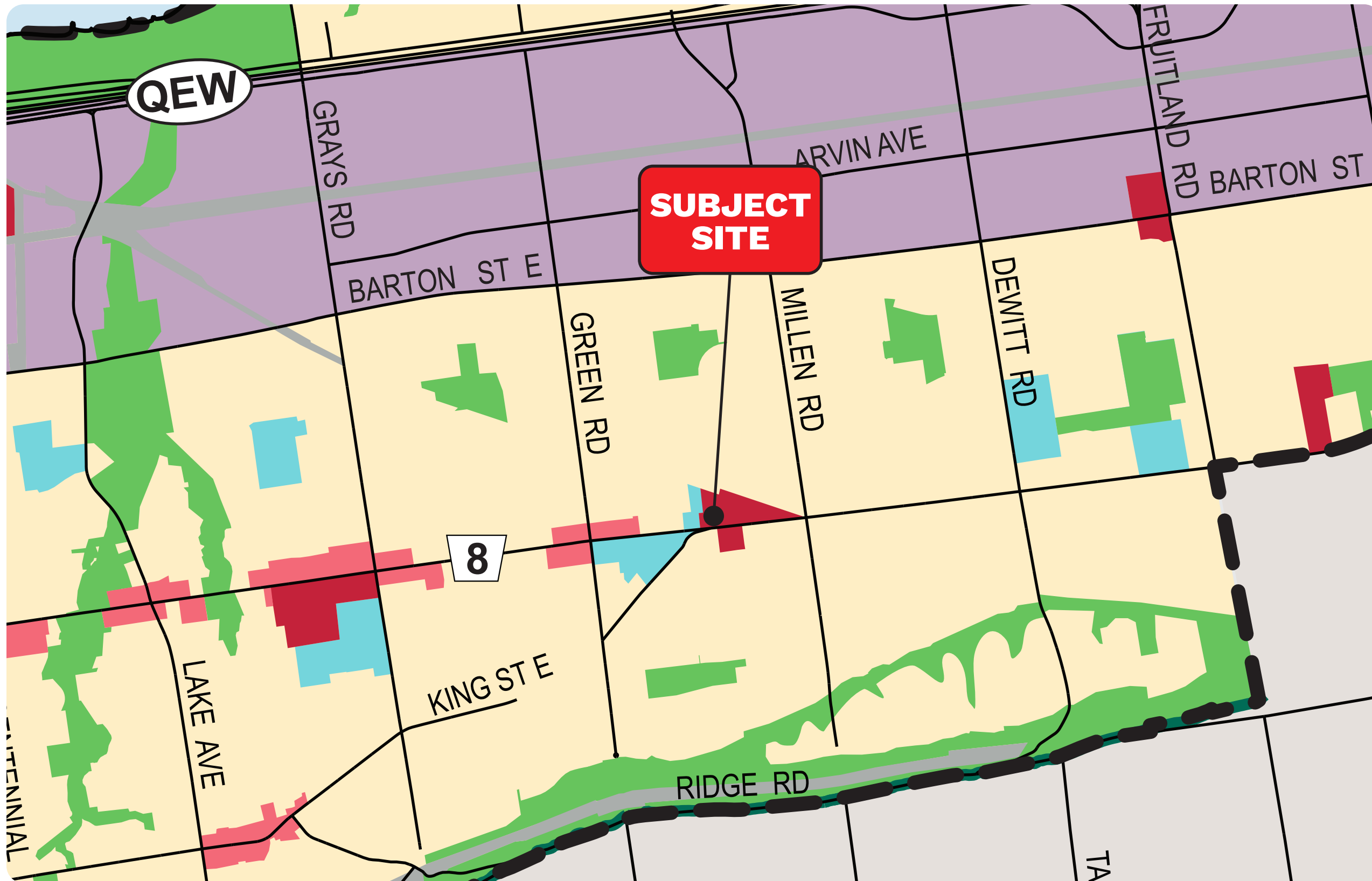
- Subject Site
- Study Area
- Low-Rise Residential
- Commercial/Retail
- Mid-Rise Residential
- Approved
- Park/Open Space
- Institutional
- # Number of Storeys

3.0 Planning Context



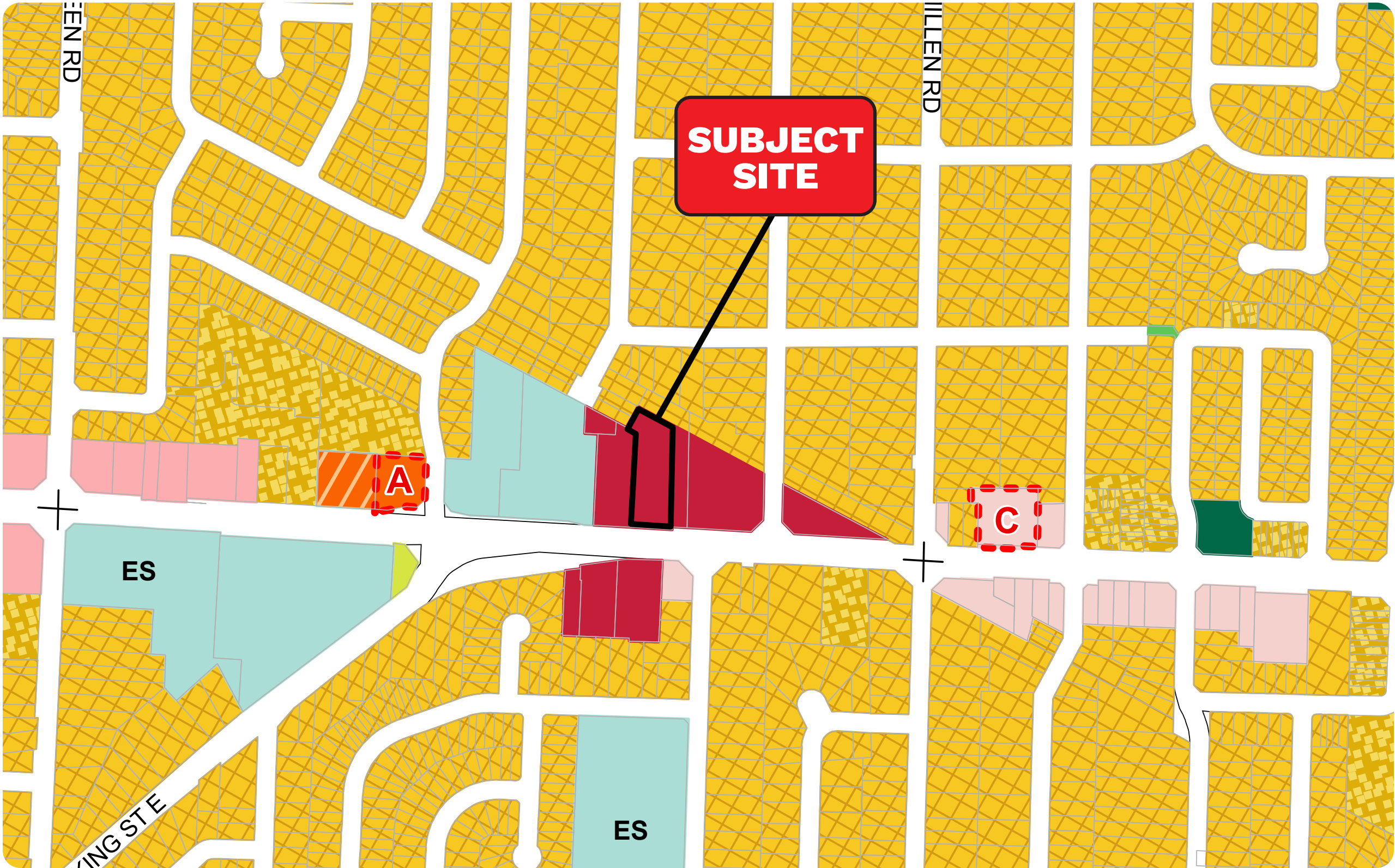
- Neighbourhoods
- Employment Areas
- Major Activity Centres
- Major Open Space
- Nodes**
- Downtown Urban Growth Centre
- Sub Regional Service
- Community
- Corridors**
- Primary
- Secondary
- Other Features**
- Rural Area
- ✈ John C. Munro Hamilton International Airport
- Niagara Escarpment
- Urban Boundary
- Municipal Boundary
- ▨ Lands Subject to Non Decision 113 West Harbour Setting Sail

Urban Hamilton Official Plan - Schedule E1
Urban Land Use Designations



- Neighbourhoods
- Open Space
- Institutional
- Utility
- Commercial and Mixed Use Designations**
- Downtown Mixed Use Area
- Mixed Use - High Density
- Mixed Use - Medium Density
- District Commercial
- Arterial Commercial
- Employment Area Designations**
- Industrial Land
- Business Park
- Airport Employment Growth District
- Shipping & Navigation
- Other Features**
- Rural Area
- John C. Munro Hamilton International Airport
- Niagara Escarpment
- Urban Boundary
- Municipal Boundary
- Lands Subject to Non Decision 113 West Harbour Setting Sail

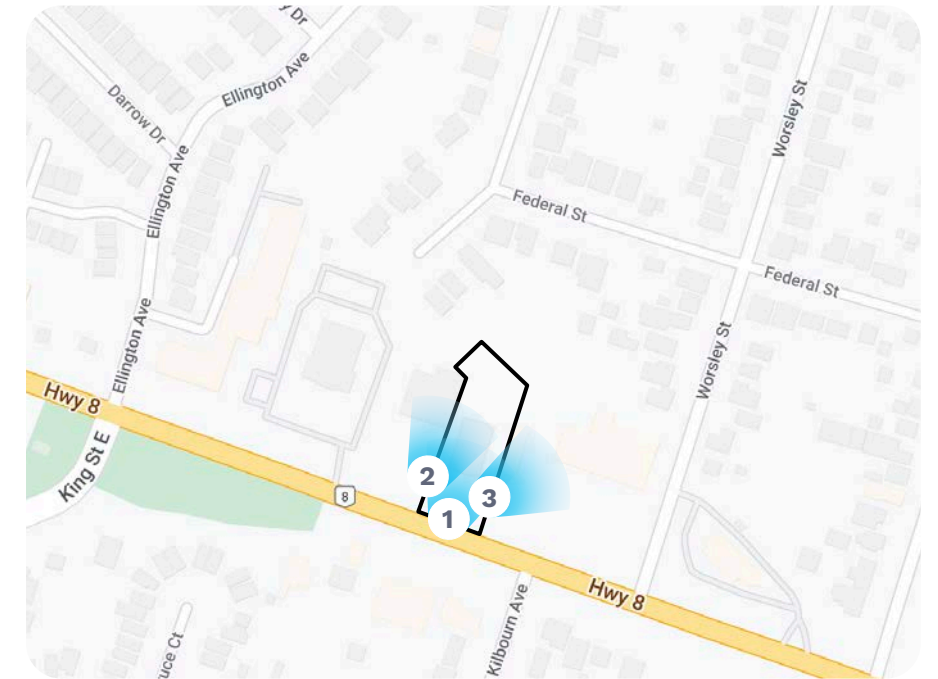
Western Development Area Secondary Plan Land Use Plan - Map B.7.1-1



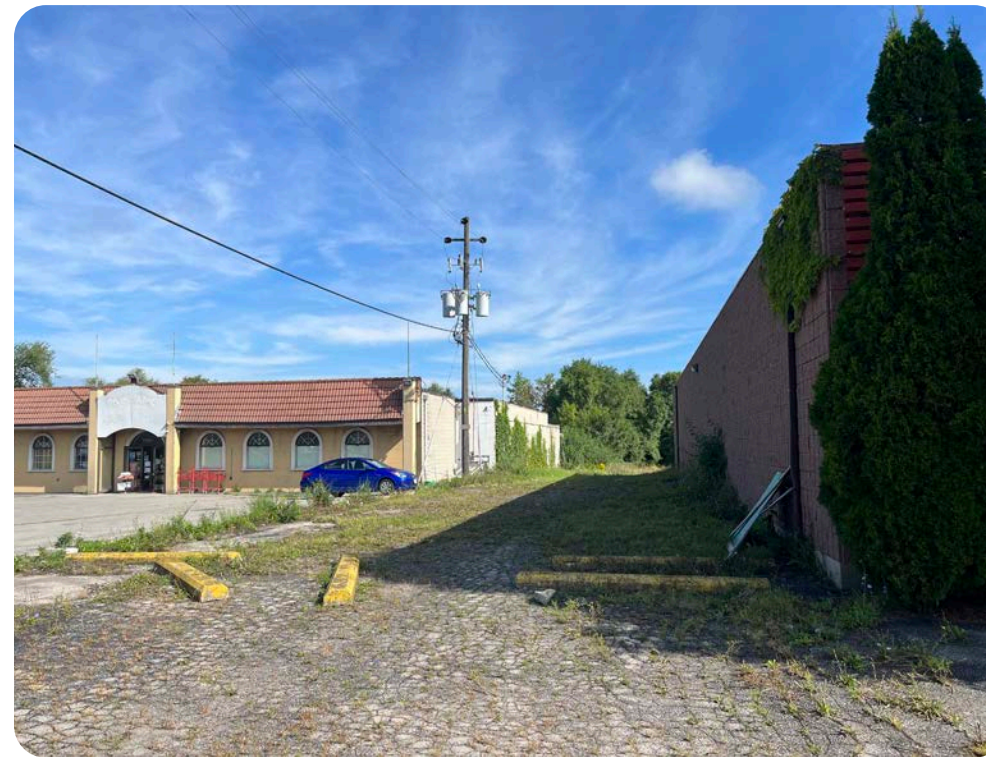
- Low Density Residential 2b
 - Low Density Residential 3c
 - Medium Density Residential 3
 - High Density Residential 1
- Commercial and Mixed Use Designations**
- Local Commercial
 - Mixed Use - Medium Density
 - District Commercial
- Parks and Open Space Designations**
- Parkette
 - Neighbourhood Park
 - Community Park
 - General Open Space
 - Natural Open Space
- Other Designations**
- Institutional
 - ES** Elementary School
 - SS** Secondary School
- Other Features**
- Area or Site Specific Policy
 - Secondary Plan Boundary

4.0 Site Context Photos

Key Map



1. On Highway 8 looking north towards the subject site.



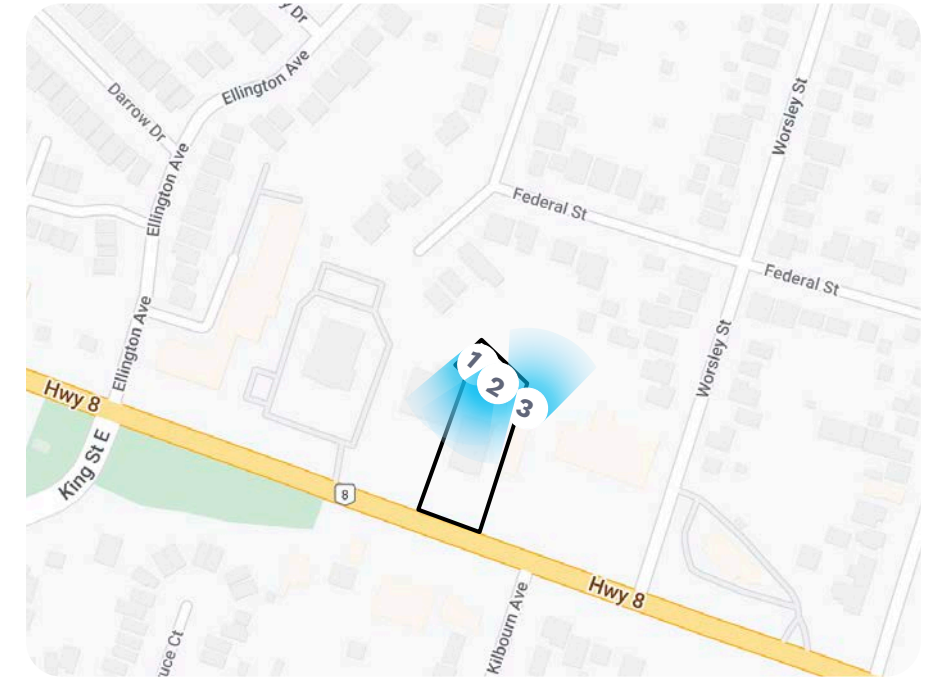
2. On Highway 8 looking north on the west side of the subject site.



3. On Highway 8 looking northeast.

Site Context Photos

Key Map



1. On the subject site looking south towards Highway 8.



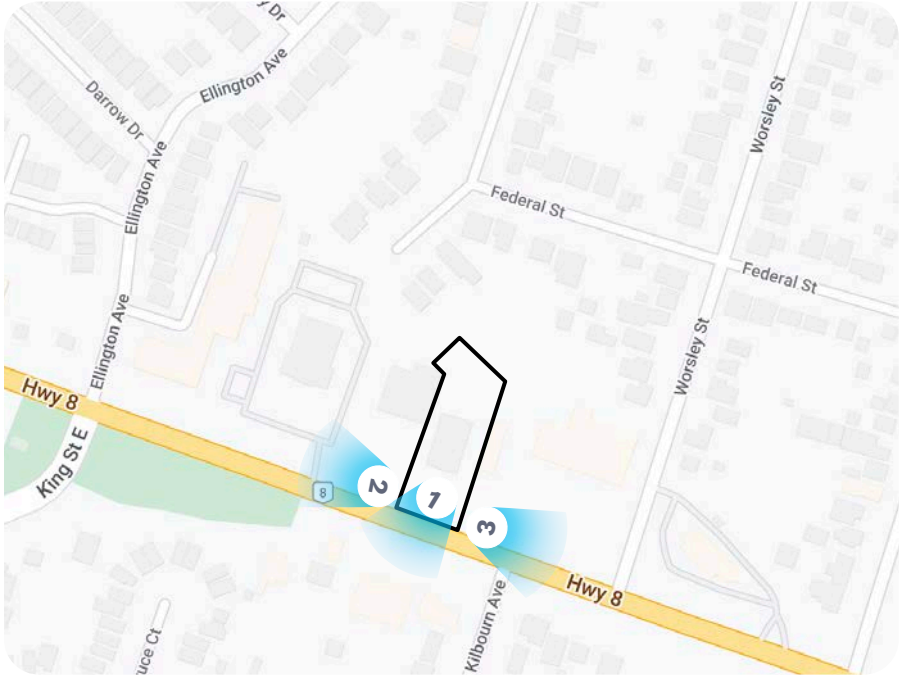
2. On the subject site looking south toward the rear of the existing building on site.



3. On the subject site looking north.

Site Context Photos

Key Map



1. On the subject site looking south towards Highway 8.



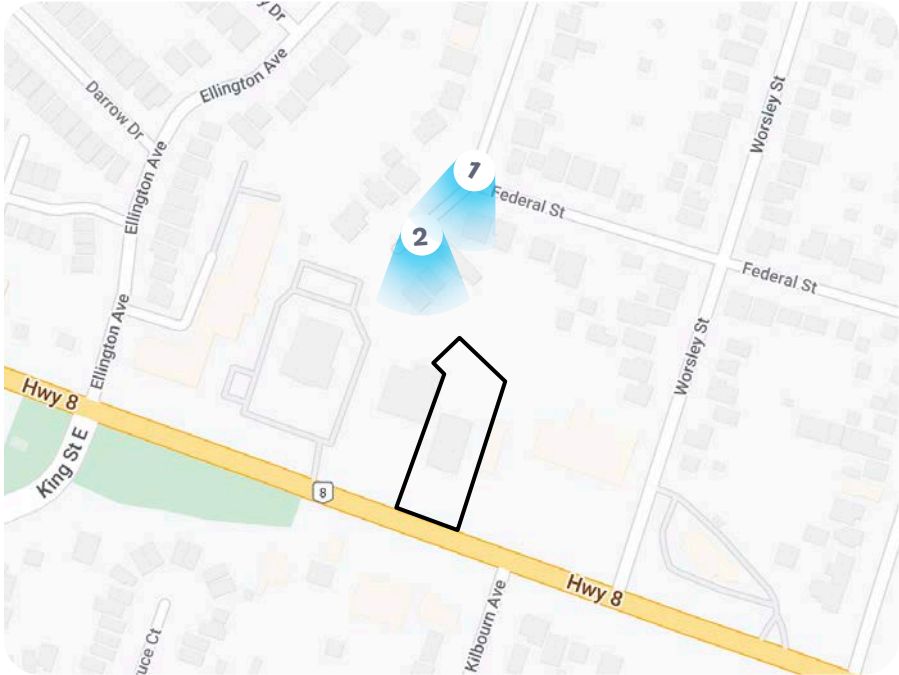
2. On the subject site looking west on Highway 8.



3. On the subject site looking east.

Site Context Photos

Key Map



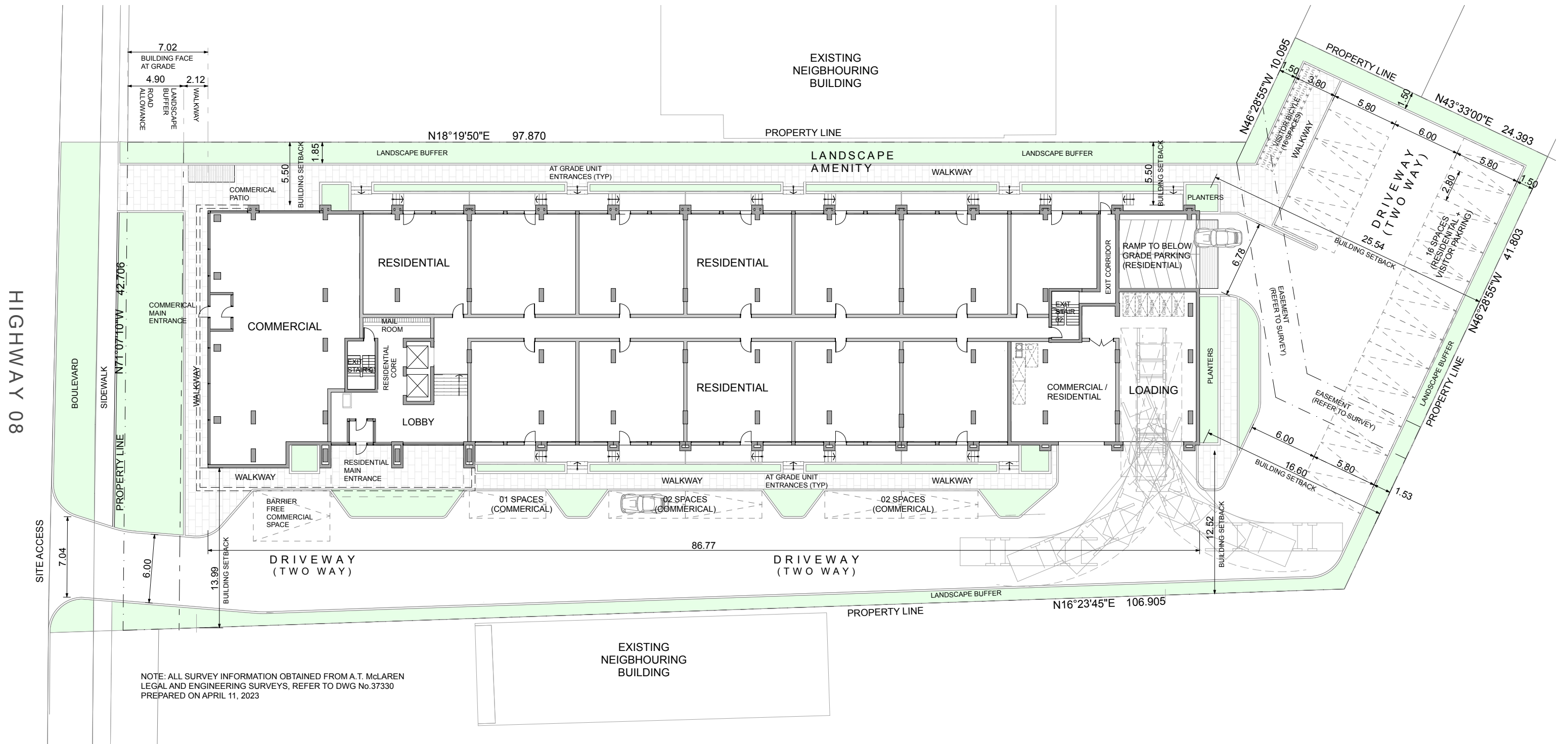
1. On Deerhurst Road looking south towards the subject site.



2. On Deerhurst Road looking southwest towards the subject site.

5.0 Architectural Plans

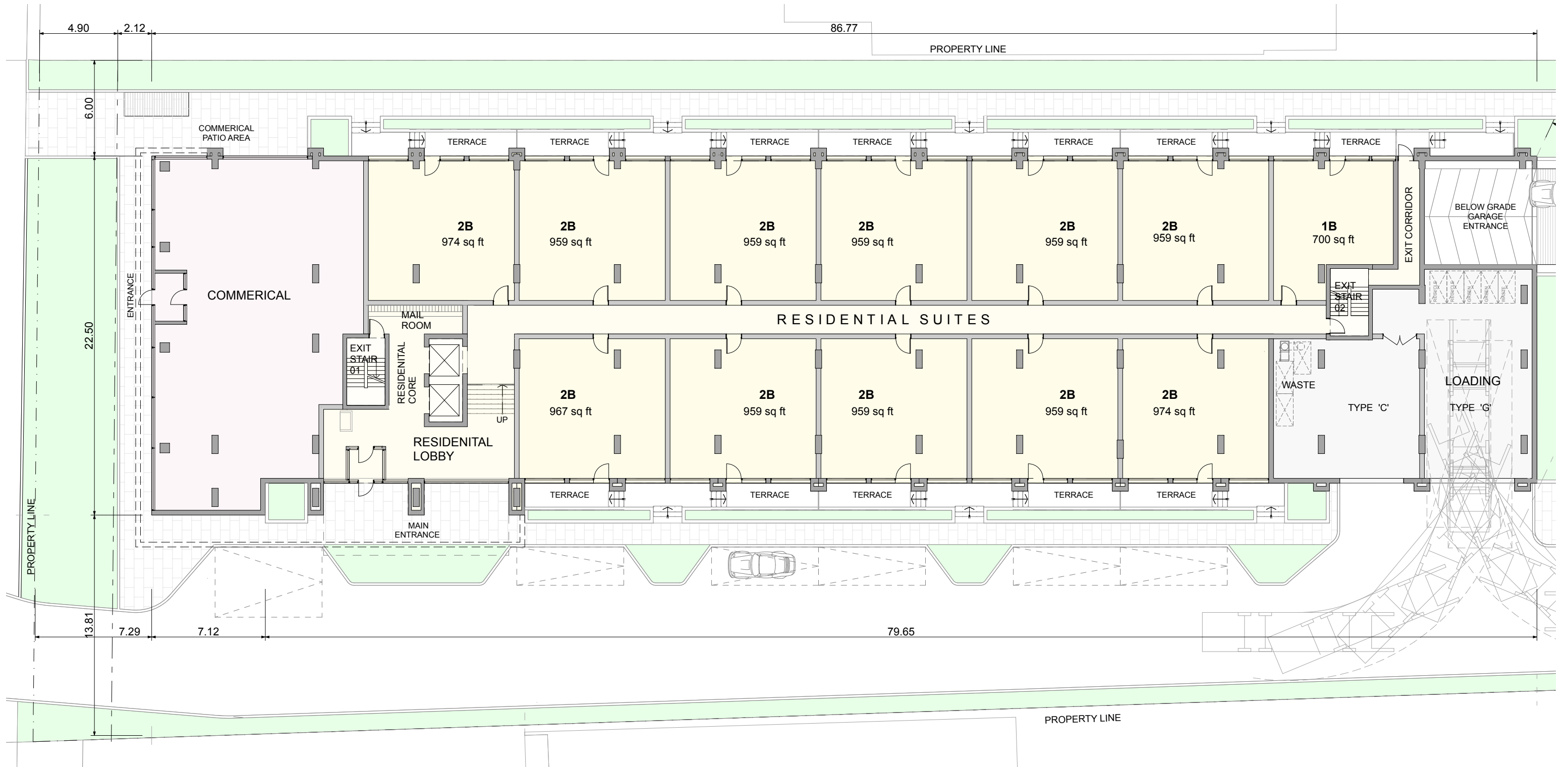
Site/Ground Floor Plan



■ Landscape



Ground Floor - Building

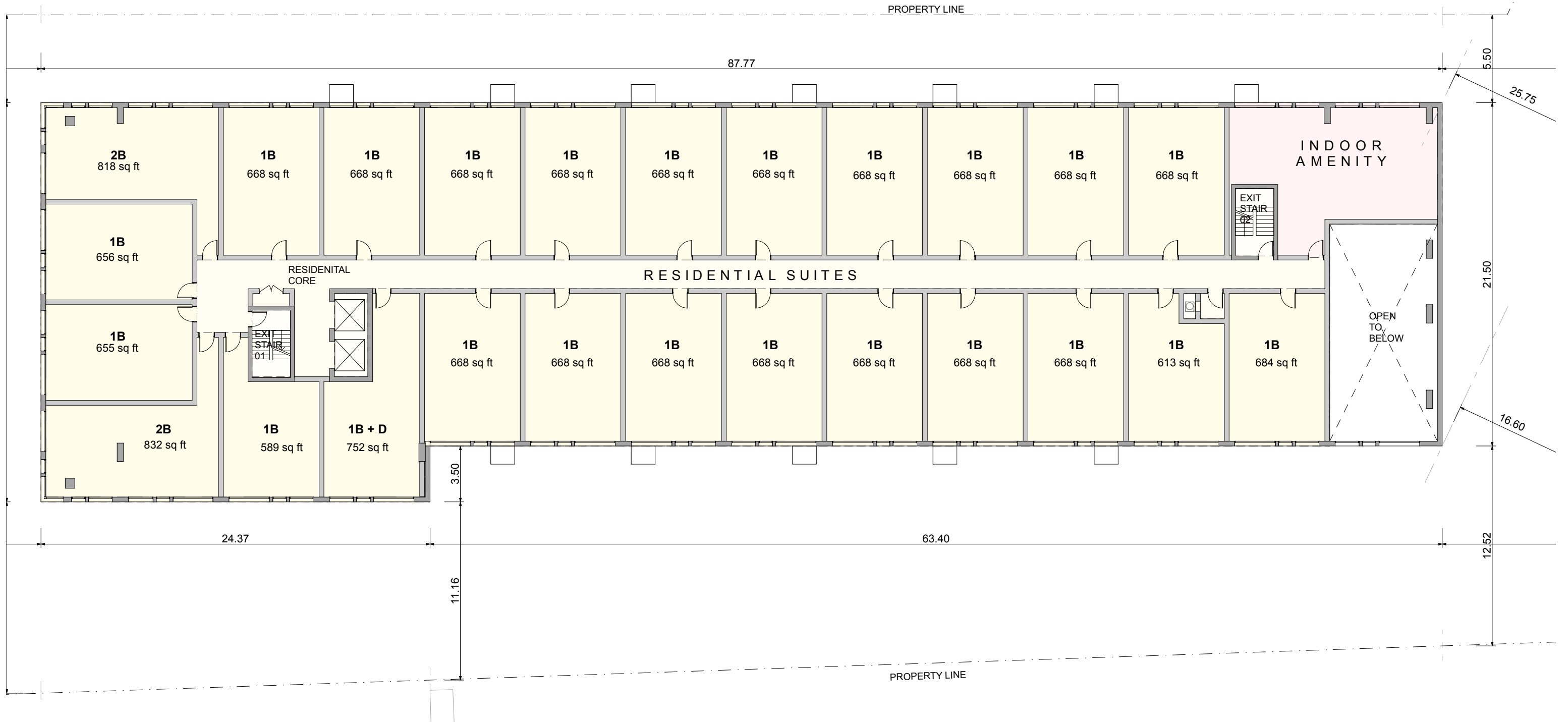


■ Landscape

■ Commercial

■ Residential

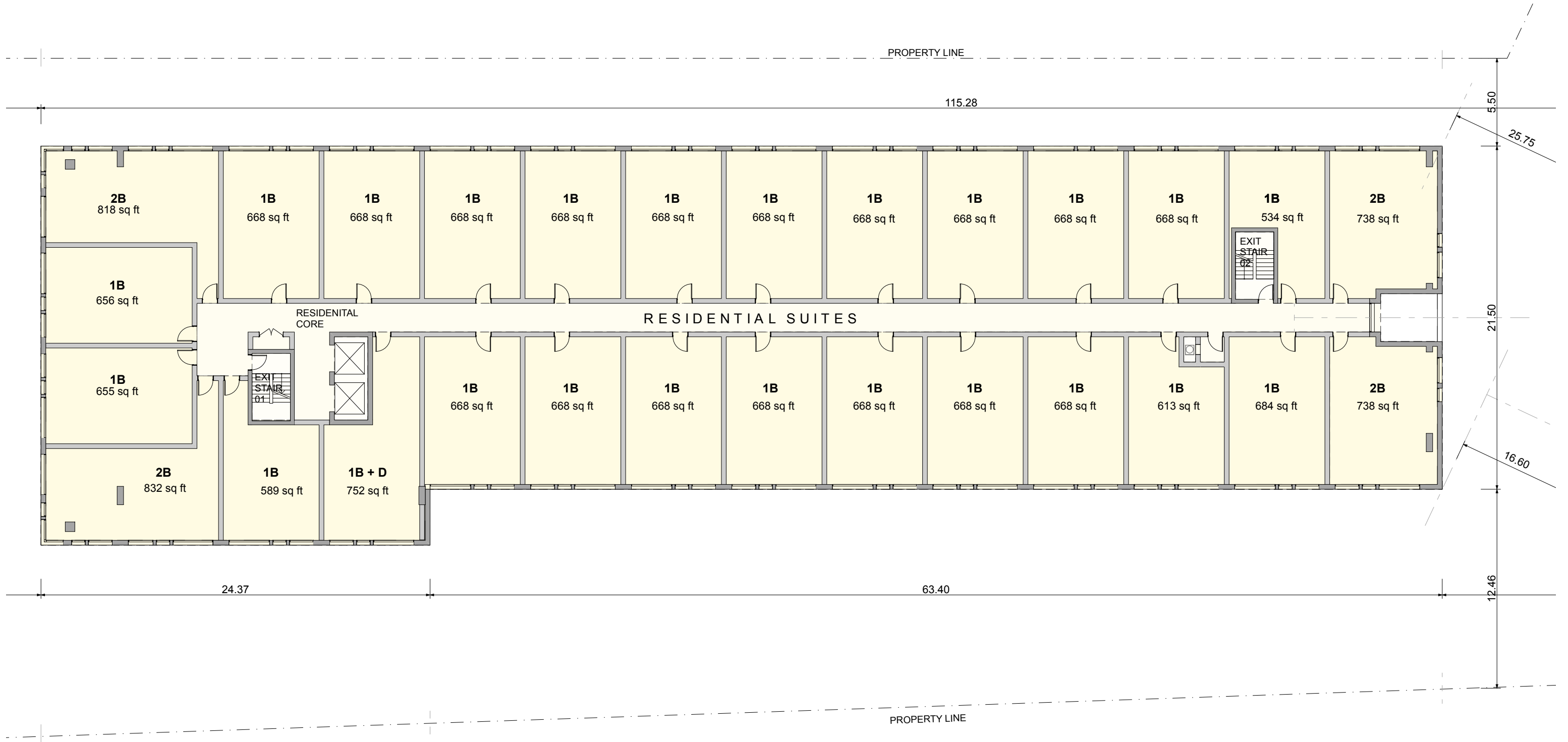




Indoor Amenity

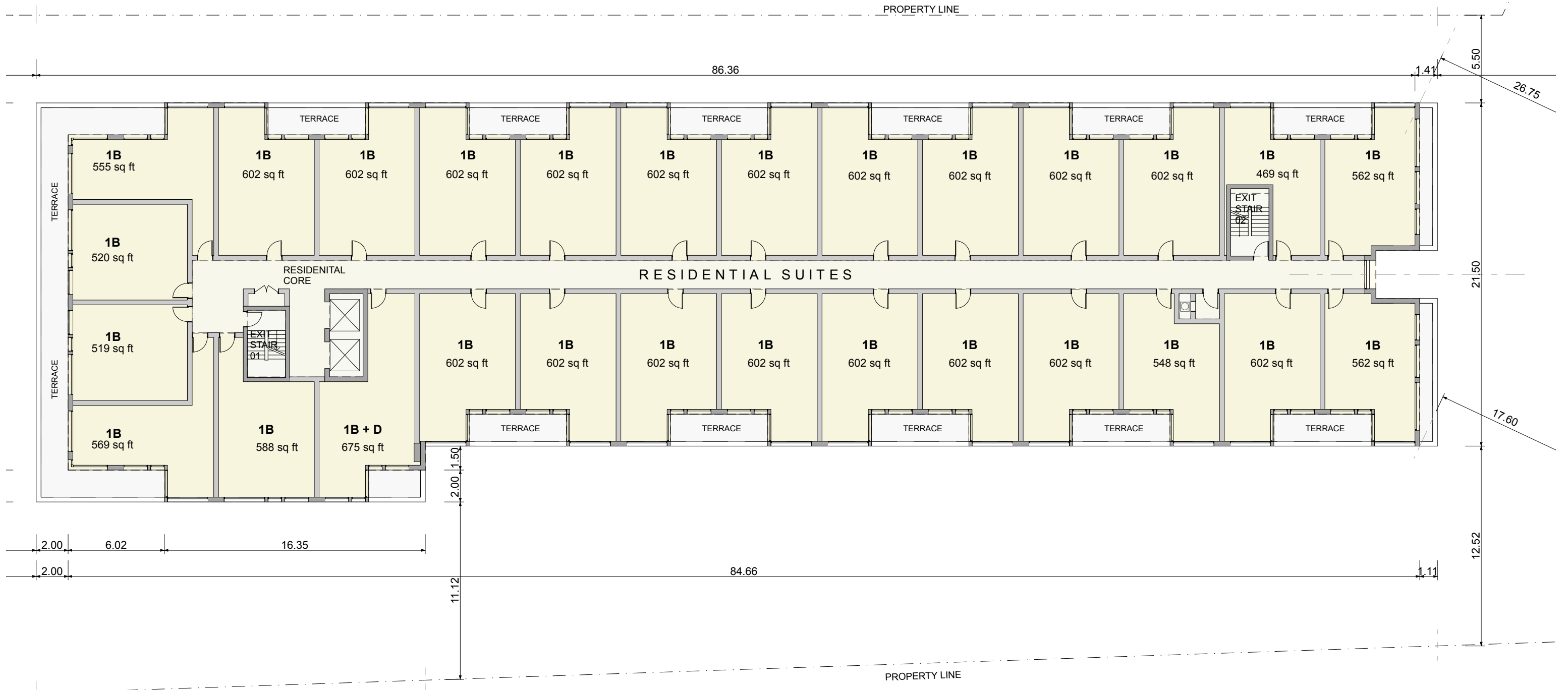
Residential





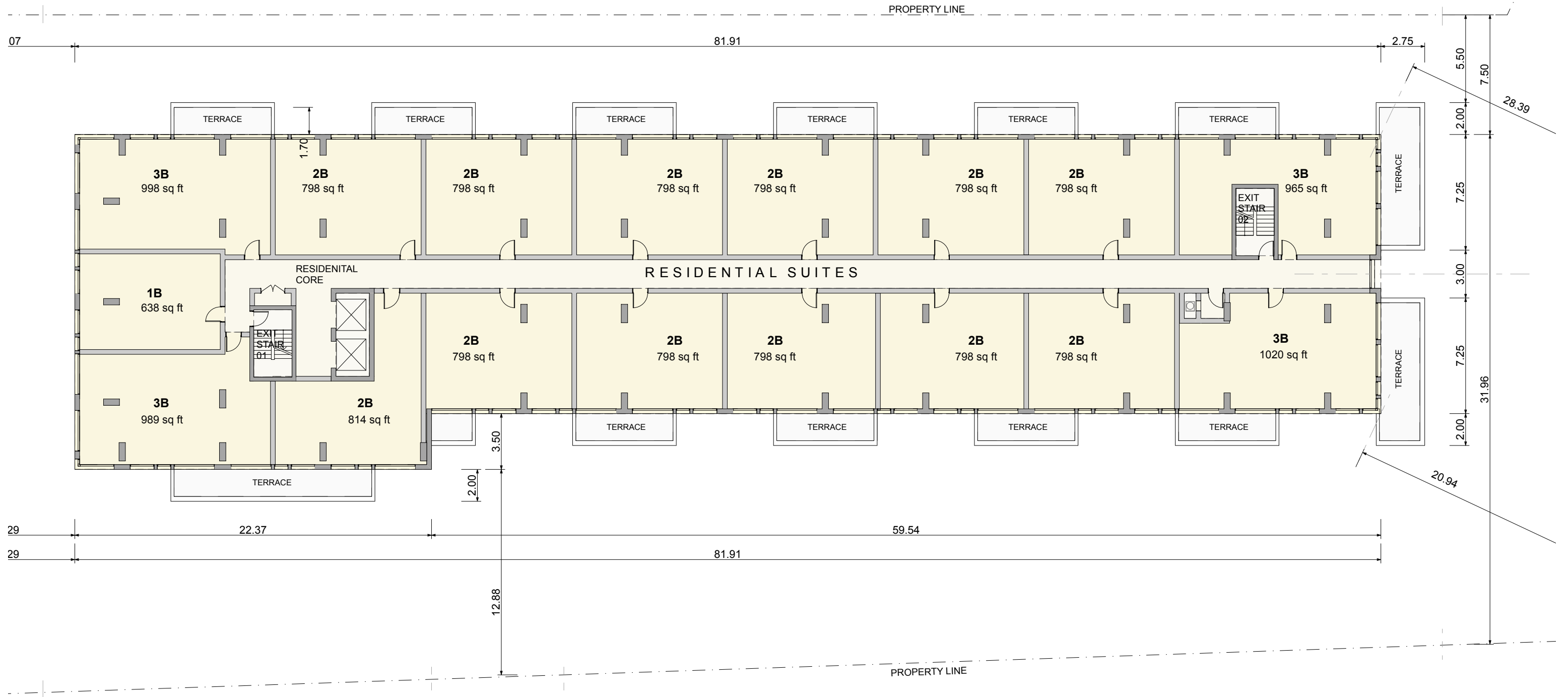
Residential





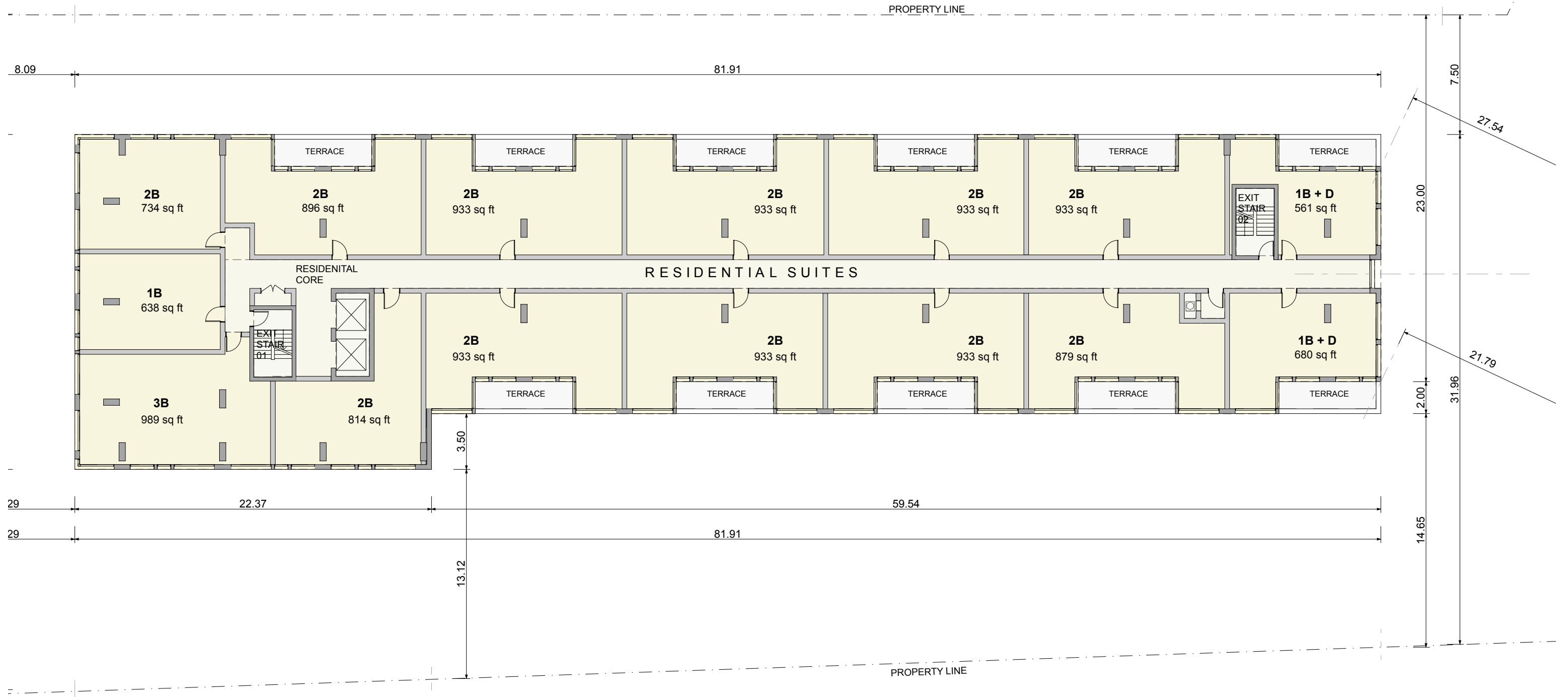
Residential





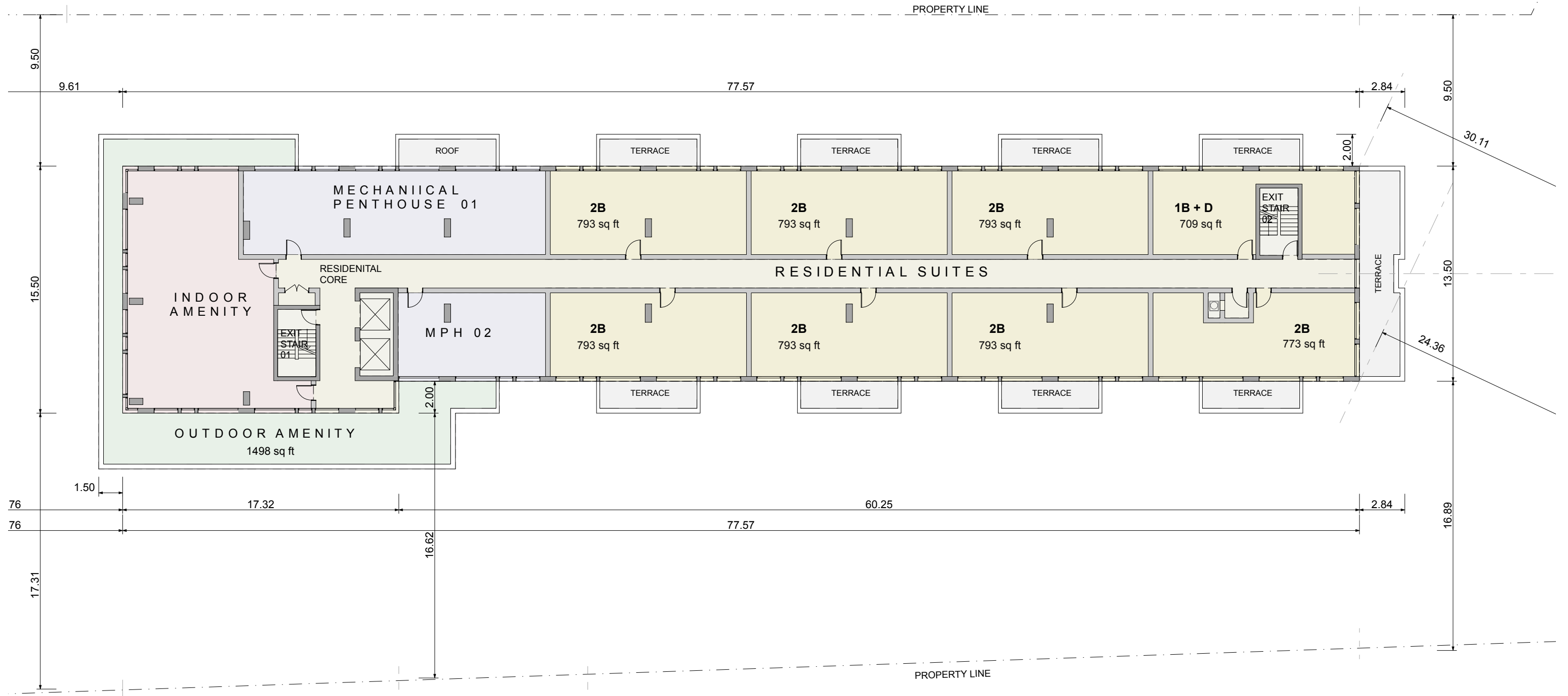
Residential





Residential





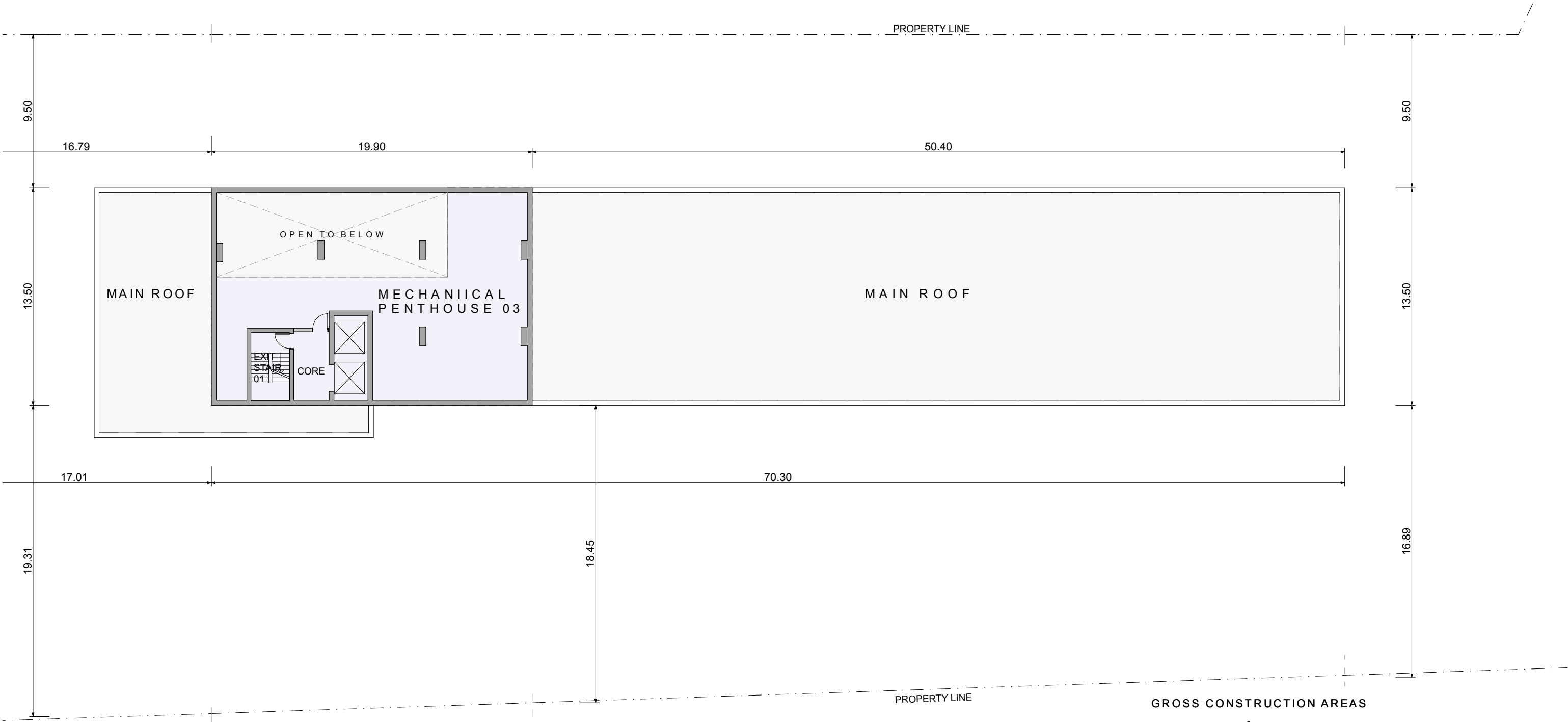
Outdoor Amenity

Indoor Amenity

Residential



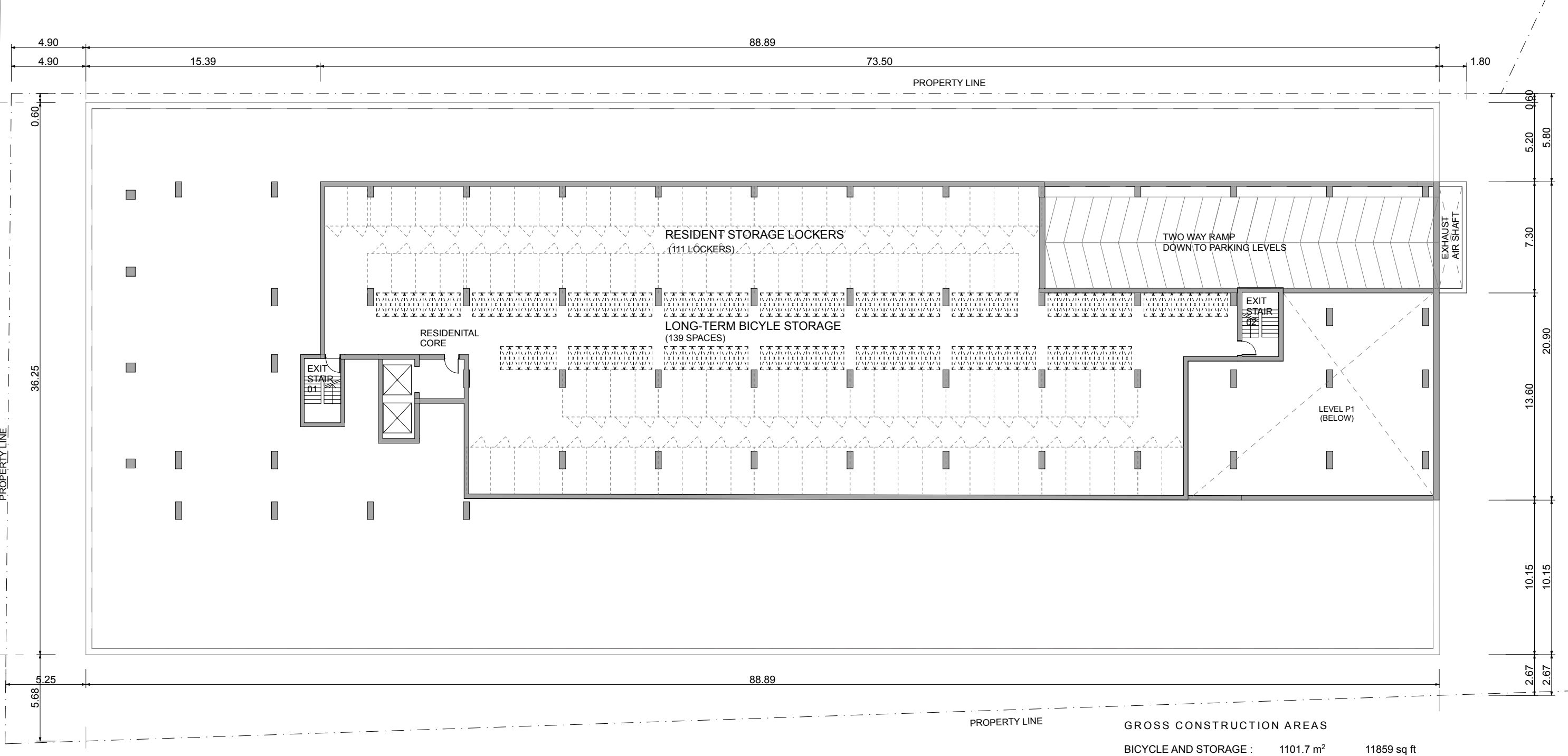
Roof Plans



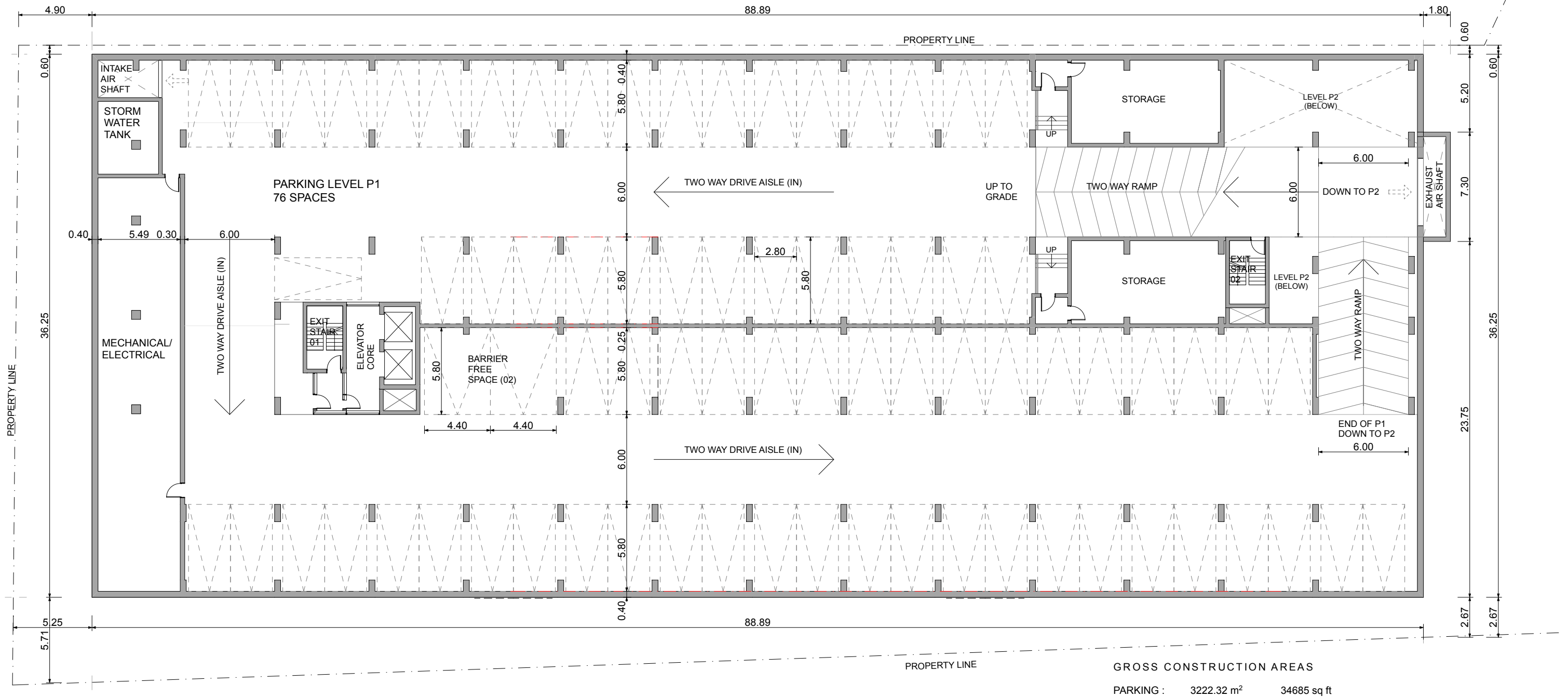
GROSS CONSTRUCTION AREAS
 MPH 03 : 187.32 m² 2016 sq ft



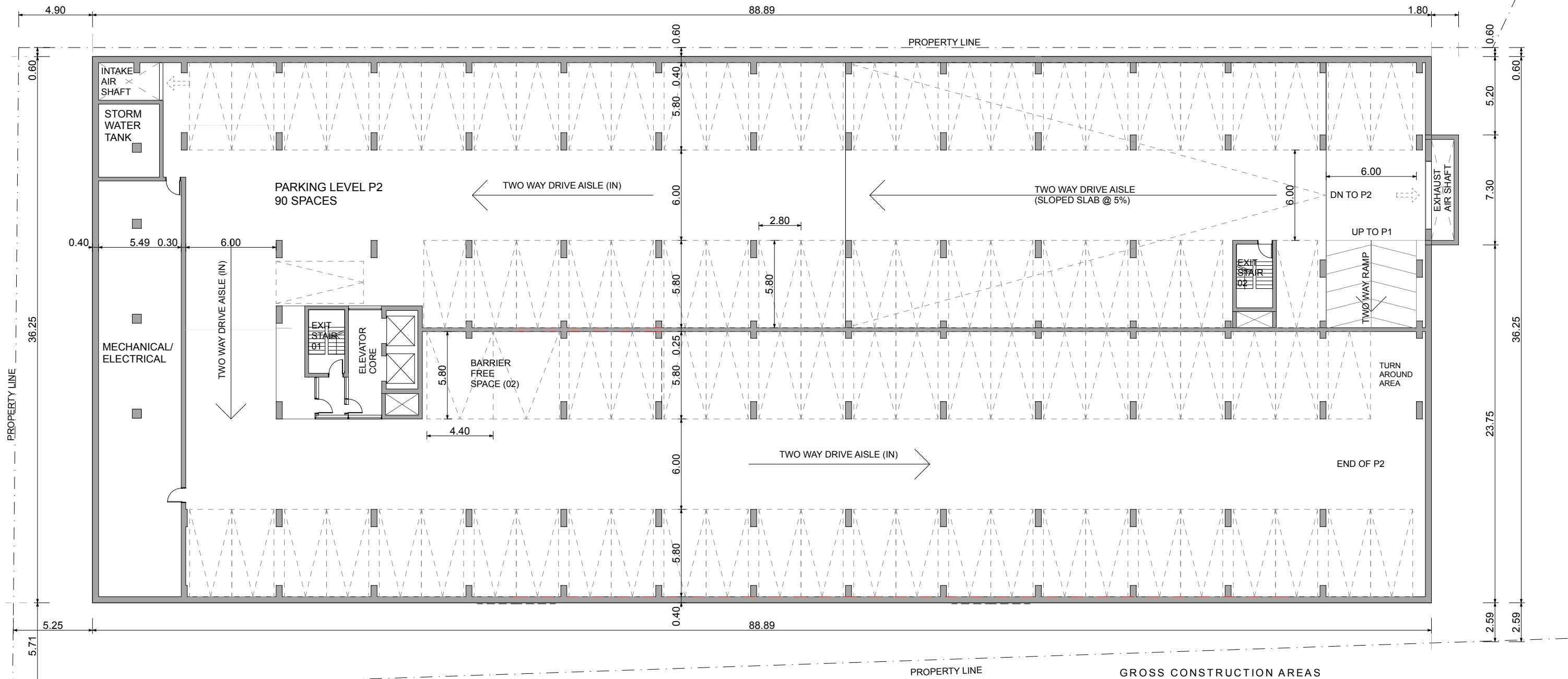
Bicycle Parking and Storage



Parking Level P1



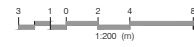
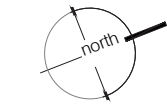
Parking Level P2



GROSS CONSTRUCTION AREAS
 PARKING : 3222.32 m² 34685 sq ft



6.0 Landscape Concept



SMALL CANOPY DECIDUOUS TREES PLANTED IN BOULEVARD DUE TO OVERHEAD WIRES
- TREES TO BE PLANTED AND SPECIES TO BE SELECTED BY THE CITY OF HAMILTON
- TREES PLANTED AT 7.0m O.C.

RAISED CONCRETE ACCENT WALL TO MATCH ARCHITECTURAL DESIGN

EXISTING 1.5m WIDTH MUNICIPAL CONCRETE SIDEWALK TO REMAIN

DECORATIVE UNIT PAVING WITH BACKED BENCHES AT COMMERCIAL UNIT ENTRANCE

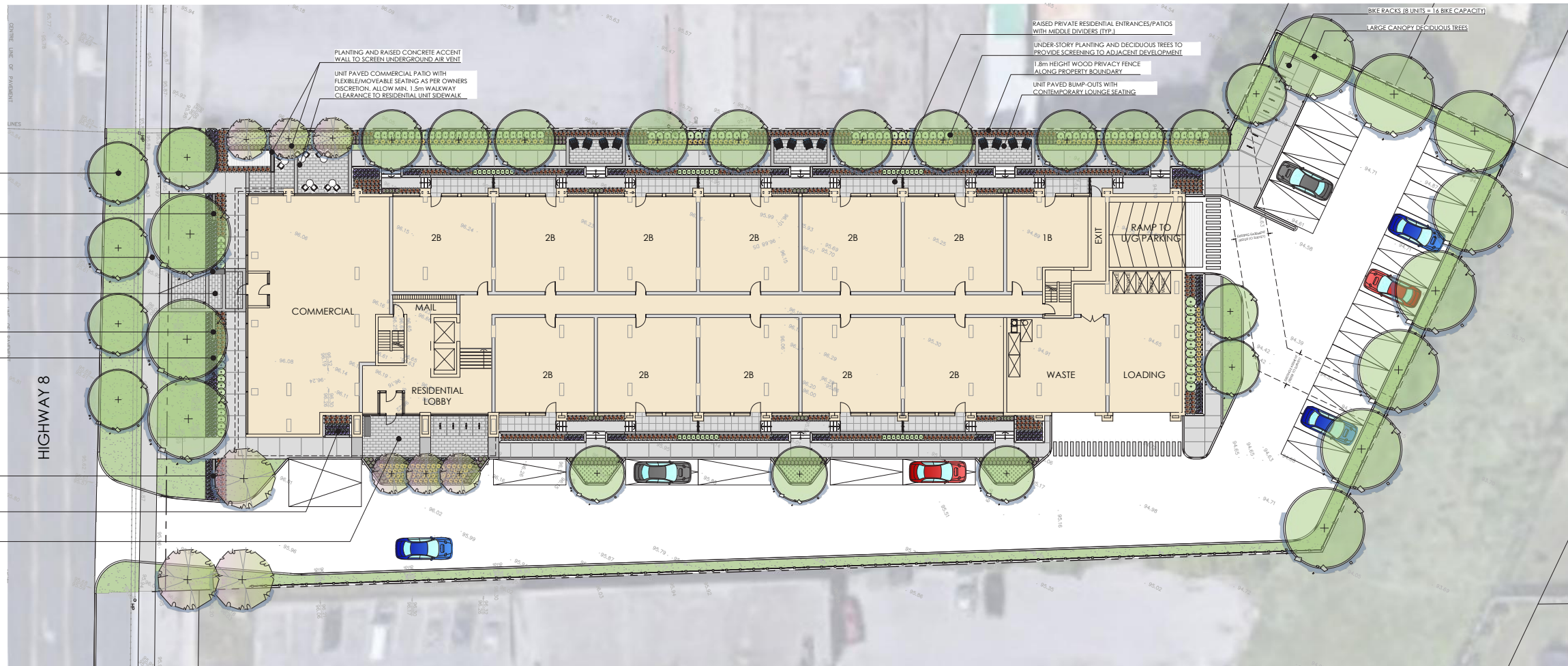
RAISED CONCRETE ACCENT WALL TO MATCH ARCHITECTURAL DESIGN

FLOWERING AND WINTER INTEREST PLANT MATERIAL ALONG FRONTAGE

RAISED CONCRETE ACCENT WALL TO MATCH ARCHITECTURAL DESIGN

SHADE TOLERANT SPECIES BELOW ARCHITECTURAL OVERHANG

DECORATIVE UNIT PAVING WITH BACKED BENCHES AND BIKE RACKS (4 UNITS) AT MAIN RESIDENTIAL ENTRANCE



Backed Benches



Bike Racks



Contemporary Lounge Chairs



Precast Concrete Unit Paving

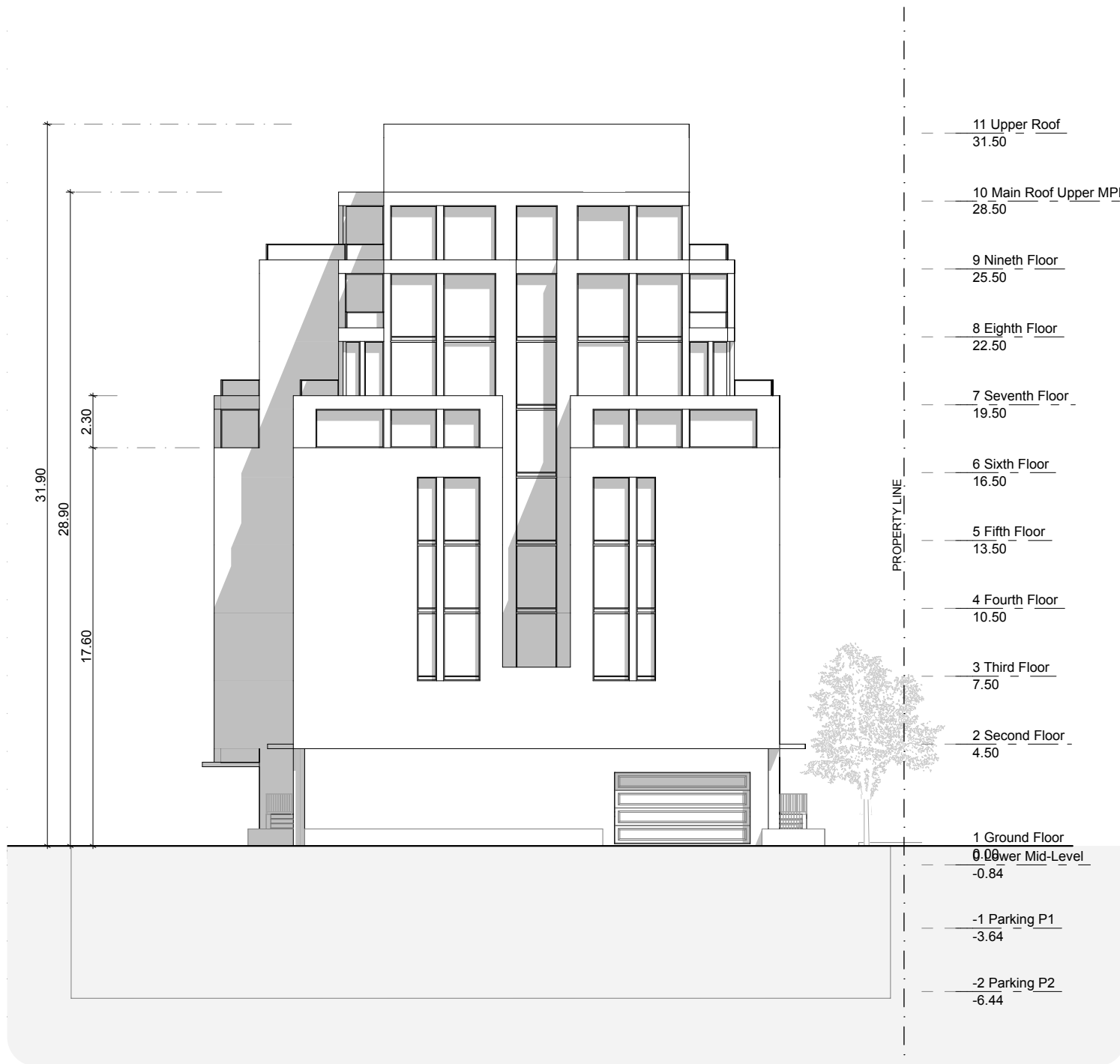


Raised Entry Accent Walls

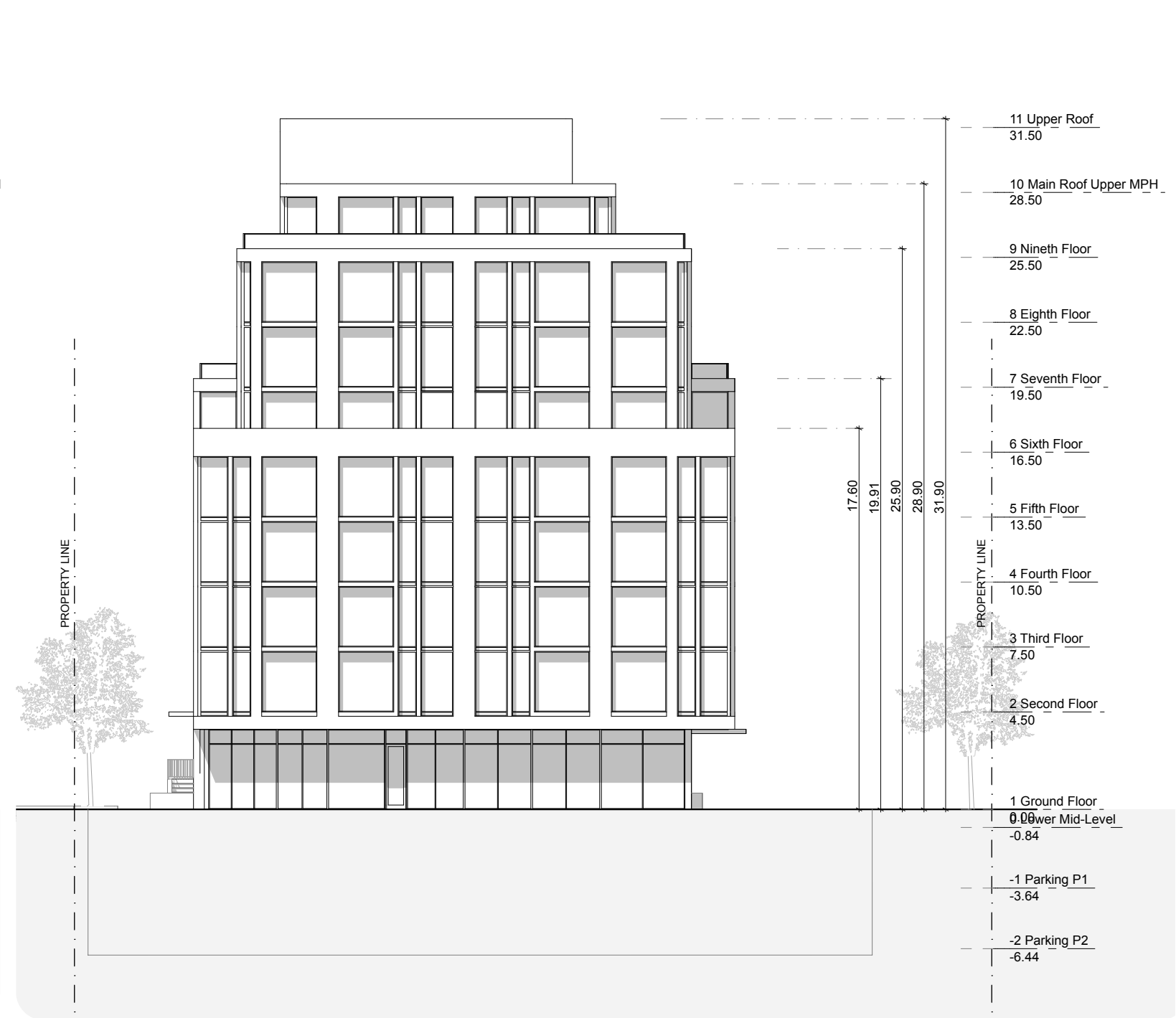


Flowering & Winter Interest Massed Planting

7.0 Elevations and Sections



North Elevation



South Elevation



West Elevation





East Elevation



8.0 Massing



3D View Looking Northwest



3D View Looking Northwest



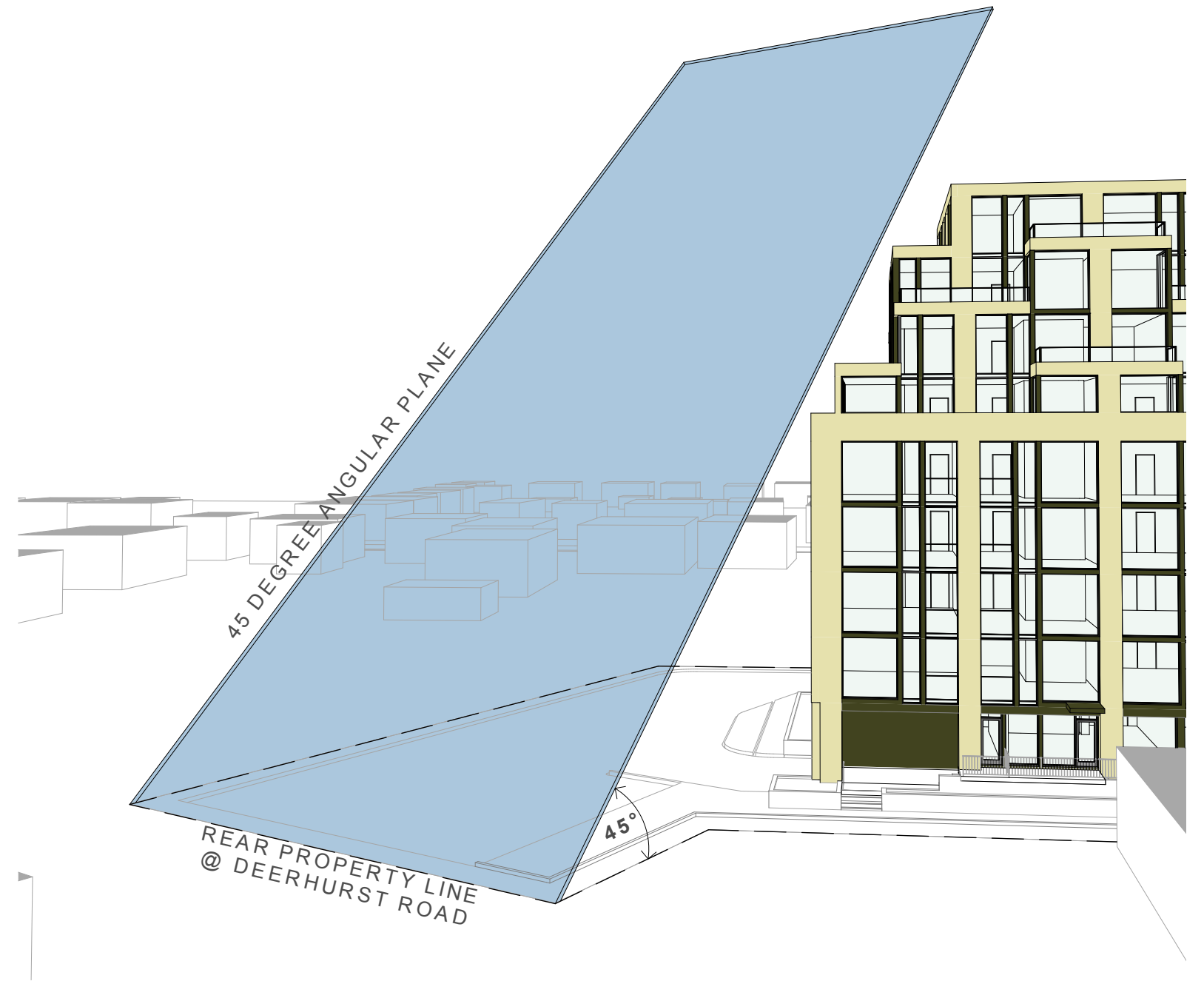
3D View Looking Northeast



3D View Looking Northwest



3D View Looking Northeast



3D View with Angular Planes Looking Southeast

9.0 Proposed Rendering

View looking Northwest



View looking Northeast



View Looking North (west side of building)

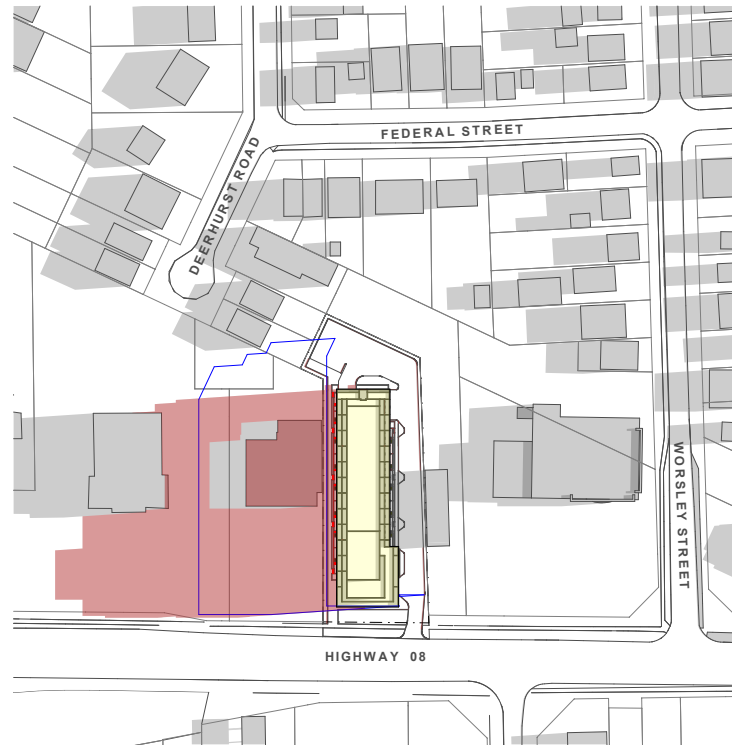


View Looking North (east side of building - Commercial)

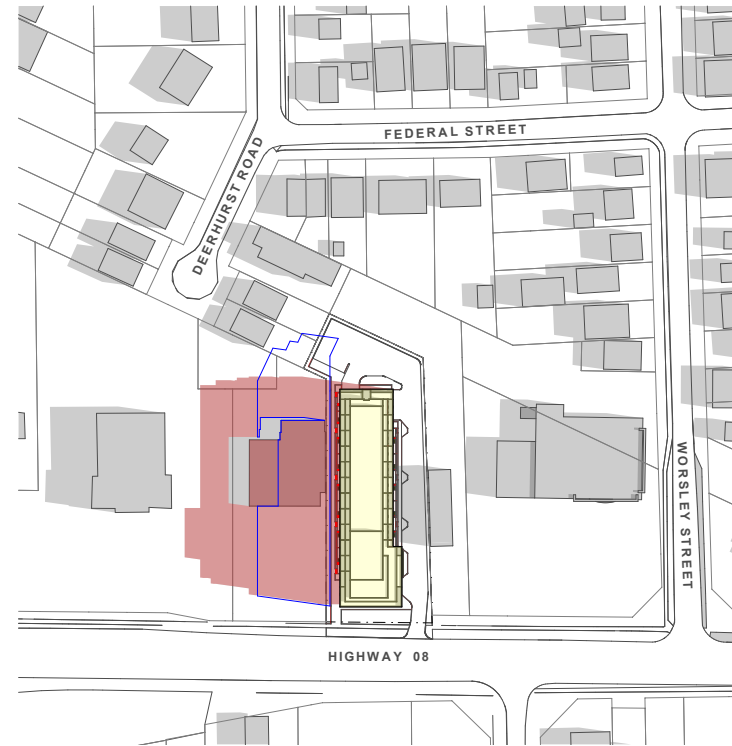


10.0 Shadow Study

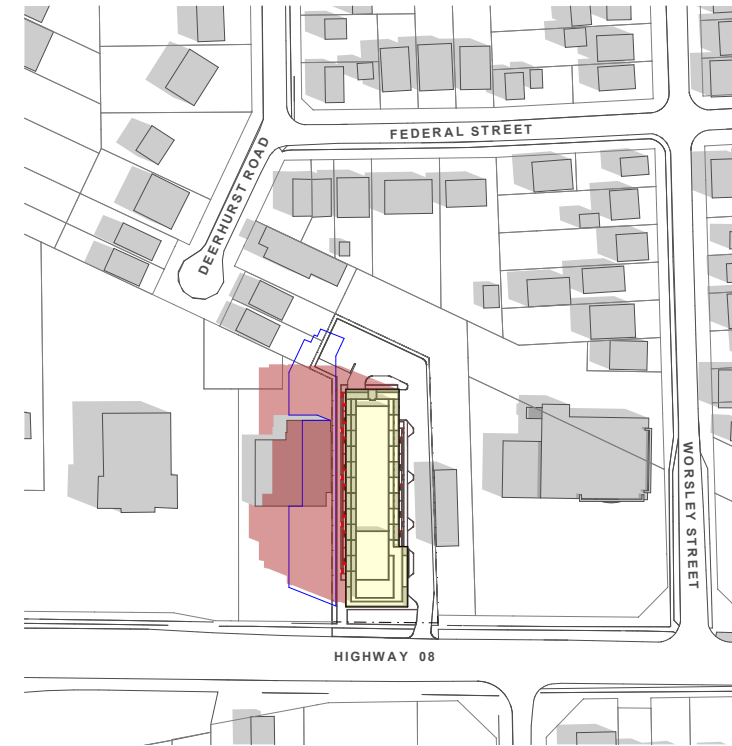
March 21st



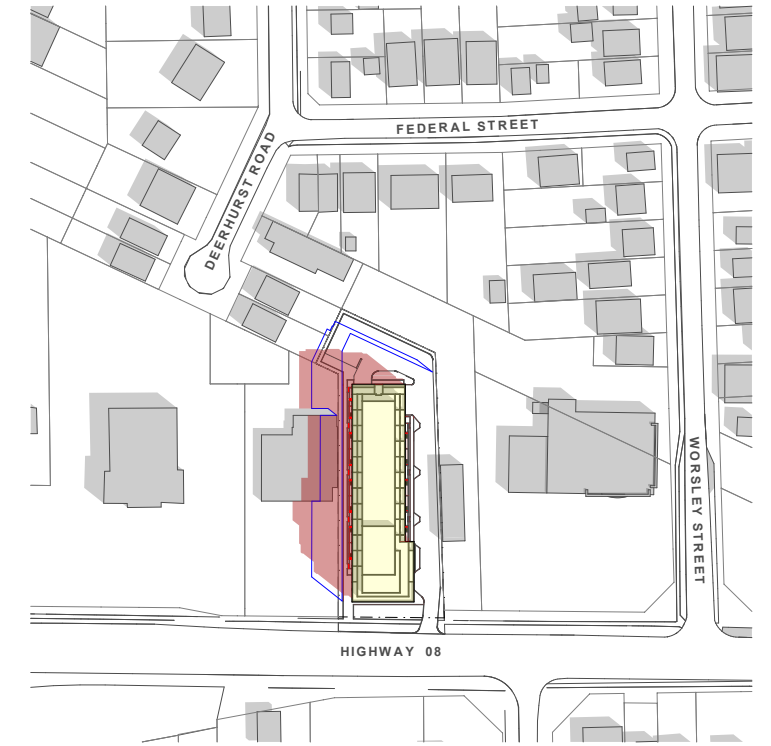
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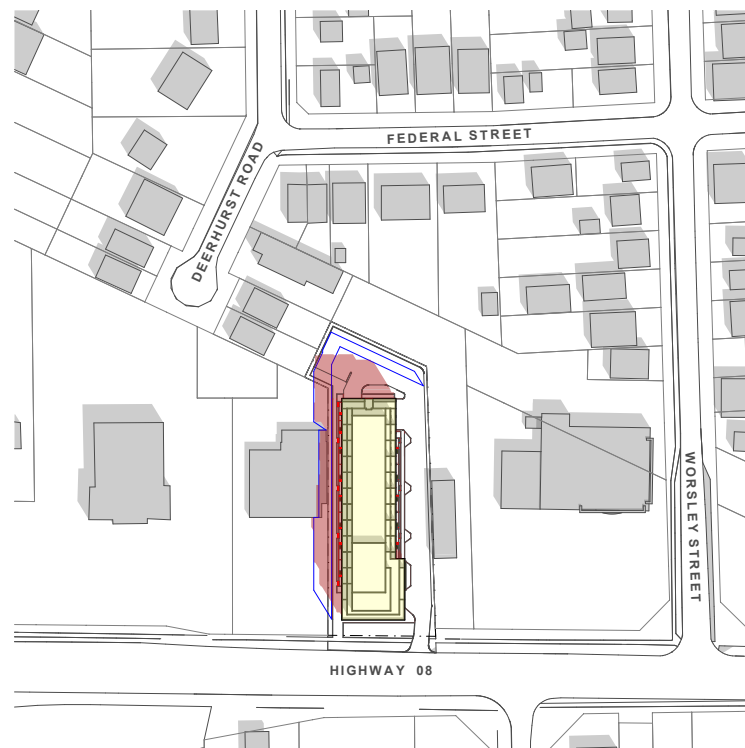
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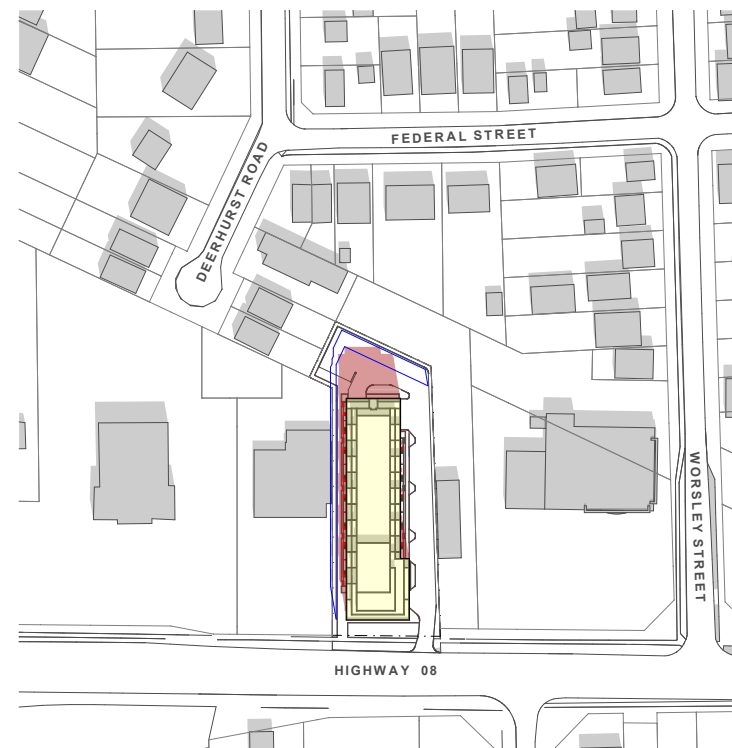
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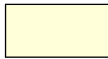

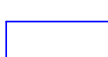

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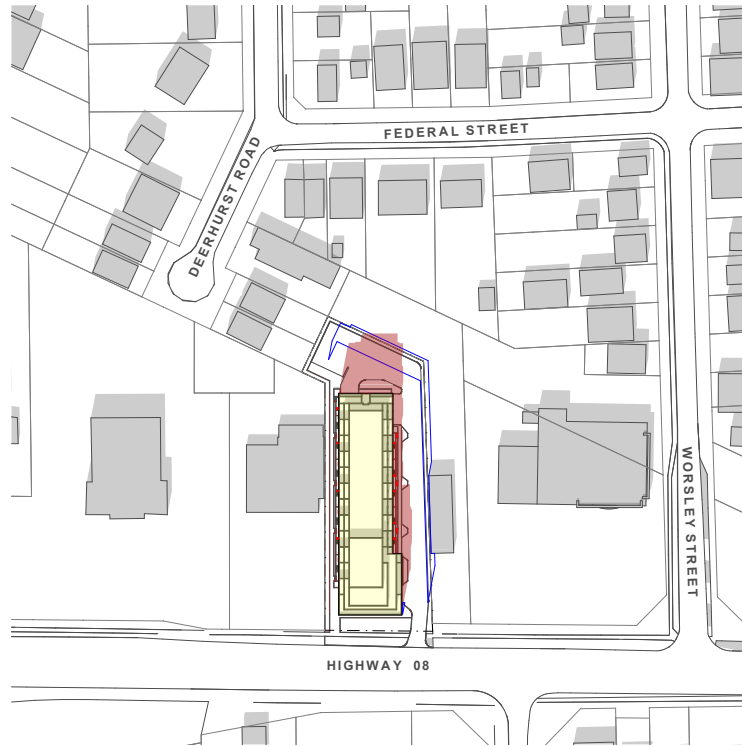


12:50 PM



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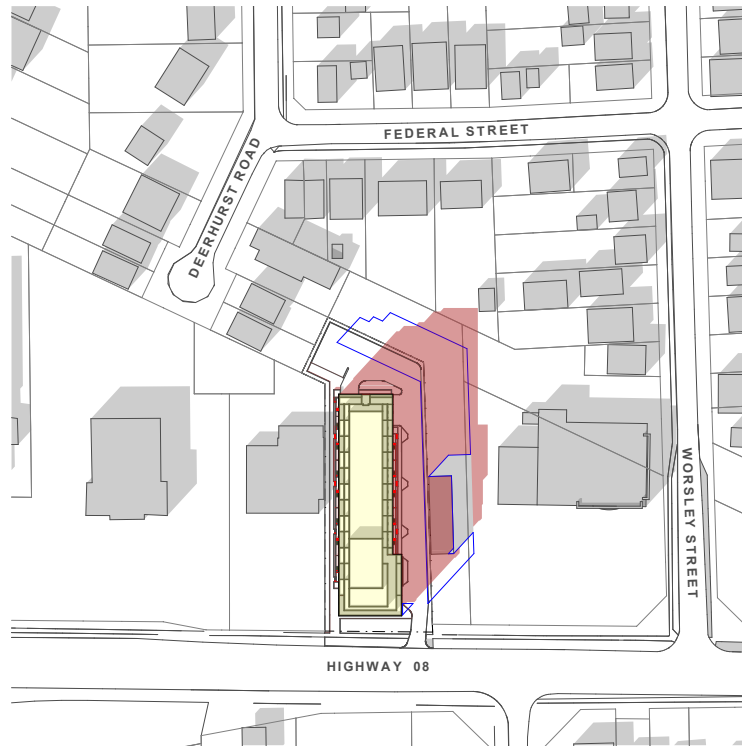
-  SUBJECT SITE BOUNDRY
-  PROPOSED DEVELOPMENT
-  SHADOWS CAST BY EXISTING BUILDINGS
-  SHADOWS CAST BY AS-OF-RIGHT BUILDING
-  SHADOWS CAST BY PROPOSED DEVELOPMENT



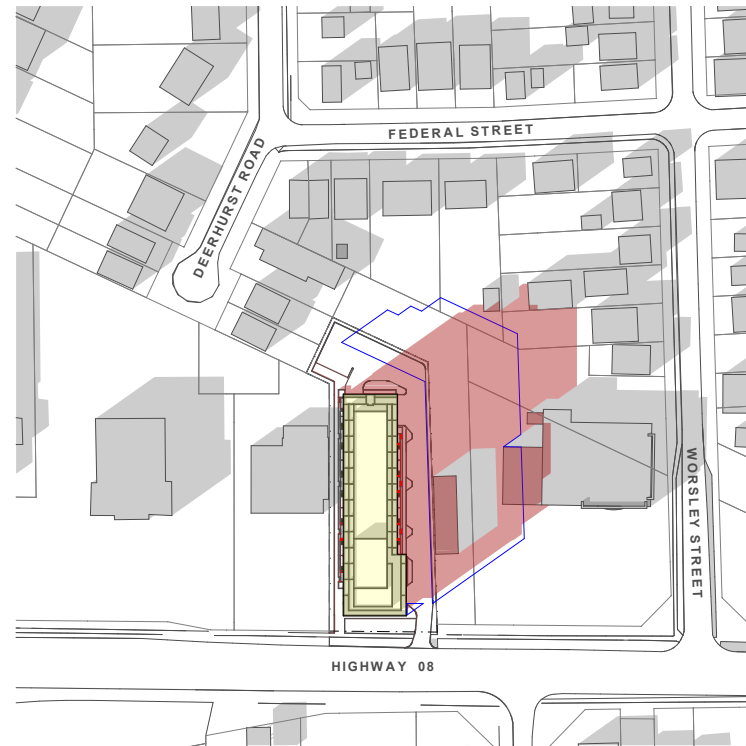
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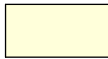

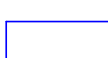

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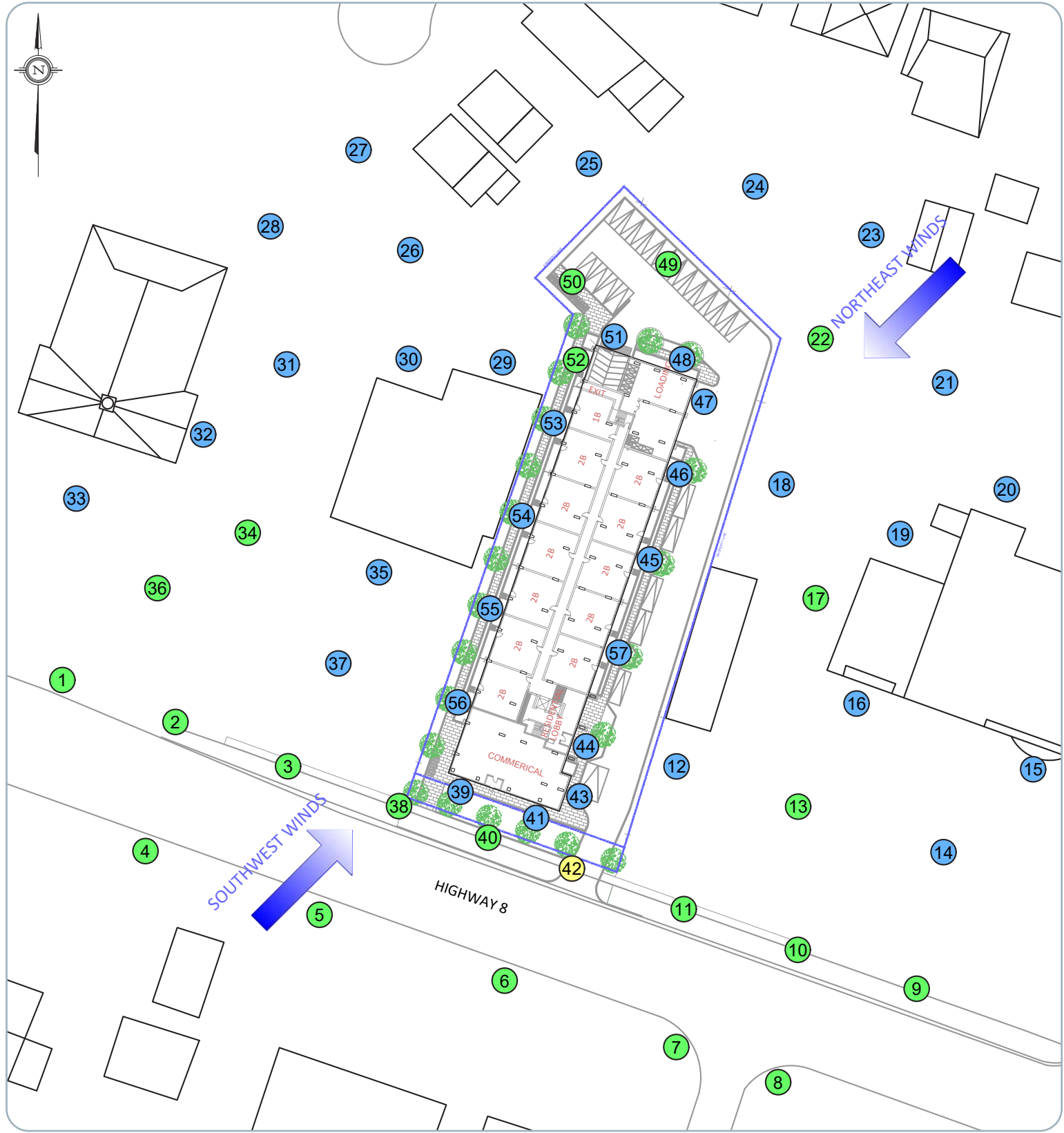
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06:04 PM

-  SUBJECT SITE BOUNDRY
-  PROPOSED DEVELOPMENT
-  SHADOWS CAST BY EXISTING BUILDINGS
-  SHADOWS CAST BY AS-OF-RIGHT BUILDING
-  SHADOWS CAST BY PROPOSED DEVELOPMENT

11.0 Pedestrian Level Wind Study - Summer

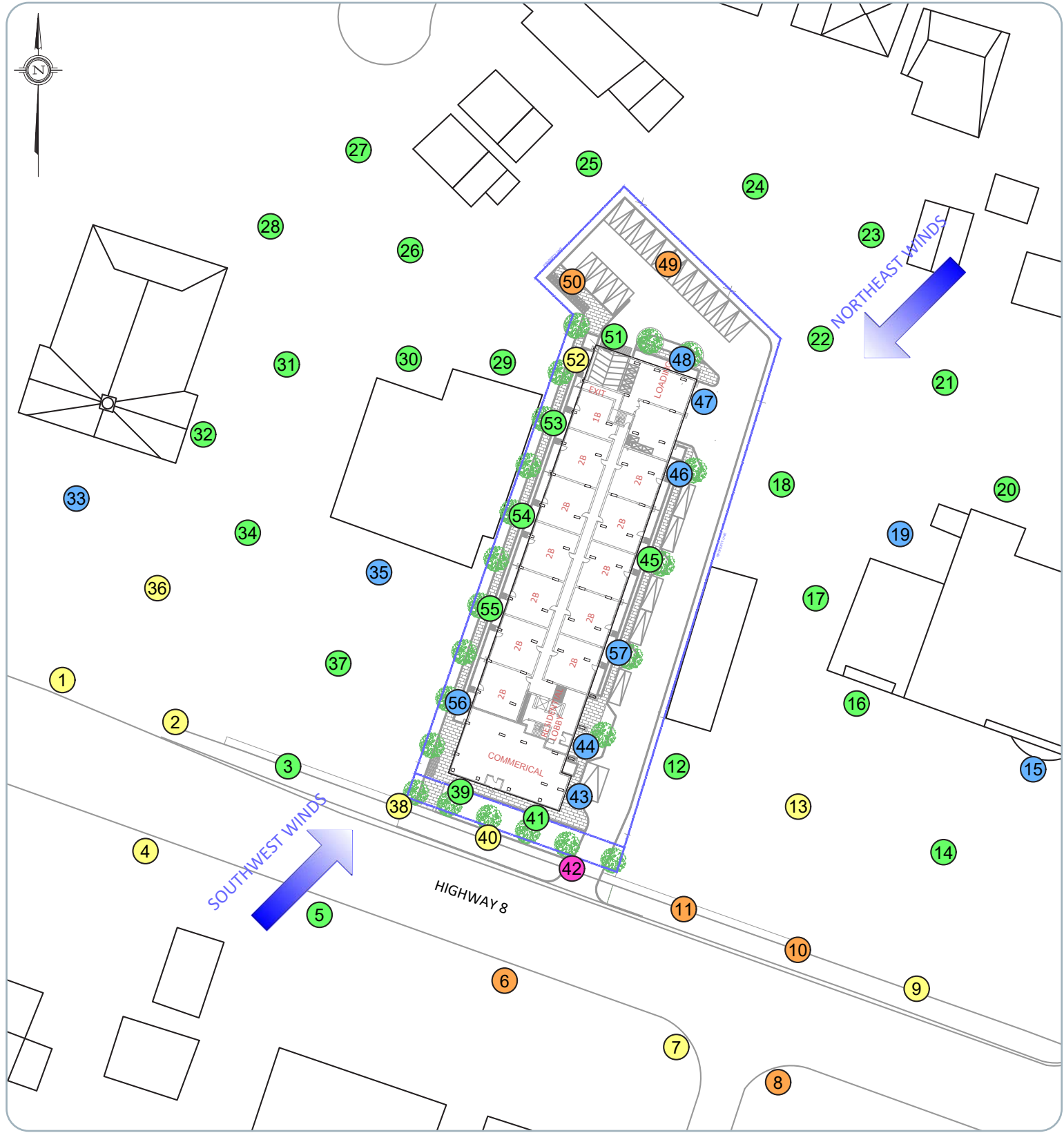


	#	SITTING
PREDICTED COMFORT CATEGORY	#	STANDING
	#	STROLLING
	#	WALKING
	#	UNCOMFORTABLE
WIND SAFETY CRITERION	#	ACCEPTABLE
	#	EXCEEDED

NOTES:

- SCALE IS APPROXIMATE.
- # PEDESTRIAN LEVEL WIND SENSOR LOCATION.

11.0 Pedestrian Level Wind Study - Winter



	#	SITTING
PREDICTED	#	STANDING
COMFORT	#	STROLLING
CATEGORY	#	WALKING
	#	UNCOMFORTABLE
WIND SAFETY	#	ACCEPTABLE
CRITERION	#	EXCEEDED

NOTES:

- SCALE IS APPROXIMATE.
- # PEDESTRIAN LEVEL WIND SENSOR LOCATION.

GRADIENTWIND
ENGINEERS & SCIENTISTS

A

Appendix A:
Planning and Urban Design Framework and Analysis

Urban Hamilton Official Plan and Western Development Area Secondary Plan

The subject site is designated District Commercial as illustrated on Schedule E-1 of the Urban Hamilton Official Plan (“UHOP”) and is located along a Secondary Corridor as per Schedule E – Urban Structure. An Official Plan Amendment application is proposed to redesignate the subject site to a Mixed Use - Medium Density designation and allow for a mixed-use building that includes retail and service commercial uses at grade.

As specified in Section E.2.4 of the UHOP, the City’s corridors provide a significant opportunity for creating vibrant pedestrian and transit-oriented places through investment in infrastructure, residential intensification, infill, and redevelopment with careful attention to urban design. Policy E.2.4.3 states that Urban Corridors shall be the location for a range of higher density land uses along the corridor, including mixed uses where feasible. Policy E.2.4.6 states that Urban Corridors shall function as commercial spines providing retail stores and commercial services that cater to the weekly and daily needs of residents within the surrounding neighbourhoods. The built form along the Urban Corridors shall generally consist of low to mid-rise forms but will vary along the length of the corridors with some areas permitted to accommodate high density and high rise built form (Policy E.2.4.10).

Policy E.2.12 identifies that Secondary Corridors are to evolve to an increasing proportion of multiple storey, mixed buildings in small cluster locations with at grade retail and service commercial uses. Policy E.2.4.16 speaks to the design of Urban Corridors and states that new development shall respect the existing built form of adjacent neighbourhoods where appropriate by providing a gradation in building height as well be located and designed to minimize the effects of shadowing and overview on properties in adjacent neighbourhoods.

The subject site falls within the Western Development Area Secondary Plan area where it is designated District Commercial. An Official Plan Amendment application is proposed to redesignate the subject site to a Mixed Use - Medium Density designation. The Western Development Area Secondary Plan defers to the Mixed Use – Medium Density Designation policies in Section 4.6 of the UHOP. The Mixed Use – Medium Density designation permits mixed-use developments between 2 and 6 storeys in height, with a discretionary additional height permission for up to 12 storeys which may be permitted provided the building includes a range and mix of unit sizes; incorporates sustainable building practices and materials; does not cause adverse shadowing on existing residential uses; progressively steps back from area designated Neighbourhoods; and, steps back from the street to minimize the height appearance from the street, where necessary (Policy E.4.6.8).

The proposed development conforms to the proposed Mixed Use Medium Density designation of the UHOP and Western Development Area Secondary Plan through the intensification of an underutilized site on a Secondary Corridor in a mid-rise built-form with a mix of residential and retail uses. The proposed built form satisfies the criteria for permitting height up to 12 storeys (the proposed building is 9-storeys) given the stepbacks along the rear of the site above the 6th level that will mitigate built form impacts on adjacent low-rise residential areas. Stepbacks are also provided along the Highway 8 façade above the 6th level to minimize the height appearance from the public realm. The ground floor retail space and residential lobby entrance located towards the front of the building will animate the streetscape from Highway 8 and provide retail space to serve the needs of the local community.

City of Hamilton Zoning By-law 05-200

The City of Hamilton Comprehensive Zoning By-law 05-200 zones the subject site Commercial District (C6) Zone. A Zoning By-law Amendment is required to rezone the subject site from the current C6 Zone to a modified Mixed use Medium Density (C5) Zone to facilitate the proposed development. The C5 zone provides specific zoning regulations pertaining to building setbacks, gross floor area, built form for new development, among others, and permits a maximum building height of 22 metres.

It is anticipated that the following modifications will be required to the C5 zoning permissions to permit the proposed development:

- Maximum building height of 28.50 m, whereas 22.0 m is permitted;
- Building Setback from a Street Line of 7.02 m (after road widening), whereas a maximum of 4.5 m is required; and,
- Section 10.5.3 a) iii) of Zoning By-law 05-200, which regulates the Building Setback from a Street Line for the portion of a building providing an access driveway to a garage, does not apply.

With respect to the maximum building height, the proposed Mixed Use Medium Density designation permits a height up to a maximum of 12-storeys, subject to meeting certain policy requirements. The proposed 9-storey built form satisfies the policy criteria for permitting heights up to 12 storeys given the stepbacks at the 6th level along the Highway 8 façade which minimize the height appearance from the public realm. Stepbacks provided at the rear of the property on the 6th level are provided to mitigate built form impacts on adjacent low-rise residential areas. The potential built form impacts (i.e., shadowing) have also been mitigated on adjacent lands designated Low Density Residential 2b (Western Development Area Secondary Plan) as per the shadow study, included in Section 10.0 of this presentation.

With respect to the Building Setback from a Street Line, the proposed building setback of 7.02 metres from the street line, after road widening, is requested to accommodate a landscaped treed area between the building and the street edge and allow for the inclusion of a walkway to connect the east and west portions of the building. The requested setback maintains the intent of the zoning regulation by bringing the building façade closer to the street edge and animating the public realm. The requested setback also provides a minimum travel distance for pedestrians to access the public sidewalk from the commercial and residential lobby entrances.

Regarding Section 10.5.3 a) iii), the proposed building includes an access driveway to a garage at the rear of the site to maximize the prominence of the commercial space along Highway 8 and animate the street edge. The proposed location of the access driveway allows for a more functional design of the building given the rectangular shape of the subject site and provides a separation between vehicular access points and areas where pedestrians will predominantly be located (commercial space and residential lobby area).

An application for a zoning by-law amendment will be submitted as part of a complete application submission.

City Wide Corridor Planning and Design Guidelines (2012)

The purpose of the City-Wide Corridor Planning Principles and Design Guidelines is to provide planning and design directions for Corridors in the City of Hamilton. As identified on Schedule E, the subject site is located along a Secondary Corridor. The subject site abuts Neighbourhoods to the north.

In relation to the guidelines, the proposed development will: facilitate intensification and infill development; create attractive, high quality, animated, and safe public streetscapes along Highway 8; minimize shadowing on adjacent properties and streets; encourage a diversity of built forms; and minimize the negative effects of overview on existing adjacent private properties through the application of an angular plane. A 6-storey streetwall height is proposed with stepbacks above the 6th storey to reduce the visual impact of the building's height on the streetscape. Parking and loading are proposed at the rear of the site, away from the building entrances. Ground floor commercial is proposed along Highway 8 to encourage an animated streetscape. A setback for the commercial entrance is proposed from the edge of the storeys above to provide for weather protection.

