Binbrook
Urban Design Guidelines

Parking located behind buildings

Pedestrian walkway to/from parking lot

Bump-outs at corners provide opportunities for pedestrian activity areas i.e. kiosks, public art etc.

4.0m wide planted median with tree and shrub planting

Primary building access oriented towards the street

Bike lane

26.2m R.O.W.

4.0   3.5

6.0

Binbrook Days August 14-21

bikes n gear

Hamilton

Table of Contents

Table of Contents

1.0 Introduction

1.1 Introduction

In December,1999, the former Township of Glanbrook approved *the Binbrook Village Secondary Plan (Amendment No. 27 to the Official Plan of the Township of Glanbrook)* for the purposes of outlining detailed policies to guide the future growth and development of the Binbrook Community.

A goal of the Binbrook Village Secondary Plan is to make the Binbrook Village Community Core the identifiable commercial centre of the village and a destination for the community and visitors.

To support the implementation of this document, the City of Hamilton's Long Range Planning and Design Division has developed Urban Design Guidelines for the Community Core as recommended in the *Binbrook Village Secondary Plan*. These Urban Design guidelines examine the design of the public realm, streetscape, and the built form and character of private development. Specifically they address the following elements:

- The character of the Binbrook Village in its historical and present day context,
- Key urban design principles,
- Heritage design,
- Streetscape design,
- Architectural design,
- Public space design,
- Community core design for the main intersection focusing on architecture and streetscape design,
- Gateway design at the entrances into the community core, and
- Parking design.

These Urban Design Guidelines were developed as a result of:
- a review of the Binbrook Village Secondary Plan,
- conducting an inventory and analysis of existing conditions in the community core,
- consultation with the general public and key stakeholders, and
- integrating the findings and recommendations of the Binbrook Village Transportation Master Plan Study.
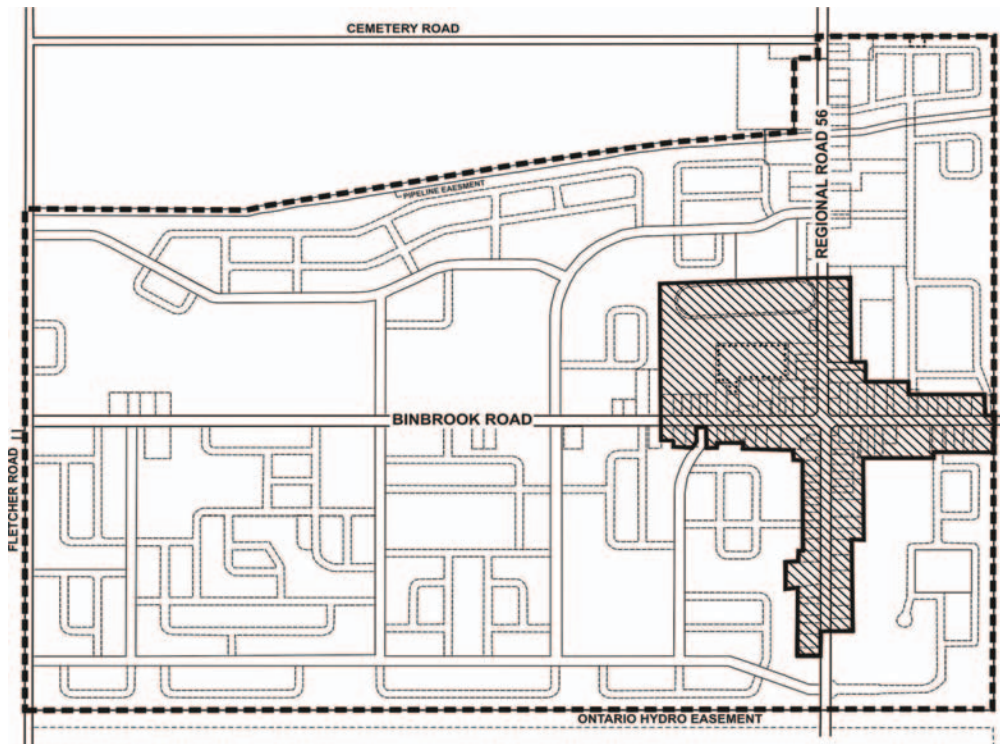
Concurrent with the development of the Urban Design guidelines, the City of Hamilton's Public Works Department conducted the Binbrook Village Transportation Master Plan Study. The purpose of the study was to review the anticipated transportation impacts and functional roadway requirements resulting from the implementation of the Binbrook Village Secondary Plan.

Introduction

The findings and recommendations of the study have been integrated with the Binbrook Village Community Core Urban Design Guidelines in order that traffic requirements and issues associated with increased traffic volumes will be addressed appropriately within the overall vision of the core.

1.2 The Public Process

The preparation of the Urban Design guidelines incorporated extensive public input and participation. Upon completion of a detailed inventory and analysis of existing conditions early in 2002, a Design Idea Workshop and a Public Open House were held in June and October of 2002 at The Binbrook Memorial Hall.

The Design Idea Workshop was facilitated by City staff and the transportation study consultants. It was conducted as a way of "brain storming" design strategies for the village core. Working within the framework of the Secondary Plan, sketches, opinions, and ideas from a cross-section of local residents and business owners were solicited, discussed and recorded. This information was integral to developing the concepts for the community core.

LEGEND

Binbrook Village Planning Area

Binbrook Village Community Core

Not To Scale

Binbrook Village Planning Area and Community Core

1.0 Introduction

At the October Open House, City staff presented a series of graphic panels that captured the findings from the June workshop and illustrated the design ideas that formed the basis of the Urban Design guidelines.

1.3 The Goal of the Urban Design Guidelines

The goal of the design guidelines and transportation master plan is to provide a functional blueprint for Binbrook's future growth. These documents are planning tools that will allow the City to achieve a vision for the core based on the Council approved *Binbrook Village Secondary Plan*.

By taking time to study the potential growth impact on traffic and the community core in general, strategies have been developed to maintain and enhance the village-like quality character of the Binbrook Community Core. The guidelines address many urban design issues which will potentially enhance the quality of life in the community.

While growth and development will be beneficial to Binbrook, it is important to preserve the Binbrook Village core's unique scale and atmosphere. This key

The Open Houses provided an opportunity for residents and stakeholders to participate in the future design of their community core.

principle in conjunction with appropriate urban design initiatives, including streetscape beautification, act as a catalyst for increased business activity further enhancing this community as an attractive place in which to live, work, and visit.

1.4 How Do the Guidelines Work?

As future public and private development proposals are initiated, each will be reviewed to determine suitability based on the application of the design principles in the guidelines. This approach to future development will ensure continuity and strengthen the functional and aesthetic qualities within the core.

2.0 Binbrook Village Character

2.1 Historical Context

Originally established as Hall's Corners, Binbrook Village originated during the 1850s as a busy agricultural service centre and crossroads settlement. The village - clustered around two intersecting dirt roads - featured churches, stores, a harness maker, taverns and a schoolhouse. Most were constructed of frame and clapboard. The Franklin Hotel, circa 1850, on the northwest corner survives to the present day.

During the latter quarter of the nineteenth century, Binbrook Village continued to grow and by the 1880's boasted a number of small agricultural industries including two blacksmiths, a wagon maker, saw mill, shingle factory and planing mill. The Binbrook Feed Mill (1890) survived as a rural industrial remnant until quite recently. Roads remained unpaved but plank sidewalks appeared and several stores featured verandahs providing shelter from snow, rain and sun.

The introduction of electricity and the automobile in the first three decades of the twentieth century resulted in dramatic changes in the village landscape. A concrete road, 8 feet wide, connecting Binbrook and Elfrida was constructed in 1923 and new residential construction; (large brick homes with hipped roofs known as "Four Squares"), began to transform the landscape.

Residential construction continued after World War Two with the appearance of "Veteran" or "Victory Houses" dotting the village landscape. Construction of brick homes continued into the 1950's. Public and private spaces also changed: dirt roads, plank sidewalks, grassed ditches, wooden fences and hedges gave way to harder, paved surfaces, curbs and gutters.
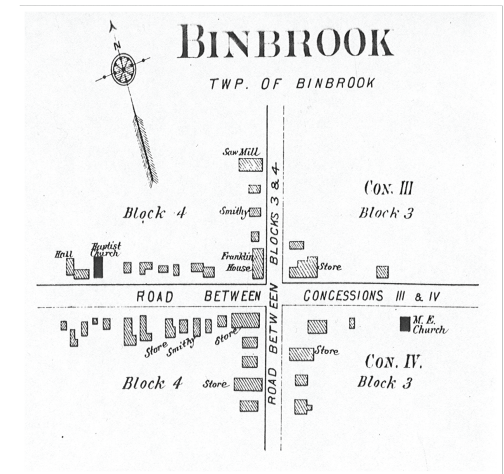
The village of Binbrook circa 1903. The intersection of Highway 56 and Binbrook Road continues to be the focal point of the community.

Binbrook Village Character

Early 1920's Edwardian Style home also known as "Four Squares"

"Veteran" or Victory housing constructed in Binbrook post World War Two

2.2 Present-Day Character

Today, Binbrook still retains some of its nineteenth- and early twentieth-century buildings but incremental growth in each of the four crossroads corners has changed the character of the settlement and much of the public landscape has been radically altered. Binbrook's low rise, twentieth century, residential development are the key determinants of its present-day character. These key defining elements are described in greater detail as follows.

The majority of the Binbrook crossroads settlement comprises residential structures (approximately 85%) with the remainder being divided equally amongst religious, public or institutional and commercial buildings. Of these approximately 15% were built prior to 1900, 44% between 1900 and 1950 and 44% after 1950.

Frame construction (77%) is the predominant construction type with synthetic cladding being the most common walling material. Wood, stone and stucco are used sparingly. Approximately 71% of Binbrook comprises buildings of one to one and half storeys and accounts for the settlement's very low profile. Many of these low structures are in the form of bungalows that are rectangular in plan and aligned parallel to the roadway (69%). Only a few buildings are set very close to the traveled portion of the sidewalk and roadway: 5% are located within 3 metres (10 feet), 25% within 3 to 6 metres (10-20 feet), 34% between 6 to 16 metres (20 to 50 feet) and 37% beyond 16 metres (50 feet).

Most buildings comprise three to four bays (65%) in the front façade and typically accommodate a single entranceway and separate windows. The majority of structures do not possess any decorative features (56%) with only a third (34%) having a porch. Roof forms vary considerably with side gable (34%), hip (26%) and cross/centre gable (25%) being the most common. Roofing is composed almost exclusively of asphalt singles.

In the property or area surrounding many of the residences over a third feature separate garage structures (36%) and a third (33%) have no garage at all. (Just under a third have garages incorporated into the main residential structure). Nearly all properties (96%) have a driveway that allows access to garages or that provides off-

2.0 Binbrook Village Character

street parking. Remaining lands within these properties are characterized by soft landscaping consisting of grass (93%) and ornamental tree species in excess of 100 mm caliper (65%).

Commercial and public buildings within the Community Core vary widely in the architectural style and building materials. Buildings that are located at the main intersection, such as the bank and food store, have setbacks ranging from 0-5.0m. Other commercial buildings such as the library, post office and doctor's office have a defined "front yard" green space and are currently set back approximately 20 to 30m. from the edge of the road.

The Public Library situated on a corner lot with lawn area and landscaping.

The Franklin Hotel currently the corner store with second storey housing units retains its original façade on Highway 56.

The Binbrook Memorial Hall constructed in 1920 is one of a few historic buildings of great prominence within the community core. Opportunities to incorporate this building into the Binbrook streetscape are numerous.
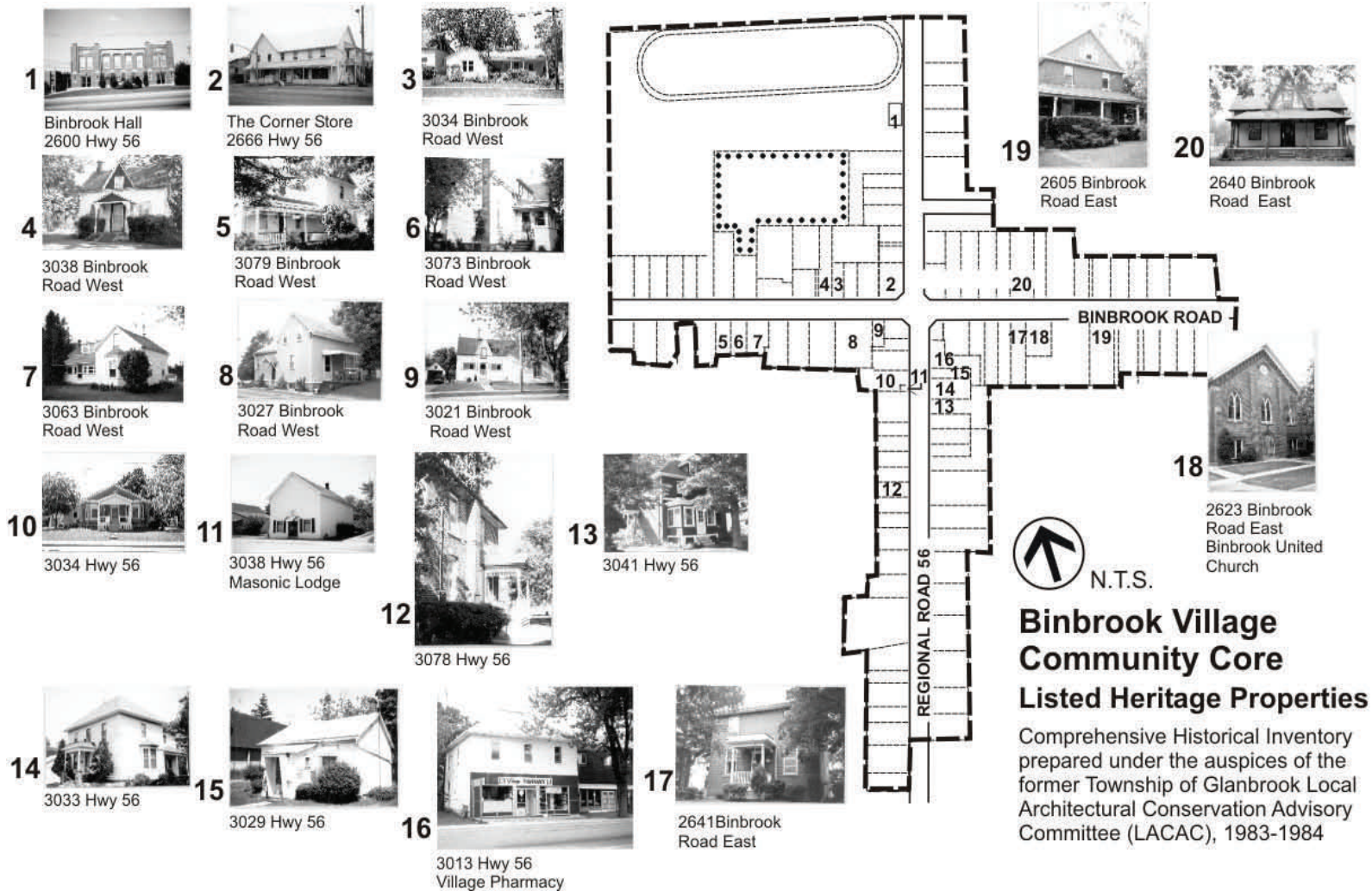

The food store and pharmacy located at the main intersection with primary parking lot behind the building.

Existing medical office with front lawn and side yard parking .

Binbrook Village Character

1 Binbrook Hall
2600 Hwy 56

2 The Corner Store
2666 Hwy 56

3 3034 Binbrook
Road West

4 3038 Binbrook
Road West

5 3079 Binbrook
Road West

6 3073 Binbrook
Road West

7 3063 Binbrook
Road West

8 3027 Binbrook
Road West

9 3021 Binbrook
Road West

10 3034 Hwy 56

11 3038 Hwy 56
Masonic Lodge

12 3078 Hwy 56

13 3041 Hwy 56

14 3033 Hwy 56

15 3029 Hwy 56

16 3013 Hwy 56
Village Pharmacy

17 2641 Binbrook
Road East

18 2623 Binbrook
Road East
Binbrook United
Church

19 2605 Binbrook
Road East

20 2640 Binbrook
Road East

BINBROOK ROAD

REGIONAL ROAD 56

N.T.S.

Binbrook Village Community Core

Listed Heritage Properties

Comprehensive Historical Inventory prepared under the auspices of the former Township of Glanbrook Local Architectural Conservation Advisory Committee (LACAC), 1983-1984

2.0 Binbrook Village Character

2.3 The Binbrook Streetscape

There has been little change in the Binbrook streetscape in recent years aside from some minor road improvements and the partial installation of sidewalks in the core. Today the streetscape in the community core is relatively undeveloped and pedestrian amenities such as street furniture, pedestrian scale lighting, public art are non-existent.

The topography in the community core is characteristically flat with no significant natural features in the core. This lack of topography permits long distance views through the core along Binbrook Road and Highway 56.

Pedestrian and vehicular circulation in the core is linear in nature due to the configuration of the two primary roads, Highway 56 and Binbrook Road. Both of these roads are two-lanes with limited on-street parking near the intersection. At the intersection these roads have curbed edges, narrow boulevards and subsurface drainage. Closer to the periphery of the core the existing road edge consists of a gravel shoulder and open grassed ditches characteristic of a rural environment.

Pedestrian sidewalks in the core are 1.2m in width. While sidewalks are in place near the intersection they are not continuous to the edge of the community core.

The location of vegetation in the public realm is sporadic in nature and is generally associated with the landscaping on individual lots. Boulevard street trees in the core are generally limited to a small area near the main intersection on Binbrook Road

There is little separation between the traffic and the sidewalk creating an uncomfortable and dangerous atmosphere for pedestrians. The 1.2m wide sidewalks in the core are generally undersized and lack any pedestrian amenities.

Highway 56 south of the main intersection. Note the gravel shoulder and open ditch.

Binbrook Road east of the intersection with concrete curbs and narrow grass boulevard.

3.0 Key Urban Design Principles

3.1 Key Urban Design Principles

In order to prepare urban design guidelines for the Community Core that address the objectives of the Secondary Plan the following tasks were completed:

- The Binbrook Village Secondary Plan was reviewed to determine implications for future development,
- An inventory and analysis of the existing conditions was conducted,
- A thorough and comprehensive public consultation process was undertaken, and
- The findings and recommendations of the transportation study were reviewed and incorporated where applicable.

The next step in the process was to synthesize this information and develop key urban design principles and strategies for their implementation. These principles form the foundation for the urban design guidelines, define the character of the Community Core and aid in the controlled development of the core.

The urban design guidelines are based specifically on the four following principles:

3.2 Greening of the Core

A goal of the *Binbrook Village Secondary Plan* is to make the Binbrook Village Community Core the identifiable commercial centre of the village and a desirable destination for the community and visitors. In order to achieve this it is important to provide a cohesive urban design theme that captures the core's uniqueness. One key characteristic that is enjoyed by the local community is the landscape treatment of individual lots and the opportunities for street trees and other landscape beautification treatments. This element of the streetscape can be enhanced and further developed through the implementation of a cohesive streetscape design. The final vision will include street tree planting and other soft landscape areas associated with both the public and private realm.

www.pedbikeimages.org/danburden

www.pedbikeimages.org/danburden

Examples of potential landscape treatments that contribute to the greening of the street.

www.pedbikeimages.org/danburden

www.pedbikeimages.org/danburden

Effective use of planted medians and street trees on the sidewalk create a desirable environment for pedestrians and motorists

3.3 Conserve and Enhance the Unique Character of the Core

One of the goals of the urban design guidelines is to provide an enhanced and integrated visual experience in the community core. To accomplish this it is important to guide development in a coordinated and well-designed manner. This can be achieved by promoting an attractive and safe streetscape design as well as giving careful consideration to individual building design, site layout and site landscaping.

These two major components of urban design: streetscape design and architecture and building design for both public and private properties are described in 3.3.1 and 3.3.2

3.3.1 Streetscape Design

Comfortable public spaces, including the sidewalk, offer pedestrians places to walk, sit, stand and gather. Amenities such as street lighting, benches, bike racks, and other accessibility features such as Urban Braille are essential. In addition to the appropriate design and placement of streetscape elements, the introduction of landscaped traffic calming features as well as attractive street-paving materials can all enhance the core's atmosphere.

3.3.2 Building Design and Site Layout

Conserving heritage structures
Heritage buildings and structures are maintained by ensuring any alterations, additions, and repair of these buildings are in keeping with the original character of the building and area.

Scale and design of buildings
Human scale design is essential in creating an intimate atmosphere for the village core. It is important that the height, mass, scale, and design of any new building is appropriate to the context of the street and adjacent structures.

Lot sizes
While it is the Secondary Plan's intention to retain the village-like quality of the community, it must be recognized that in order to accommodate future growth in businesses and mixed used functions, consolidation of lots may sometimes be necessary. The urban design guidelines will address issues of appropriate lot consolidation for new or redeveloped properties.

3.0 Key Urban Design Principles

Building setbacks

Building setbacks can help to create specific qualities in the Community Core by creating a sense of scale and enclosure through the location of the building edge which affects opportunities for "front yard" landscape treatments. In order to retain and enhance the village-like atmosphere, a hierarchy of setbacks for properties will be developed to reinforce the concept of front yard landscape or additional retail/commercial space.

The character of the new community core can be established by careful attention and consideration of the scale and design of new and renovated buildings.

3.4 Providing a Safe and Friendly Environment for Pedestrians

Good streetscape design not only functions at an aesthetic level. It also functions to help all pedestrians in the community to move safely on the streets. Understanding that there is planned future growth in village that will profoundly affect the core, the urban design guidelines provide design strategies that aim to retain the sense of intimacy, security and friendliness of a smaller community.

Elements such as traffic calming features, accessibility for the physically and visually impaired, better design for intersection and mid block pedestrian-crossings, clear direction, and well placed signage, are all important issues that the urban design guidelines will address.

www.pedbikeimages.org/danburden

www.pedbikeimages.org/danburden

These examples illustrate possible streetscape treatments that can be incorporated to create a safe and pleasant environment for users of the community core.

www.pedbikeimages.org/danburden

3.0 Key Urban Design Principles

3.5 Encourage Mixed Use Commercial Development

The Binbrook Village Secondary Plan recognizes that the Community Core will function as the primary retail and commercial centre of the village. Having a well-designed and well-used retail and mixed used Community Core is essential to a healthy and vibrant village. Existing commercial and service uses within the Community Core should be encouraged to remain in addition to providing opportunities for new developments in the core. By encouraging the grouping of permitted uses and development in a coordinated manner within the framework of urban design guidelines, balanced growth in the core can be achieved.

A variety of retail and commercial uses in the core will help to create a vibrant village centre. Site development must include a variety of opportunities for all users as illustrated in these examples.

www.pedbikeimages.org/danburden

Key Urban Design Principles

Key Urban Design Principles	Strategies for Implementation
1. Greening the Core	• Reduce the impact of asphalt parking lots and driveways by consolidating driveways, and encourage parking at the rear • Create a hierarchy of setbacks to encourage green space and additional pedestrian space • Provide streetscape landscaping to include street trees in boulevards and planters • Create a hierarchy of well landscaped public spaces • Provide opportunities for landscape treatment in the roadway medians
2. Conserve and Enhance the Unique Character of the Core	**Streetscape:** • Maintain a sense of pedestrian scale through design and selection of construction material • Include pedestrian Amenities: Bike Racks, Street Furniture, Lighting, Landscape • Promote accessibility for all user groups, e.g. City of Hamilton Urban Braille standards • Select construction materials for ease of use, maintenance and design appropriateness • Create visual harmony between all street elements **Architecture/ Building:** • Maintain lot sizes that are conducive to a pedestrian scale, small town character • Design and scale of buildings (maximum 3 stories) in keeping with the village character • Define a hierarchy of setbacks to create scale, landscaped areas and useable pedestrian space • Design elements and construction material to be compatible and reinforce the core's character • Ensure that existing Heritage structures are properly conserved
3. Provide a safe and friendly environment for pedestrians	• Incorporate traffic calming features to create a safer and more pedestrian friendly community core • Ensure that streetscape design to enhances pedestrian comfort and convenience • Provide accessibility for the physically impaired • Create better design for intersections and mid block pedestrian crossings • Create direct visual and physical access to buildings along the street • Provide parking at the rear of buildings with combined access for multiple lots • Promote clear direction through well placed and well designed signage
4. Encourage Mixed Use Commercial Development	• Develop a comprehensive zoning by-law to ensure balanced growth in the community core. • Provide opportunities for mixed-use functions such as businesses, cultural centres, community services, and residential units to ensure a vibrant community core

3.0 Key Urban Design Principles

The character of the Binbrook Village streetscape as envisioned in the Secondary Plan and defined in the Urban Design Guidelines is based on the key principles of :

- *Greening the core,*
- *Conserving the unique character of the core,*
- *Providing a safe and friendly environment for pedestrians, and*
- *Encouraging mixed use commercial development*

LEGEND

1. Well defined pedestrian access to rear yard parking areas. Opportunities for entry features, seating, lighting, etc. and landscaping to create an inviting entrance with a strong street presence.
2. New or redeveloped commercial buildings with street oriented facades, entrances and landscape treatments.
3. Shared driveway access with off-street parking located behind buildings. Entrance to parking to be well defined with signage and appropriate landscape treatments.
4. Elements such as lighting and street furniture are to be integrated into the Binbrook Village streetscape. The addition of flower planters and banners provide an opportunity to enhance the character of the community core.
5. Provide opportunities for outdoor retail or restaurant space at grade or on second or third floors

4.0 Heritage Design Guidelines

4.1 Introduction

Binbrook Village has witnessed considerable change over the past two hundred years. Just as change has occurred in the past, change will obviously occur in the future. The intent in guiding and managing future change is to try and ensure that alterations and additions do not detrimentally affect the overall character of the Binbrook Village Community Core and its component building stock.

Within the core are a variety of heritage buildings as well as more recent structures that are not considered to be of heritage interest. Accordingly, this section provides guidance on a number of topics relating to building changes, notably:

- alterations to heritage buildings and structures (Section 4.2);
- additions to heritage buildings and structures (Section 4.3); and
- conversion to mixed use commercial (Section 4.4)

4.2 Alterations to buildings and sites: Guiding principles

Alterations to heritage buildings may be pursued for a number of reasons. Residing facades, insulating wall surfaces, cutting new window openings, enlarging entrances have much to do with providing a fresh look to a building or improving interior comfort levels. Where alterations are necessary every attempt should be made to make changes on the least visible facades, in discreet locations, away from the street presence of the building.

In all cases the following guiding principles should be consulted:

- Historical, architectural and landscape features and building materials should be maintained and enhanced;
- Any proposed change to a heritage property should be based upon a clear understanding of the particular problem with the building or site. Wherever possible proposed alterations should be based on a sound knowledge of the building type, materials and form;
- Contemplated work should attempt to be limited avoiding wherever possible extremes of over enthusiasm, replacing too much; cleaning too well; or adding inappropriate historic detailing;
- "Quick fix", "maintenance free" and "magic remedies" should be avoided as they may be simply ineffectual or at worst may be capable of causing irreparable damage to a significant building.

4.2.1 Features and spaces around heritage buildings and sites

Features and spaces around heritage buildings and sites are important in providing the context or setting of a heritage property and enhancing the presence of the building in the streetscape. Accordingly, every effort should be made to:

- Maintain traditional views of the property by avoiding the masking or hiding of prominent building features. Ensure that front lawns, tree plantings, hedges and fences are given adequate care;
- Keep parking areas and outbuildings including garages and utilities such as heat pumps and satellite dishes to the side or rear in those areas traditionally designated for domestic activities and structures;
- Continue to use historic means of access: drives, paths and doorways. Encourage required new entrances to be installed on secondary elevations, generally those at the side or rear. Where external staircases are proposed they should be located at the rear;

- Maintain proper site drainage in any work so that water does not collect or drain towards foundations.
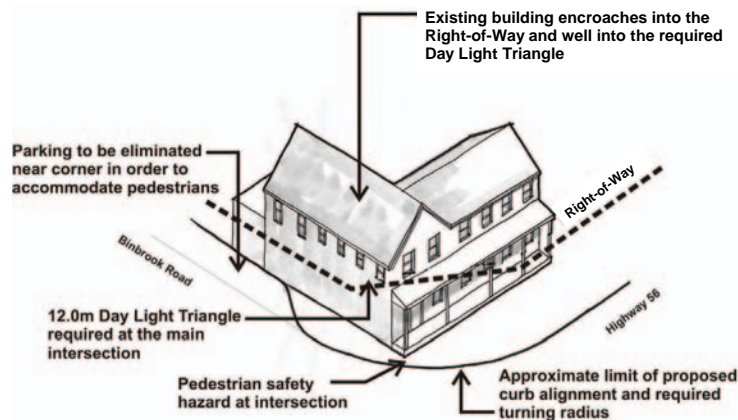
4.2.2 Heritage building fabric

The following general principles provide the context for any work that potentially involves work on heritage building materials:

- Attempt to repair rather than replace;
- Base all designs for replacement or restoration of former features on dependable documentary evidence and try to avoid guessing the size, shape and materials of former features;
- When undertaking repairs, replacement or restoration, use the same materials as the original. New or repair work should not confuse the historic character of an area by creating an impression of greater age or of a different region or even country;
- Signs of age or irregularities found in older work and materials should be respected and should not be covered up or obscured.

- Maintain the symmetry or other important features of architectural design, particularly on the main elevation(s);
- Avoid moving heritage structures either into or out of the village core.
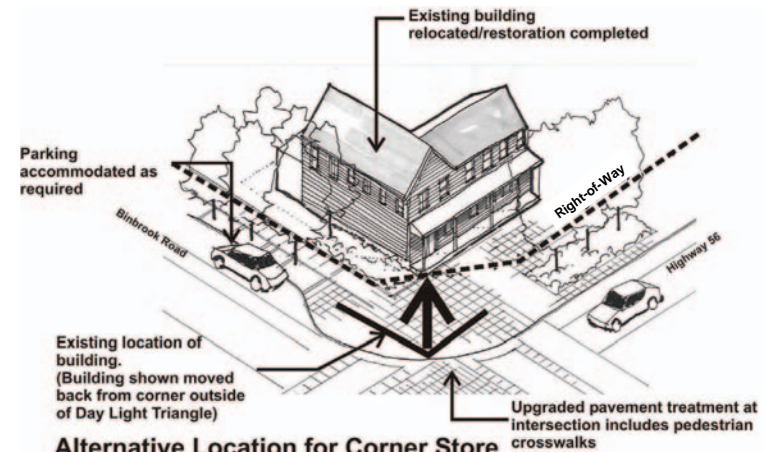
4.0 Heritage Design Guidelines

Existing building encroaches into the Right-of-Way and well into the required Day Light Triangle

Parking to be eliminated near corner in order to accommodate pedestrians

Binbrook Road

Right-of-Way

Highway 56

12.0m Day Light Triangle required at the main intersection

Pedestrian safety hazard at intersection

Approximate limit of proposed curb alignment and required turning radius

Retaining Corner Store At Existing Location (Franklin Hotel Circa 1850)

Issues:

- Building encroaches into the Day Light Triangle obstructing views for vehicles
- Pedestrian safety is compromised due to space limitations at corner
- Opportunities for streetscaping are limited

The corner store (Franklin Hotel) is a well known landmark building in Binbrook and an excellent example of the existing heritage fabric. The integration of the existing building would provide a significant opportunity to retain an important part of Binbrook's history, maintain a village character in the core and set a precedent for future building design. However, due to existing deficiencies in traffic operations compounded by the planned growth in traffic, an alternative location for the building is required .
Efforts to conserve rather than replace should be made where feasible as illustrated.

Existing building relocated/restoration completed

Parking accommodated as required

Binbrook Road

Right-of-Way

Highway 56

Existing location of building. (Building shown moved back from corner outside of Day Light Triangle)

Upgraded pavement treatment at intersection includes pedestrian crosswalks

Alternative Location for Corner Store (Franklin Hotel Circa 1850)

Benefits of Relocation:

- Significant heritage resource maintained in the Community core and at prominent corner site
- Vehicle sightlines are greatly improved due to the relocation of building outside of the Day Light Triangle
- Pedestrian safety issues are addressed by providing adequate room at corner as a result of increased sidewalk space
- Opportunities for streetscaping (tree planting, street furniture, lighting etc.)

The Franklin Hotel: Conservation Alternatives

4.2.3 Roofs

- Decorative roof features and original roofing materials should be retained and conserved wherever possible;
- Ensure that vents, skylights and other new roof elements are sympathetic in material and that they are discretely placed out of general view from the street and public rights-of-way;
- Original roof types should be respected when constructing additions to heritage buildings. Flat roofs will not be permitted in the core.

4.2.4 Foundations and walls

- Protect original wall surfaces from cleaning methods that may permanently alter or damage the appearance of the surface or give a radically new look to the building, for example, sandblasting, strong liquid chemical solutions and high pressure water cleaning;
- Attempt to avoid application of new surfaces or new coatings that alter the appearance of the original material, especially where they are substitutes for repairs. This may include the application of waterproof and water repellent coatings, paint, aluminum or vinyl siding and stucco.

4.2.5 Windows

- Protect and maintain original window openings as well as their distinguishing features such as materials, frame, surrounds, shutters, sash, muntins and glazing;
- Modifications to the size or shape of window openings, removal of muntins, installation of snap-in muntins, replacement with sealed units or covering of trim with metal or other material is discouraged;
- Avoid removing or blocking up windows that are important to the architectural character of the building;
- New windows should be installed sensitively, in an area that is inconspicuous. New window design that is compatible with the original in terms of proportions, rhythm and scale is encouraged; however, the new should not attempt to replicate the original in terms of historical details;

4.2.6 Entrances

- Protect and maintain entrances, verandahs and porches notably on principal elevations where they are often key elements in defining the character of the building. Retain the historic means of pathway and/or stairway access and conserve important features such as doors, glazing, lighting, steps, balustrades and door surrounds;
- Restoration of a missing verandah or porch should be based wherever possible upon accurate research using both pictorial and physical evidence;
- Where documentation does not exist, the design and construction of a contemporary entrance, verandah or porch sympathetic and compatible with the character of the building is preferred over a conjectural design of the original;
- Where new entrances are required, they should be installed on secondary elevations.

Heritage Design Guidelines

4.3 Additions to heritage buildings and sites

While alterations are usually undertaken to improve comfort levels in an existing house (more light or heat for example) additions are usually undertaken to provide needed living space, such as additional bedrooms for growing families, separate kitchens, and adaptive re-uses e.g. from residential to commercial. Accordingly the following guidelines provide advice on how best to add desired space to an existing heritage structure.

4.3.1 Location

- Exterior additions, including garages, balconies and greenhouses are encouraged to be located at the rear or on an inconspicuous side of the building, limited in size and scale to complement the existing building and neighbouring properties. Additions at the rear should always be slightly lower than the existing roof line and stepped in at the sides in order not to overpower or dominate the existing heritage building and the view from the street. Additions so constructed will also tend to be more compatible with adjoining property owners;

- Multi-storey exterior additions are best set back as deeply as possible from the existing front wall plane in order to be unobtrusive in the streetscape and differentiate the addition from the older structure;

- Additions to structures with symmetrical facades should avoid creating asymmetrical arrangements (imbalance) in building form.

The addition to the Binbrook Memorial Hall is an uncomfortable fit both in its location and height. While an effort was made to match the existing brick type and colour, the forward placement of this addition detracts from the grandness of the existing building. Following the advice in these guidelines may assist in selecting better locations and building forms.
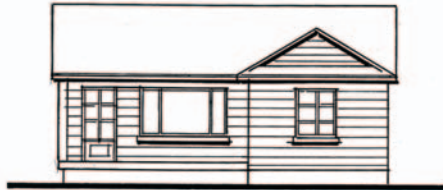
Original Building
Typical one storey sided bungalow with a gable roof.

Original Building With Appropriate Addition
Appropriate scale and massing of addition. Treatment of roof style/material, windows and siding also respecting the original character of the building.

Addition not Appropriate
Scale and massing of addition overshadows the original building. Inappropriate use of siding, window types and roof style further detract from the character of the original building.

Another example of an addition located at the rear of an existing building that adheres to the principles of additions to heritage buildings as outlined in these guidelines. Additions to buildings such as these will provide opportunities for commercial redevelopment of existing residential dwellings in the community core.
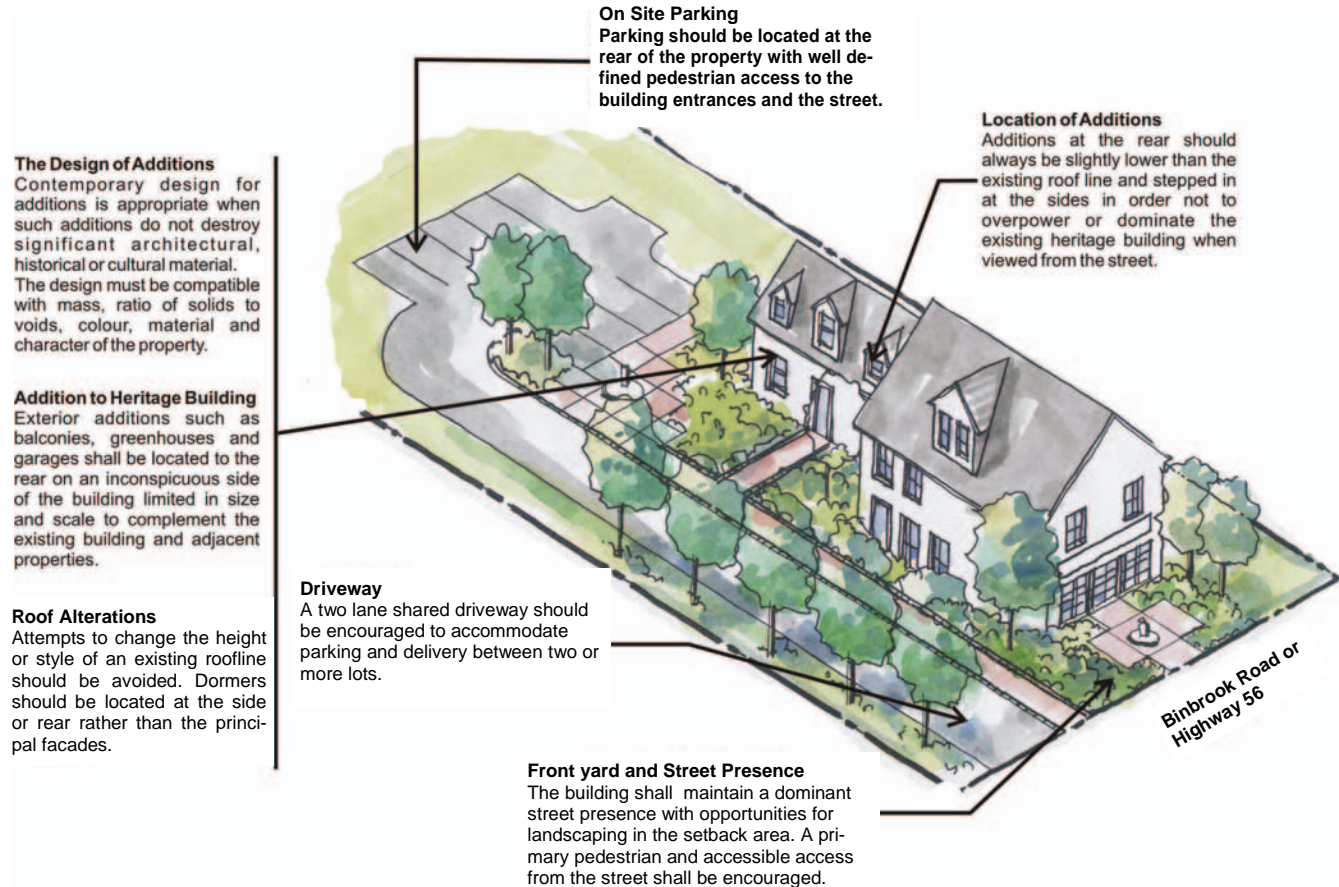
The addition onto the back of this building has recognized the form and scale of the original structure keeping the character intact. This successful example illustrates the principles of scale, location and design of additions to heritage buildings.

This addition to this building for conversion to a dental office retains the majority of the original character. Due to site limitations the location of this addition was added to the front of the existing building. While this may not be desirable it does recognize the use of similar materials, colour and detailing. As a result the character of the street has been maintained.

Heritage Design Guidelines

On Site Parking
Parking should be located at the rear of the property with well defined pedestrian access to the building entrances and the street.

Location of Additions
Additions at the rear should always be slightly lower than the existing roof line and stepped in at the sides in order not to overpower or dominate the existing heritage building when viewed from the street.

The Design of Additions
Contemporary design for additions is appropriate when such additions do not destroy significant architectural, historical or cultural material. The design must be compatible with mass, ratio of solids to voids, colour, material and character of the property.

Addition to Heritage Building
Exterior additions such as balconies, greenhouses and garages shall be located to the rear on an inconspicuous side of the building limited in size and scale to complement the existing building and adjacent properties.

Roof Alterations
Attempts to change the height or style of an existing roofline should be avoided. Dormers should be located at the side or rear rather than the principal facades.

Driveway
A two lane shared driveway should be encouraged to accommodate parking and delivery between two or more lots.

Front yard and Street Presence
The building shall maintain a dominant street presence with opportunities for landscaping in the setback area. A primary pedestrian and accessible access from the street shall be encouraged.

Binbrook Road or Highway 56

The placement and design of additions to existing buildings will be critical in order to maintain and enhance the character of the community core.

4.4 Conversion to Mixed-Use Commercial

With the current and future growth of the village, it is planned that the Community Core will continue to evolve as a viable commercial centre. It is possible that many of the existing residential buildings may be adapted for commercial uses. Where this adaptive re-use of an existing structure is contemplated, guidelines for commercial applications will be required. Some of the key principles of commercial design will involve:

- Window area for retail display
- Signage and lighting
- Building entry
- Visual clarity
- Architectural detailing
- Appropriate landscape treatment
- General accessibility
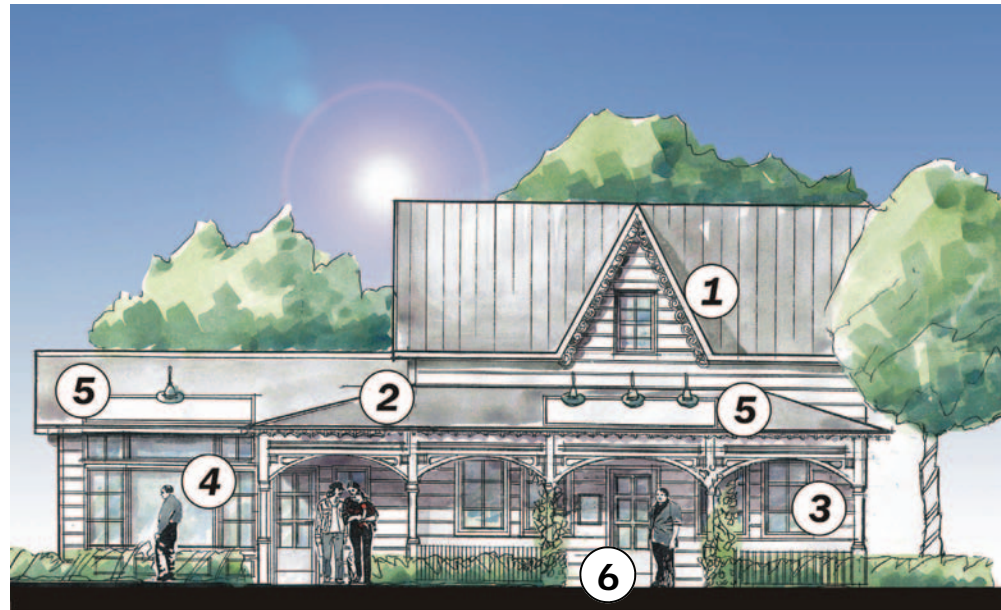
Legend

1. Maintain original roof elements and decorations
2. Maintain heritage elements on original porch. Porch additions should respect scale, material and architectural detailing of the original
3. Renovations of heritage buildings should consider vernacular, vertically proportioned windows
4. Street elevations should be accentuated through cornice lines, distinctive storefront design, awnings and exterior lighting.
5. Signage with an artistic flair or heritage orientation will enhance and enliven the heritage buildings and clearly identify commercial establishment.
6. At grade (or ramped) accessibility to accommodate all users

4.0 Heritage Design Guidelines

The successful conversion of a heritage building into a mixed use residential/commercial use. The storefront design respects the character of the existing building and greatly enhances the streetscape.

Incorporation of a "front yard" outdoor patio space provides a complementary use for this restaurant in a renovated heritage building.

Storefront design and renovations must also consider physical access. While the brick wall and steps may be aesthetically pleasing, as illustrated in this example, the lack of an at-grade access is unacceptable .

5.0 Streetscape Design Guidelines

5.1 The Binbrook Streetscape

Design Objectives

The intent of these streetscape design guidelines is to create a vibrant and prosperous village Community Core.

The implementation of these guidelines will create landscaped streets with interesting architecture and public spaces, streets that are interesting and visually stimulating, streets that have a balance between vehicle traffic, parking and pedestrians, and streets that support a wide variety of commercial and retail activity.

Typically, the street can be considered the largest system of publicly accessible open space in a community. A shared-use space that is essential to pedestrian activity, the street supports a variety of commercial and retail uses and accommodates vehicular traffic. The streets of the Community Core must be designed to provide a full range of pedestrian amenities and pro-

vide opportunities for spontaneous social interaction.

Two important objectives drive the streetscape design of the Community Core. These are:

- Developing an appropriate theme that will reinforce the Community Core as a unique place within the Village of Binbrook. The emphasis of this vision will maintain the existing rural character, and
- Defining elements within the public right-of-way, i.e. street furniture, sidewalks, landscaping, signage, lighting, etc., that would support and embellish the character of the Community Core.

Within the Community Core area of Binbrook the proposed road right-of-way will be 26.2 metres. The configuration of the road will consist of two travel lanes 3.5m wide with an adjacent bike lane of 1.6m wide. A centre turn lane / centre median of 4.0m width will also be incorporated.

The remaining space will be dedicated to on-street parking and landscaped boulevards.

Parking located behind buildings
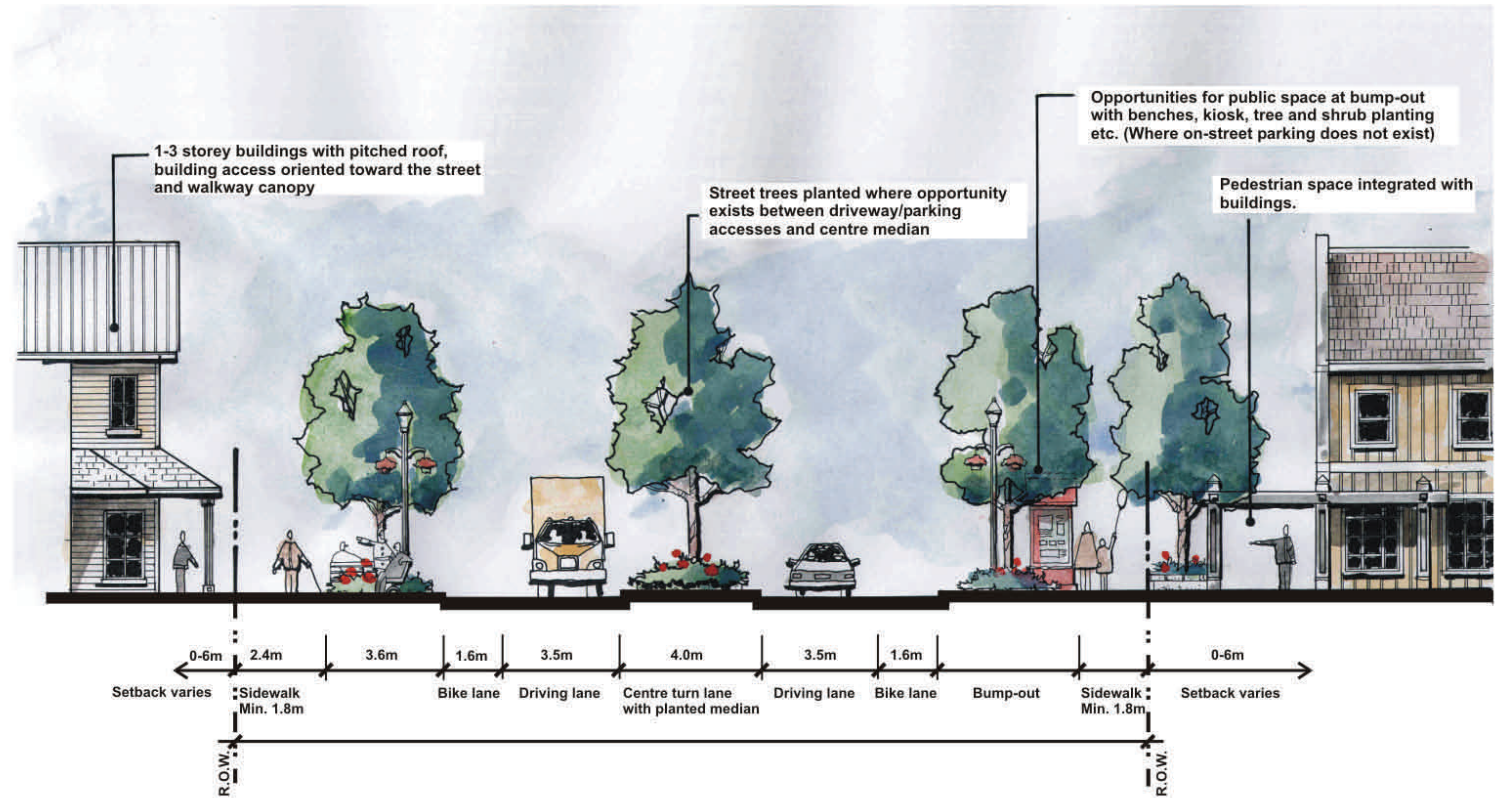
Landscaped areas between building and sidewalk

Pedestrian walkway to/from parking lot

Landscaped pedestrian access to parking area

Bump-outs at corners provide opportunities for pedestrian activity areas i.e. kiosks, public art etc.

Bike lane

4.0m wide planted median with street trees and shrub planting

26.2m R.O.W.

Primary building access oriented towards the street

Bike lane

Vehicular and pedestrian access to consolidated parking lot behind retail/commercial

Boulevards provide opportunity for landscape treatments and street furniture

Binbrook Village Community Core: The Secondary Plan Design Intent

The Binbrook Village Streetscape as envisioned in the Secondary Plan places emphasis on convenience and comfort for pedestrians, efficient traffic movement and parking, opportunities for streetscaping, and an overall aesthetic quality characteristic of a village setting. The above drawing illustrates how a good village core can be designed.

5.0 Streetscape Design Guidelines

1-3 storey buildings with pitched roof, building access oriented toward the street and walkway canopy

Street trees planted where opportunity exists between driveway/parking accesses and centre median

Opportunities for public space at bump-out with benches, kiosk, tree and shrub planting etc. (Where on-street parking does not exist)

Pedestrian space integrated with buildings.

| 0-6m | 2.4m | 3.6m | 1.6m | 3.5m | 4.0m | 3.5m | 1.6m | | 0-6m |
| Setback varies | Sidewalk Min. 1.8m | | Bike lane | Driving lane | Centre turn lane with planted median | Driving lane | Bike lane | Bump-out | Sidewalk Min. 1.8m | Setback varies |

R.O.W.   R.O.W.

Proposed Street Section: No On-Street Parking

In areas of the street where on-street parking is not accommodated, the boulevards provide an opportunity to provide expanded pedestrian space. This additional boulevard width can accommodate formalized sitting areas or activity nodes. It may also provide opportunities for landscaped planters or street trees and opportunities for the placement of public art.
Extensive use of street trees and landscaping will effectively add an aesthetic quality and further emphasize the small town rural character of the Binbrook Village Community Core.

Front yard landscape treatment adjacent to the public sidewalk may include additional retail/patio space

Street trees in median where opportunity exists between driveway/parking accesses

Street trees, landscaping and utilities in boulevard

Proposed building or existing building with modifications

0-6m	3.6m	2.4m	1.6m	3.5m	4.0m	3.5m	1.6m	2.4m	3.6m	0-6m
Setback varies	Boulevard	Parking	Bike lane	Driving lane	Centre turn lane / planted median	Driving lane	Bike lane	Parking	Boulevard	Setback varies

R.O.W.

R.O.W.

Street Section: With On-Street Parking

The boulevards narrow to 3.6m in areas with on-street parking . The typical boulevard in these locations will include a minimum 1.8m wide sidewalk , landscaping, street trees and other pedestrian amenities. Integration of the private space in the setback area adjacent to the sidewalk can provide additional pedestrian space and opportunities for street related activities such as commercial and retail uses.

5.2 Sidewalks and Pedestrian Circulation

Design Objectives

The sidewalk is the unifying element that knits the Community Core together and provides pedestrian linkages to the adjacent neighbourhoods. Sidewalks in the community core are the principle place of pedestrian activity including movement and social interaction. They can be designed to create street life and vitality within the core.

The sidewalk in the Community Core is also designed to accommodate many of the streetscape elements that are found in the public right-of-way. These include street trees, landscaping, lights, street furniture, kiosks, etc. For this reason the sidewalk must be designed with a set of minimum standards. These standards ensure that the sidewalk will function not only to ensure safe, direct and convenient access to buildings or parking lots but will also create a strong sense of visual connectivity within the core and help to make the core enjoyable to experience.

Bike Lane

On-street parking

Pedestrian zone with enhanced pavement treatment to accommodate benches, trash receptacles, bike racks, street lights etc.

1.8-2.4m wide sidewalk with Urban Braille standards

Right of Way

Access to retail/commercial
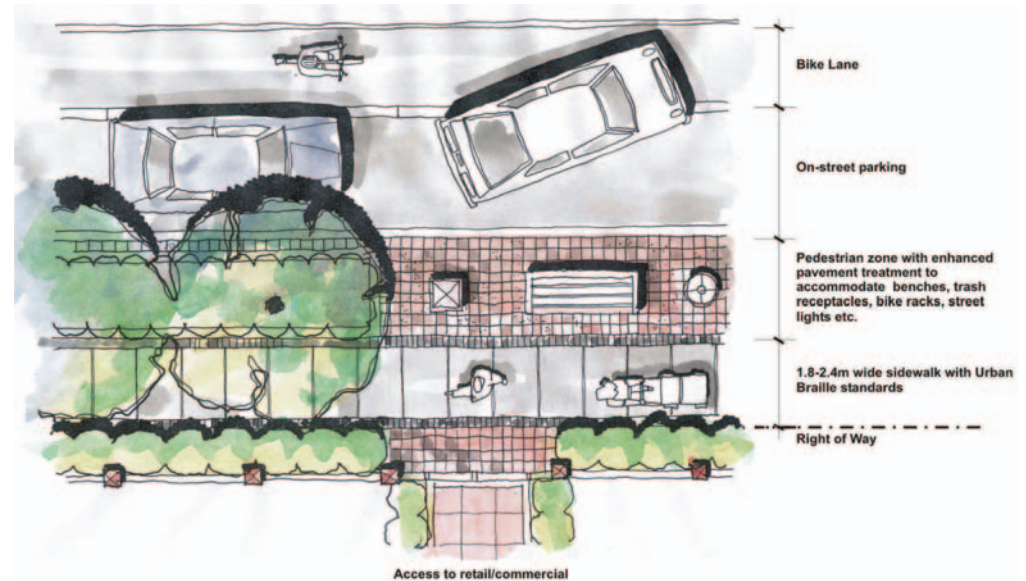
Sidewalk Design Guideline Illustration

The sidewalk in the boulevard is designed to provide comfort, convenience and safety for the pedestrian.

Design Guidelines

The following guidelines apply for side-walks within the community core:

- Sidewalks on Highway 56 and Bin-brook Road should be in the range of 1.8-2.4m in width, (6'-8') or greater to accommodate larger volumes of pedestrians, increased activity and pedestrian amenities such as street furniture and lights;
- A 1.5m (5') wide planted buffer and a 0.6m paved maintenance strip will provide the necessary separation from vehicular traffic;
- Sidewalk surface material must permit ease of use with no obstructions and allow for maintenance, i.e., snow removal and regular cleaning;
- The City of Hamilton Urban Braille standards are to be incorporated;
- Sidewalks should be extended through an intersection to create well defined crosswalks;
- Sidewalks that provide a connection to adjacent neighbourhoods should be constructed to a minimum width of 1.5m (5').
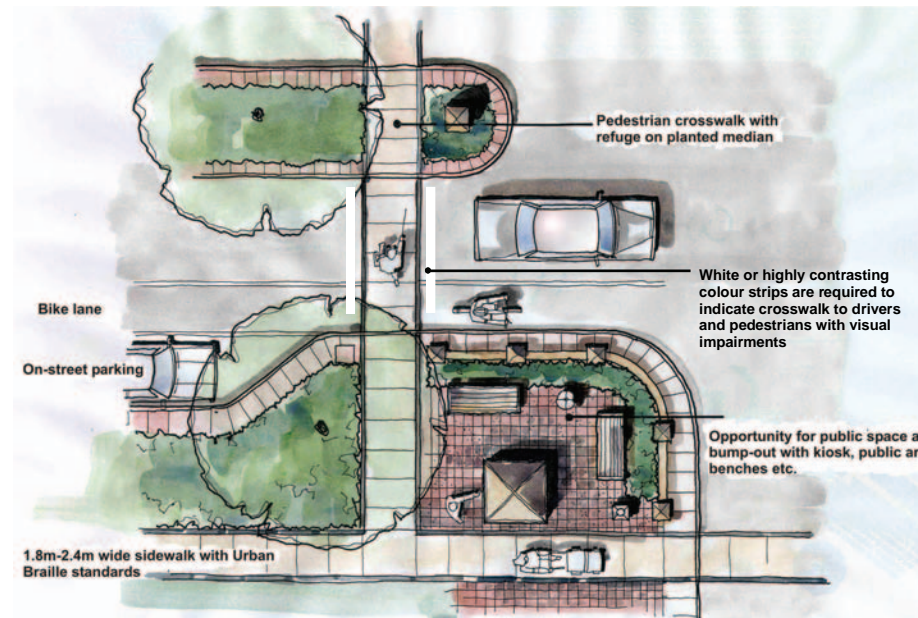
Bump Out Design Guideline Illustration

Plan of a corner bump out including a crosswalk and planted median . These bump outs may be conveniently located at a driveway access or side street. Where warranted they may also incorporate crosswalks as illustrated.

Pedestrian Node at "Bump Out"

A pedestrian gathering area or "bump out" along street can be designed as a sitting area to accommodate a wide variety of street furniture, tree and shrub planting and public art. These bump-outs will be strategically located to provide additional pedestrian space and opportunities for enhanced street crossings.

5.3 Main Intersection

The Community Core of the village is centred at the intersection of Binbrook Road and Highway 56. It is the intent of the Secondary Plan to develop the intersection as a prominent focal point for the community. Opportunities to create a positive image through building design, landscape treatments, and public open space enhancement are significant due to the exposure that the four corners provide.

As an additional focus for the core, a village square is to be created on the north east corner of the intersection. A village square at this prominent location will help create an intimate, pedestrian friendly environment and define a character for the community core.

This major intersection will also be designed to accommodate large volumes of vehicular, bicycle and pedestrian traffic with an equal level of importance placed on all of the user groups.

5.4 Building Design

Building design and architectural detailing at the main intersection is an important element in the development of the community core. The following principles are to be adhered to in the design of buildings at this location:

- Buildings located at the intersection should be set close to the lot line;
- Buildings should be designed with prominent focal features that will contribute to the character of the village. The use of vertical elements such as clock towers can provide visual interest;
- Buildings should be angled or setback at corners to ensure that adequate sight lines are maintained;
- Primary access to buildings should be highly visible to motorists and pedestrians and should be located at the corners;
- The building mass on the long sides of a corner building is to be articulated into a series of 7-10m modules of retail space along the street face in order to create a building that is sympathetic to the pedestrian scale. This modulation is to extend to the upper floors;
- Corner buildings are to be constructed to a maximum of 3 stories in height.

5.0 Streetscape Design Guidelines

Photograph courtesy of Micheal Ronkin, ODOT

A good example of contrasting colours used to enhance visibility for both the motorist and the pedestrian.

Legend

Aerial view illustrating key urban design principles at the main intersection.

1) 0– Setback
2) Maximum 3 storey building height
3) Corner buildings with landmark features
4) Colonnade
5) Space for outdoor patios or retail display
6) Parking located to the side and rear of the lot and building
7) Landscaped entrances
8) Village square
9) Upgraded road surface pavement treatment including pedestrian crosswalks with highly contrasting colours for visibility for approaching motorists and for visually-impaired pedestrians
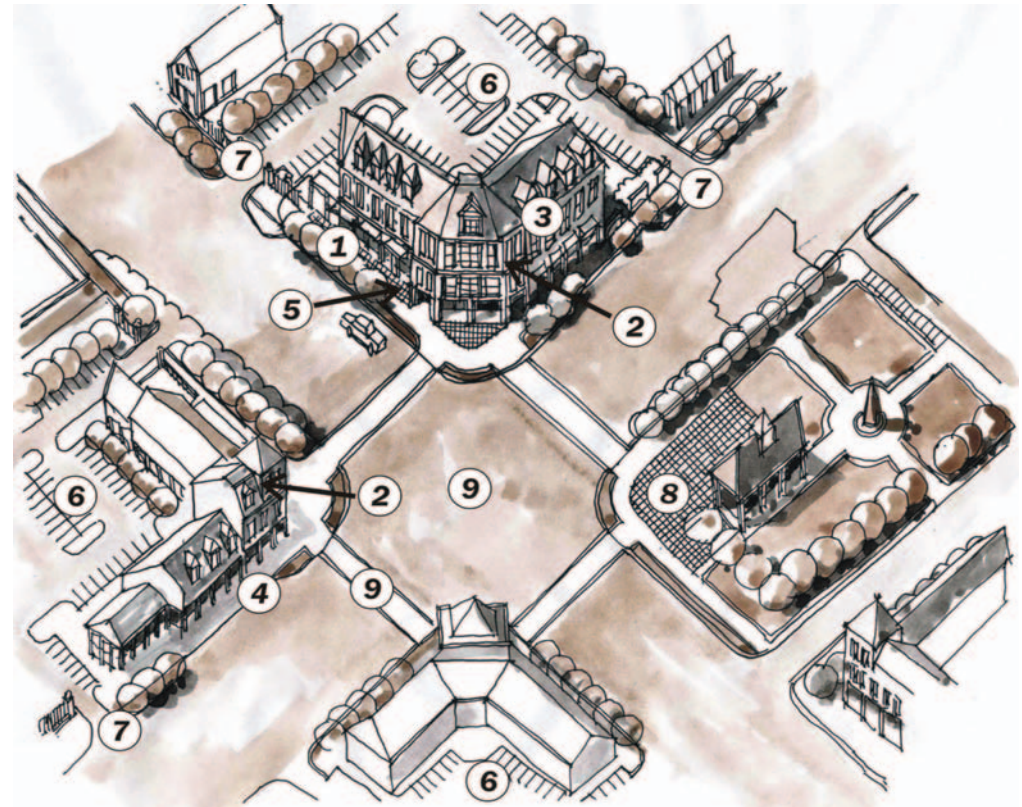
A conceptual alternative development at the main intersection creating a focal point for the community.

Streetscape Design Guidelines

Buildings at the main intersection should be designed with prominent architectural features and emphasis on pedestrian access. Commercial requirements such as outdoor patios and retail space can also be integrated with the streetscape treatments.

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5.0 Streetscape Design Guidelines

5.5 Intersection Landscape Treatments

One of the objectives of the urban design guidelines is to create a pedestrian friendly environment. The following guidelines for the treatment of the intersection will help to integrate the future built form, create a functional and aesthetic community core and provide a safe environment for pedestrians.

- Pedestrian crossings should be constructed of a contrasting material including highly contrasting colour to provide high visibility for both motorists and pedestrians;
- Pedestrian crossings should be integrally designed with the rest of the intersection;
- Additional paving treatments should be considered for the infill of the intersection in order to further emphasize the main intersection as the focal point of the core;
- Curb extensions or "bump outs" should be provided at the four corners of the intersection as a means of reducing pedestrian travel distance across the intersection, providing additional sidewalk space, providing additional opportunities for streetscape treatments and to slow traffic;
- Street trees and raised planters should be considered where they will not interfere with pedestrian movement or obstruct sightlines for both motorists and pedestrians.

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The streetscape design at the main intersection must consider pedestrian circulation and amenity space. Streetscape elements such as crosswalk pavement treatments not only increase the aesthetic qualities but serve as traffic calming features helping to heighten the motorist's awareness of pedestrians.

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A well defined intersection can be created through the use of different paving materials of highly contrasting colour

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A bump out can provide an opportunity for a street crossing. The street narrowing and landscape treatments also act as a traffic calming element encouraging vehicles to slow down.

5.6 Street Trees

Design Objectives

Street trees are essential for creating beauty and improving the quality of life in an urban setting. The benefits include: providing shade and cooling effects, providing a sense of enclosure, providing definition and scale to the street, protection from wind, separation from automobile traffic, and reducing airborne dust and pollutants.

Many opportunities exist for street tree planting in the Community Core of Binbrook. The most favorable locations are:

- Within the sidewalk to enhance the streetscape;
- In traffic islands to reinforce traffic calming;
- In parking areas to provide shade for vehicles and to break up expanses of asphalt;
- In public spaces;
- Adjacent to buildings, and
- At focal areas such as sign features or gateways.

Existing trees within the public right-of-way should be examined for growth characteristics, the health of the tree and suitability of the location. Every effort should be made to protect mature and suitable specimens.

Design Guidelines

Considerations for the selection and location of street trees include:

- The ultimate growth, height and spread of the tree canopy in order to allow for unrestricted growth;
- The density of the canopy, ultimate shape and branching pattern. Trees will provide varying degrees of shade due to branching habits and size and type of leaf;
- Access to adequate sunlight and rain water. Considerations of other microclimatic limitations such as reflected light sources from surface pavements and buildings and increased wind and snow load caused by building design should be considered when locating trees either on the street or as part of a site plan development;
- The location of existing and proposed utilities to allow uninhibited growth without disturbance to the tree crown or root zone;
- Maintenance requirements of street trees including watering, fertilizing, pruning and repair of damage caused naturally and by humans, i.e. removal of bark and broken branches;
- Height of the tree canopy to protect sightlines along the street for both motorists and pedestrians.

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- The entrances to buildings and other significant architectural features to ensure that the trees do not interfere with physical or visual access to the buildings;
- The ability to withstand harsh conditions. Trees located in sidewalks or other hard surface areas will require adequate tree pits and surface grates that allow for air, water and regular maintenance.

5.7 Landscaping

Design Objectives

The addition of trees, shrubs and flowers can greatly enhance the quality of the core and add interest to a building and the streetscape. When plant material is used in conjunction with fences, walls, arbours, trellises or other landscape elements, enhanced levels of visual interest are achieved.

Design Guidelines

- Landscaping can be used to provide accents at key focal points such as intersections, entrances into buildings, outdoor patios, public art etc. Shrubs, annuals and perennials can be accommodated in raised or at-grade planting beds as well as in planters or hanging baskets;
- Trees, shrubs and vines can be used to soften blank walls, screen views or provide a buffer between incompatible activities;
- Existing landscaping should be maintained and incorporated into new site development where the planting is in a healthy growing condition;
- Landscaping can be used in conjunction with low walls and fences to screen at grade parking lots;
- When preparing a landscape design consider the character of the existing building and overall context of the site. The landscape should blend and be compatible with the adjacent properties to ensure consistency and compatibility;
- The proposed landscape should be at an appropriate scale for the development;
- The selected hard landscape materials should be compatible with the existing building or neighbourhood character, e.g. pavement, retaining walls, bollards, etc;
- Landscaping should not interfere sightlines for drivers and pedestrians at intersections, crossings and driveway entrances.

An inviting entrance and courtyard can be created through well designed landscape treatments and the effective use of space.

5.8 Fencing, Walls and Screens

Design Objectives

Fencing in the community core should be governed by the same level of standards that dictate all other aspects of urban design within the public right-of-way. Fencing may be used for protection of the private and public domain, to influence pedestrian circulation, to protect against hazard, provide definition to a property and to screen views. A hierarchy of fence types should be established in order to fulfill these different functions and to create visual consistency within the core.

Design Guidelines

- In order to maintain consistency, fencing that is constructed within the same view shed as the building should be compatible in terms of design, height, material and colour;
- Wood fence construction must be robust to prevent sagging and leaning. The sizing of the individual members including posts and beams is also critical in order to ensure longevity;
- Where fences are located in visually prominent areas they should be constructed from upgraded fencing material such as cedar or wrought iron;
- Where pressure treated wood is used it should be painted. The colour of painted fences should be compatible with architectural and other landscape treatments;
- Long expanses of fences should be interrupted with aesthetically designed columns and plant material where possible;
- Column and gate design should reflect the architectural character of the adjacent building to visually tie the site together;
- The use of chain-link fence should be limited to rear yard use. Chain-link fence should be vinyl coated with consistency in the colour throughout the core.
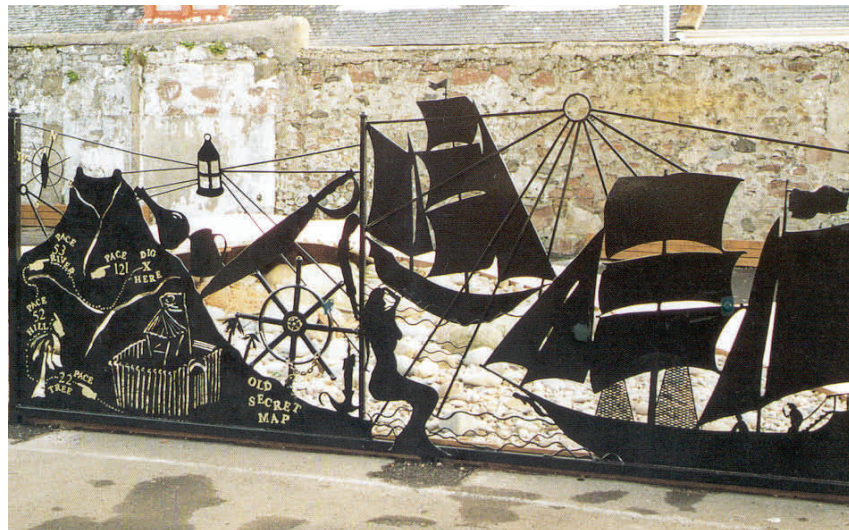
Where appropriate, fences and screen can be designed as key features in the landscape. Every effort should be made to construct aesthetic and durable fencing in the community core.

5.9 Street Furniture

Design Objectives

Providing comfort and convenience for pedestrians is key to creating a vibrant atmosphere in the Community Core. The placement and selection of street furniture will ensure a comfortable environment for the pedestrians and a visual unity for all areas of the core. Street furnishings should be chosen to adapt to the context of the streetscape. For example, this may include groupings of benches to encourage social interaction at public spaces versus singularly placed benches to provide a necessary rest but for shorter periods of time.

Ideally, street furniture should be clustered to provide amenity nodes for pedestrians. Adequate space should be allocated to accommodate the various types of street furniture and anticipated volumes of users.

Street furniture in the Community Core includes:

Benches	Banners
Kiosks	Flower baskets
Trash receptacles	Planters
Bicycle racks	
Water fountains	
Tree guards	

The design of street furnishings can help to reinforce a particular character of the community core. Furnishings as shown in the photographs below are examples that have been used as features to help define the character of a commercial area.
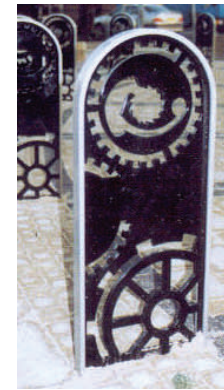
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Streetscape Design Guidelines 5.0

Design Guidelines

- Street furniture should be selected for ease of maintenance, durability and to complement a particular theme and design style in the core;
- Styles of street furniture may be selected for unique locations or areas. However in situations like this, there should be some common thread in the design such as style, form, colour, and material to maintain continuity within the overall context of the community core;
- The placement of street furniture should be looked at within the context of the entire streetscape in order to maximize the effectiveness of use and to complement pedestrian circulation patterns;
- In general, benches should be placed on the sidewalk to provide rest areas along the street. The benches should be situated out of the line of major pedestrian circulation patterns in order to avoid any potential conflicts;
- Street furniture may be located at key nodes where pedestrians are intended to gather. This may be at the forecourt of a building or a pedestrian node on the sidewalk where space permits such as a

bump out location, in public spaces for informal or spontaneous uses and programmed uses;
- Street furniture should be oriented to encourage conversation and social interaction;
- Kiosks should be located where there are major volumes of pedestrian activities such as the main intersection, primary public spaces or at bump-out areas on the street. Kiosks are useful for the display of community events, providing direction and limited advertising;
- A continuous clear pedestrian path

Convenience for the pedestrian are critical in creating a pleasant and well used commercial core. A comfortable bench can provide much desired rest stop for a wide variety of users in the core. Even planter walls can provide necessary short-term seating .

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of a minimum 1.5m must be maintained on sidewalks for pedestrians including those with visual or mobility impairments. This clear zone must be free of all obstructions including, overhangs, moveable planters, portable signs, etc.;
- Bicycle parking rack style and placement should meet current guidelines such as APBP's (Association of Pedestrian Bicycle Professionals) bicycle parking guidelines.

5.0 Streetscape Design Guidelines

5.10 Street lights

Design Objectives

Quality lighting is required to encourage use of the community core at night and to provide a safe environment for pedestrians and vehicular traffic. There are several key factors in selecting light fixtures and preparing a lighting design for the core area. These include:

1. Maintaining a consistent level of illumination to provide a safe environment for pedestrians and vehicles;

2. The type of light source, i.e. metal halide, high pressure sodium, etc.;

3. The durability and construction of the light fixture;

4. Maintenance and replacement costs of the light fixture; and

5. Selecting a lighting style that enhances the character of the street.

In addition to functional uses, lighting is also used in a wide variety of aesthetic applications. Lighting for accents and special effects provide another level of interest to the streetscape of the community core.

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There are many different styles of lighting to choose from. It is important light standards selected for use in the core are aesthetically pleasing, durable and provide quality illumination for pedestrians and motorists.

Design Guidelines

- Pedestrian scale lighting should be incorporated in the commercial core and designed with a luminare height of 4.5-6 metres in height;

- The selection of the light pole and luminare should reflect the design theme in the Community Core and complement street furniture and architectural styles;

- Lights that prevent glare and adhere to the "dark sky" concept, i.e. don't light upwards, should be selected;

- Maintain a consistent type of light source and spacing within the Community Core, i.e. select all Metal Halide lights;

- Encourage the use of low-level pedestrian lighting such as bollards to accent pathways and entrances into buildings;
- Encourage the use of accent lighting to illuminate building facades or special features. Other accent lighting should be encouraged to create special effects such as silhouettes on building walls or spot lights to highlight architectural features, street trees or other landscape features;
- Maintain adequate lighting in parking lots including the pedestrian access to adjacent buildings;
- Locate light standards to avoid any unnecessary clutter within the sidewalk;
- When locating light fixtures consider the ambient lighting from adjacent buildings and the effect that any existing mature street trees may have in the light distribution pattern;
- Eliminte any "dark spots" caused by inadequate spacing of light fixtures. Minimum lighting levels should be achieved in order to maintain a consistent foot candle level.

5.11 Signage

Design Objectives

With the anticipated commercial growth in the Community Core, guidelines for signage design and placement will be important in order to avoid the visual clutter that is caused by diverse types and applications of signs. The emphasis of the sign guidelines are to create consistency in design, placement, and use of materials.

Another aspect of the signage design guidelines is to define a hierarchy of sign types that can be incorporated into a variety of locations while reflecting and maintaining a village character.

Diversity in the design of signs is encouraged. Signs should be constructed of quality materials and be compatible with the character of the building and the surrounding context.

Design Guidelines

- A uniform sign system is to be developed to mark all parking areas, street names and public buildings;
- Encourage signs that are integrated into the architecture of a new building;
- Exterior lit signs are favoured over backlit metal box signs;

- Commercial signage should provide identification and advertising without being obtrusive;
- Signage should be treated as an integral part of the building or storefront, not as a separate entity;
- Signage must be compatible in size, style, colour, shape and use of material with the size of the building and visual character of Binbrook Core;
- In the Village Core, it is encourged that signage has an artistic flair to contribute to the special character of the place;
- Excessive amounts of information or inappropriate colour combinations should be avoided as not to interfere with the legibility of the sign;
- Signs must be visually coherent, legible, and with good graphic design related to the store and commercial activity;
- Corporate logos should have limited scale and size in order not to overpower a building;
- A sign uniformity plan should be prepared for multiple unit commercial developments. Such a plan will establish the framework for visually coherent and compatible signage across the development.

5.0 Streetscape Design Guidelines

Signs should be located in a consistent sign band that is incorporated into the design of the façade of the building;

- Large pedestal mounted, pylon signs, billboards and mobile signs in the core shall not be allowed. Portable signs shall not be allowed in the Community Core area;
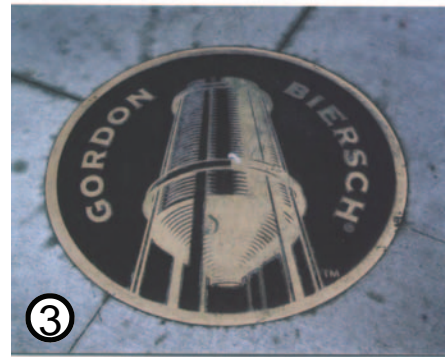
There are many effective and aesthetic ways of incorporating commercial signs in the community core. The following are a few examples:

1. *Colourful and simple lawn mounted externally lit sign*
2. *Use of sculpture in addition to building signage*
3. *Ground mounted marker effectively placed at the entrance to a craft brewery/dining establishment,*
4. *Eye-catching and creative design of a sign mounting bracket*

5.12 Public Art

Design Objectives

Public art is noticeably absent from the existing streetscape of Binbrook. It is recognized that the addition of public art can add to the visual quality of a streetscape, create interest, provide a focal point, help a community celebrate its history, reinforce a theme and provide cultural enrichment.

Public art can be integrated into buildings, streetscapes, landscape elements, or stand alone as three dimensional art. Public art applications include;

- Murals on blank building walls
- Embedded into the ground plane
- Street furniture, lighting,
- Free standing sculpture (variety of materials can be used)
- Traditional sculptures

These guidelines are intended to promote the inclusion of public art into new public and private development initiatives and provide opportunities for significant pieces of public art at key locations within the street.

Design Guidelines

- Public art should be incorporated into key open spaces with considerations for visual access and establishing prominence for the art work;

- Property owners are encouraged to provide outdoor public art to enrich the community core user's experience and create a more cultural atmosphere;

Opportunities for public art in the community core are limitless as illustrated in these examples.

- Public art competitions are encouraged to help foster a greater sense of well being and community pride;
- When locating public art, consideration must be given to its context within the site to ensure that the piece of art is well integrated into the surrounding landscape.

5.13 Pavement

Design Objectives

The pavement treatment of ground surfaces is often overlooked when new commercial developments and streetscapes are planned. There are many aesthetic and functional benefits in providing alternate paving materials such as interlocking paving stones, patterned concrete or exposed aggregate

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Alternative paving treatments can dramatically improve the visual appearance of the core and can be useful in providing functional benefits such as demarcating pedestrian crosswalks. Paving treatments can also be used as a form of public art to create interest through imprinting patterns onto concrete.

Streetscape Design Guidelines

Pedestrian walkways can be made inviting through the use of paving material such a concrete unit paving stones.

Design Guidelines

- Consider upgraded paving materials such as precast concrete paving stones when designing focal areas such as public spaces and pedestrian nodes along the sidewalk;
- Paving treatments should be consistent throughout the core in order to maintain a sense of continuity;
- Alternate paving treatments can be used to distinguish pedestrian

travel areas from vehicular travel areas;

- Sidewalk paving can be extended across streets to help to visually define the pedestrian crossing zone, improve the visibility for both the motorist and the pedestrian and to provide a traffic calming element;
- The City of Hamilton Urban Braille system should be included as an integral component of public realm sidewalk reconstruction in the Community Core. High contrast in colour and suitable widths of contrasting treatments must be provided to ensure visibility of crossings by approaching motorists and by visually impaired pedestrians.

5.14 Utilities

Design Objectives

Above ground utilities are a common element found within the context of the streetscape. These utilities include hydro poles and lines, transformers, telecommunication pedestals, traffic signal control panels etc. Often these utilities are located with little or no regard for the overall aesthetic appearance of the street. While access and maintenance

issues are important and not to be overlooked, special design considerations must be taken into account in order to minimize their cumulative negative visual impact on the street.

Design Guidelines

- Whenever possible, utilities should be located to the side or back of a building where they will be out of view from the street;
- Utilities shall be screened from view through the use of planting, fencing or architectural screening. The design of the screen or fence should be complementary to the building material and existing fences or screens if located elsewhere on the property;
- Ensure that maintenance personnel can access the utility without obstructing the movement of pedestrians or vehicles to other parts of the street or site.

6.0 Architectural Design Guidelines

6.1 Design Principles

The following key architectural design principles for residential, mixed use and commercial properties will guide new development within the Binbrook Village:

6.1.1 Balance

In architectural design, balance is a principle that brings a special quality to the building's overall appearance. In the Binbrook Village some of existing buildings employ symmetry as the simplest and most common way of creating balance.

AVOID

6.1.2 Proportion

In architecture, proportion is the relationship between one portion (dimension) of the building to another. All aspects of a building's design can be referred to as a ratio between width and height of the building or elements of the building. Good proportions of individual buildings within the Binbrook Village will help create a consistent quality of the entire streetscape.

AVOID

6.1.3 Scale

Scale is a measure of the size of the building or architectural element in relationship to the dimensions of the average person. In the Binbrook Village Community Core, human scale plays an important role in the creation of sense of place and a small town character.
The intent is to:
- avoid sharp contrasts in scale between adjacent buildings which are visually disruptive, and;
- create a similarity of scale that in turn will create a continuity in the streetscape and reinforce a small

AVOID

6.2 Architectural Design Guidelines

The following architectural design guidelines are based on the understanding that new infill development and additions to existing buildings create opportunities to upgrade existing built forms and create a visually attractive commercial Village Core. These guidelines will establish common understanding of desired architectural quality and assist developers, individuals and architects in achieving that quality over time.

Above, an existing residence prior to design intervention aimed at conversion to a commercial use.

Legend

1. Front gable enhances building presence on the street and contributes to the traditional Binbrook Village character.
2. Use of stone as an accent material, not main façade material.
3. Use of siding to contribute to the traditional architectural character.
4. Use of vertically oriented and aligned traditional windows and muntin bars.

5. For commercial space provide sufficient storefront window display area.
6. Use of porch as a unifying element for commercial use on the ground floor.
7. A two storey porch will provide a colonnade and sheltered ramp access to the commercial space on the ground floor (in buildings where the ground floor is raised).
8. Incorporate an access ramp to the building in a fitting manner
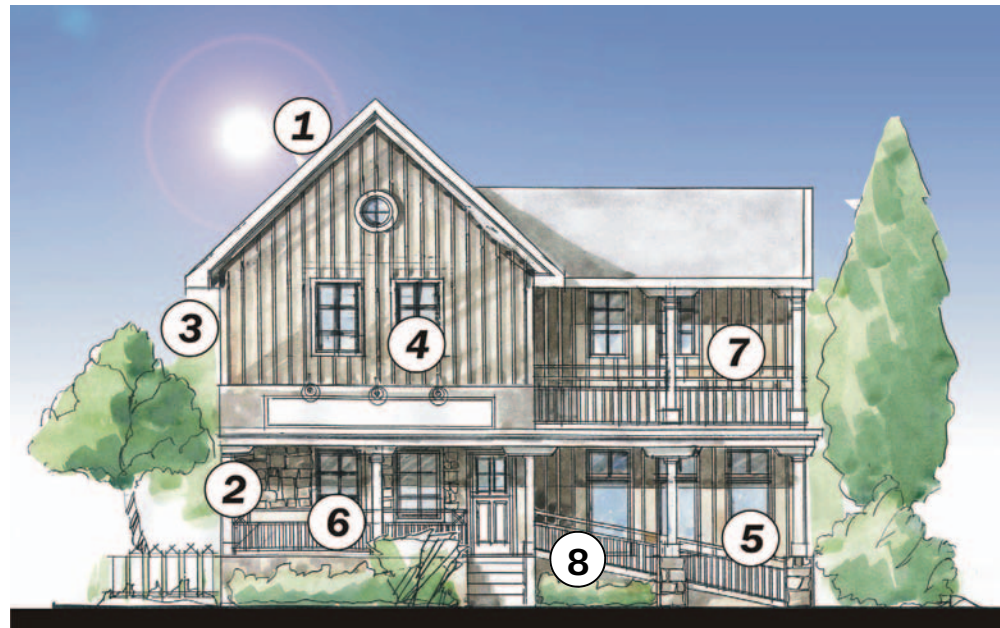
6.0 Architectural Design Guidelines

6.3 Guidelines for changes and additions to buildings

The Secondary Plan encourages the conversion of existing residential buildings into new retail/commercial uses. For those buildings, the following design guidelines may apply:

Access & Entrances

All retail space must be easily accessible to the general public. There may be a need to create new ramps or steps in order to accommodate that need and these should be incorporated into new front porches or colonnades.

Storefronts

Conversions will create a need for new storefronts that are designed for optimum retailing benefits and are aesthetically pleasing.

Signage

Conversions will create a need for new commercial signage. Signage location, type and design must be carefully considered in order to maximize retailing opportunities and to provide visual enhancement in the core.

6.3.1 Additions

Additions to existing buildings (non-heritage) are permitted if additions will be in part (ground floor) or in whole used as retail/commercial space allowed in the Zoning by-law. Additions to non-heritage buildings will be guided as follows:

Addition Location

Additions to existing non-heritage buildings may be located at the front, side or rear of the existing property if space allows. Front additions are desirable for retail uses, and should conform to new setback guidelines and zoning requirements in the Village Core.

Additions-scale and Massing

Additions to existing non heritage buildings should carefully blend with the existing residential or commercial building.

New additions should conform to all guidelines related to orientation of retail space, massing, scale, roof design, fenestration, storefront design, signage and materials used as outlined in the following section.

	EXISTING RESIDENTIAL	FRONT ADDITION	SIDE ADDITION	REAR ADDITION
1	1-storey height	Max. 2 storeys with optional porch/ colonnade	Max. 1 storey allowed	Max. 1 storey
2	2-storey height	Min. 2 storeys with optional porch/ colonnade	Min. 1 storey	Min. 1 storey
		Max 3 storeys with mandatory porch/ colonnade	Max. 2 storeys allowed	Max. 2 storeys

Architectural Design Guidelines
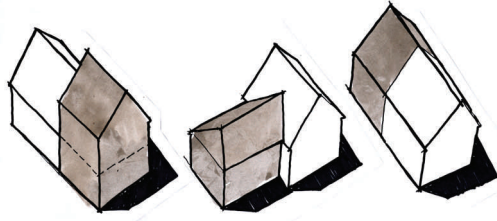
Appropriate additions to a one storey building. If a building is to have a two storey addition, it should be added to the front of the building, space permitting.

A good example of a one storey addition on this converted residence provides valuable commercial floor space. The size and scale of the addition also respects the existing building and complements the character of the street.
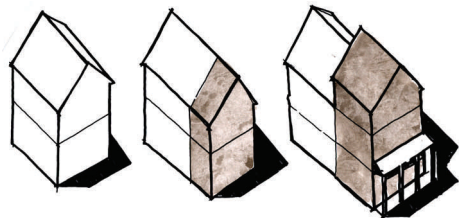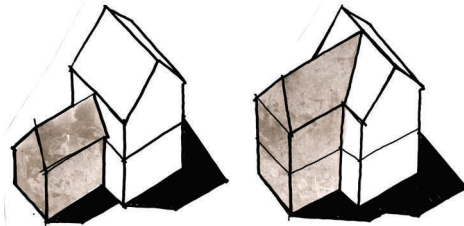
Options for side additions to a two storey building include one and two storey additions. For a three storey addition, a sense of scale can be maintained through the addition of a front porch or colonnade.

6.0 Architectural Design Guidelines

6.4 Guidelines for New Buildings

New mixed use or commercial infill development in the Binbrook Village Core should be planned and designed to achieve functional and visual continuity with the existing buildings and street patterns. The design of the new infill buildings can fulfill this objective by a variety of means: building massing can be broken into visually smaller segments, new architectural forms may be integrated with more traditionally designed older buildings, as well as existing footprints re-used, repeated or replicated. Existing and new built form should connect visually and spatially in order to retain and enhance the existing Binbrook Village character.

6.4.1 Character and Context

- New mixed-use infill construction should follow patterns of site utilization as guided in this document. New infill should be planned and designed in a manner that protects, sustains, and enhances the existing architectural character of the Binbrook Village Core;
- In addition to the setback requirements established, the new infill should match the existing build-ing's width, height and massing;
- New infill buildings should maintain the size and rhythm of space between buildings along the established streetscape.

6.4.2 Massing

- New mixed use infill development should conform to the proportion, scale and massing of existing residential or commercial buildings adjacent to the infill development;
- Larger architectural structures can achieve visual harmony with smaller buildings by the use of repetitive massing, roof forms, materials used and cladding colour. New, larger buildings may also be subdivided into smaller elements reflective of the mass or detailing of surrounding buildings (i.e. roof form may employ gables, mansard windows, front porches, colonnades, etc.).
- Although these guidelines do not prescribe architectural style, the goal is to avoid monolithic and "boxy" massing in order to create attractive buildings that reflect human scale;
- New commercial infill development may have a maximum three storey height as set in the Zoning By-laws. Massing should be encouraged such that height differences between adjacent buildings should attempt to be no more than one storey in order to provide smooth transition between the building masses;
- Commercial buildings, especially those located at the intersection of Binbrook Road and Highway 56 should be distinguishable by special architectural features other than large commercial signage. Clock towers, turrets or similar special features/elements may be used to create special landmark features.

6.4.3 Building Height

- New infill buildings should be encouraged to vary their height but not to extremes. The difference between adjacent or adjoining building heights should be no more than one storey. This guideline is to prevent inappropriate suburban commercial developments which may be out of character.

AVOID

An undesirable integration of buildings. The single storey centre building is dwarfed by the adjacent buildings. This approach is out of character in the Village Core.
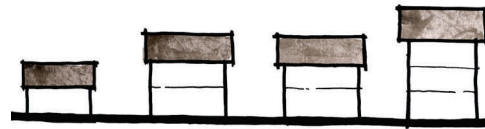
Three storey buildings can be accommodated in the core provided there is a transition between adjacent properties as illustrated.

6.4.4 Roof Form

- The majority of new mixed use buildings in Binbrook Village should have sloping roof forms. These forms should be derived from existing roof typology in the area. Flat roofs will not be permitted.
- More complex roof forms, consisting of a principal main roof with additions, is preferred in order to create human scale and harmony with surrounding context;
- Dormers, gables, chimneys and roof cornices add visual richness to a roofscape. These elements should be encouraged in the design of new buildings and can be used to create useable interior space within roofs;

- Minimum roof slopes should have 6:12, ideally 9:12 ratio or higher (12:12) are required in order to preserve rural/small town character of the Village;
- Balconies are encouraged on upper floors and over entry porches. Those will provide an additional visual interest, protection from elements, and opportunities for social interaction, street life and added security;
- Roofing materials may include asphalt shingle, wood shingle or shakes, copper and slate.

6.4.5 Street Façades

Street facades are usually the most important elevations that assist in enhancing the visual character of the Binbrook Village Core. The front elevations of new mixed-use buildings must contribute positively to the attractiveness of the streetscape and the Village Core.
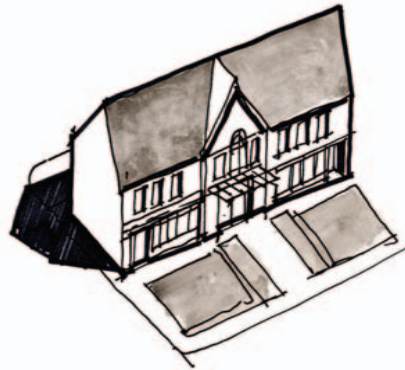
A well defined separation between different uses in a mixed use commercial building is important in order to maintain visual clarity.

- The entrances to mixed use buildings should be always oriented to and be highly visible from the street. Entrances for retail uses must be separated from residential entrances. Additional residential or service entrances connected to the rear parking may be provided from the rear or side of the buildings.

6.4.6 Rear and Side Elevations

- Although the front facades of mixed-use buildings are critical elevations, these buildings should also be designed to be seen from all sides;
- Materials and textures applied to the front façade should continue around the building;
- The roof form visible from the rear should be identical or similar to the front of the unit. False roof facades are discouraged;
- 50:50 split of façade materials (i.e. ground floor with brick and upper floors with siding) should be avoided on the sides and may be considered only at the rear elevations. Materials, colour and surface treatments should be consistently used on all elevations visible from the streets in the Village Core.

AVOID

Partial treatment of a building elevation should be avoided.

The preferred option for building facades that are visible from the street is a continuous treatment.

AVOID

A 50/50 split of materials is not acceptable where the building is visible from the street.

6.4.7 Architectural Style and Visual Character

These guidelines do not prescribe any particular architectural style for the Village Core. There is range of possibilities to creatively replicate or interpret traditional building forms with styles ranging from Victorian to Contemporary. An inaccurate or thoughtless mix of elements is undesirable.

- The architectural design of new mixed-use buildings in the Village Core should be based on the best examples of the surrounding build-ing fabric, character and architec-tural styles that predominate in the Core;

- Architectural details are essential to create human scale;

- Commercial buildings at the inter-section of Binbrook Road and Highway 56 may house uses of "name" commercial corporations. Their architectural designs should be customized to respect the Bin-brook scale and context. Standard "cookie-cutter" or "repeat" designs from other locations are strongly discouraged;

- Consistent rhythms of similar (not identical) details and architectural elements should be used to rein-force the framework and scale of the street and aid in the creation of a strong Village Core image;

- New buildings and conversions of the existing residential buildings into commercial use should com-plement the basic façade ele-ments, which include a transparent glazed lower storefront, a sign fas-cia over a projecting cornice and a solid upper façade punctuated by vertical windows.

6.4.8 Architectural Detailing

Architectural elements and details that appear artificial and inaccurate for par-ticular architectural style are not recom-mended. Examples include:

- Snap-in muntin bars inside win-dows;

- Fake shutters that are not one half of the width of the window opening;

- Paired windows, when only single windows typified the particular ar-chitectural style;

- Materials which are applied only to the main façade and only to the surrounding edges by the depth of the material (minimum returns on side elevations should be 2.4m (8'-0");

- Sliding doors with snap-in-muntins.
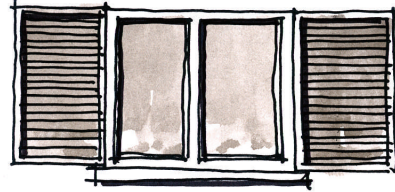
The shutters on this window are a good example of architectural detailing and reflect accurate scale and proportion.

The shutters on this window reflect a lack of proportion.

6.4.9 Architectural Materials

To avoid clutter one building material is encouraged as the primary cladding and a maximum of two other materials used as accents (i.e., stone used for foundations and entrance area, brick as main material and stucco for gables and dormers or entire upper floor);

- Excessive use of grey and pink brick colour is discouraged. These colour schemes do not contribute to the quality of the streetscape;
- If buildings of a period style are employed, colours selected should be compatible with the style and period.
- Materials for commercial buildings should be carefully selected to provide visual interest in texture and colour;
- Large scale panels constructed with materials used in large commercial or industrial developments are to be avoided. Other large scale materials/glass paneling must be scaled down to human proportions and be compatible with traditional materials;
- Glass curtain walls, pre-cast concrete and formed concrete shall be limited in use.

A good example of the use of materials whereby the main entrance is accentuated with stone. In this example the completed façades is limited to two primary materials, stucco and stone.

6.4.10 Fenestration

- Windows and doors should seek to continue or establish a rhythm along the streets;
- Windows on upper floors should be vertical in proportion rather then horizontal;
- Rows of windows on upper floors should be used to clearly define each storey;
- On new buildings, the placement and proportions of height to width of windows should complement those of the surrounding buildings;

The sensitive use of materials and colours is critical in achieving desirable streetscape appearance.

6.5 Multiple/Combined Lot Building Development

- There are opportunities in the development of combined lots to create building footprints other than strictly rectangular forms. L, U, or reverse U shape building footprints can promote landscaped front courts or provide an emphasis on the centre of the building;
- Multiple lot development will allow for larger building footprints. These larger building configurations will require a more careful approach to massing and façade treatment in order not to overwhelm existing adjacent buildings or the streetscape;
- The architectural design of buildings on a multiple lot development should ensure that long continuous elevations and roofscapes are appropriately detailed, divided and varied to provide visual interest and relief;
- Multiple lot developments should have limited and combined access points and parking areas to allow for better landscaping of the site and streetscape.

Single Lot Development

Existing Building   New Building

Combined parking & access

Double Lot Development

Existing Building   New Building

Combined parking & access
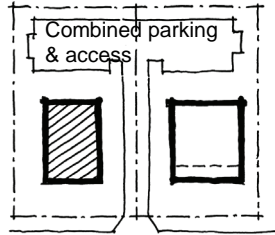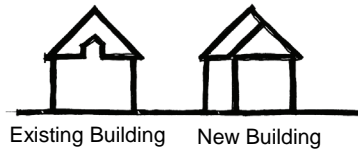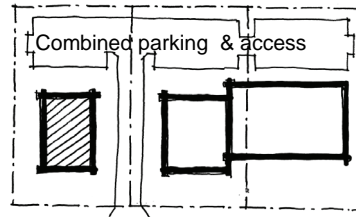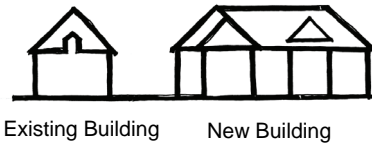
New L shape building with potential for large court yard
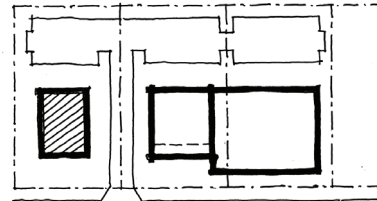
New L shape building with minimal setback

Triple Lot Development

Existing Building   New Building   Existing Building

Combined parking & access
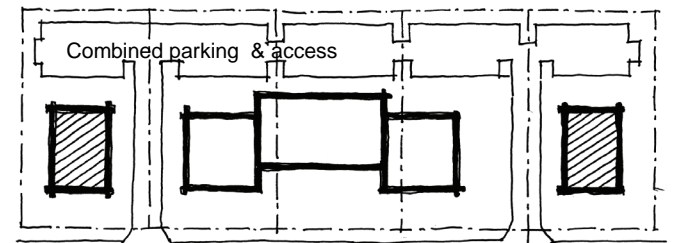
U shape building with emphasis on the ends of the building. This configuration provides an opportunity for recessed court yard
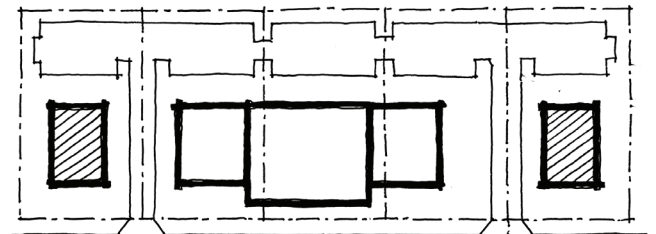
Reverse U shape building with minimum setback area provides emphasis on the centre of the building

Architectural Design Guidelines 6.0

6.6 Commercial Building Orientation

The following guidelines are based on a view that commercial development should have a high quality storefront design and detailing, as well as integrated commercial signage.

- Commercial buildings should allow access from the rear yard parking ;

- All commercial buildings within the Binbrook Village Core are to be street oriented. Buildings should provide continuous storefronts along the street/sidewalks with primary access from and to the front façade;
- Corner buildings are visually prominent and all street elevations should be given equal design treatment and care;

- Excessive blank walls demonstrating no specific architectural design on main commercial façades are to be avoided in the Village Core;
- Main façades shall have sufficient amount of glazing to provide casual surveillance and to visually break up the building mass.

A new commercial building that incorporates the principles of good storefront design as well as opportunities for landscaping as an integral part of the site development.

Architectural Design Guidelines

6.7 Storefront (Commercial Façade Design Treatment)

- In order to strengthen the pedestrian environment in the Village Core it is essential to provide continuous interest, variety and commercial activity at the ground floor level. Storefront design may greatly enhance the visual character of the Core;
- At the street level, windows should be sufficiently large to expose goods within shops and encourage a retail presence;
- Excessive glazed areas without wooden or metal frames/joints should be avoided. Storefront panes should be divided vertically to establish a rhythm along the street;
- Glazing on commercial building storefronts should avoid use of reflective glass in order to allow better visual contact between the interior and exterior. Reflective glass on upper floor windows may be considered where there is architectural merit in using reflective or unusually deep colour tints;
- Storefront entries may be recessed for weather protection, visual interest and more window display;
- Awnings should be encouraged to enhance the small town traditional streetscape, shelter pedestrians and provide additional visual interest;
- Simple awning patterns should be encouraged for buildings of a decorative style, while more decorative awnings are appropriate for simply styled buildings; and
- Signs should be front-lit only. Boxy, back-lit signage is discouraged

A pedestrian friendly store front that incorporates a colonnade as an opportunity to provide weather protection and additional retail display area.

The renovated storefront on this building incorporates the principles of good storefront design; ample window area to provide good retail exposure, a recessed doorway, and front lit signage on the awning.
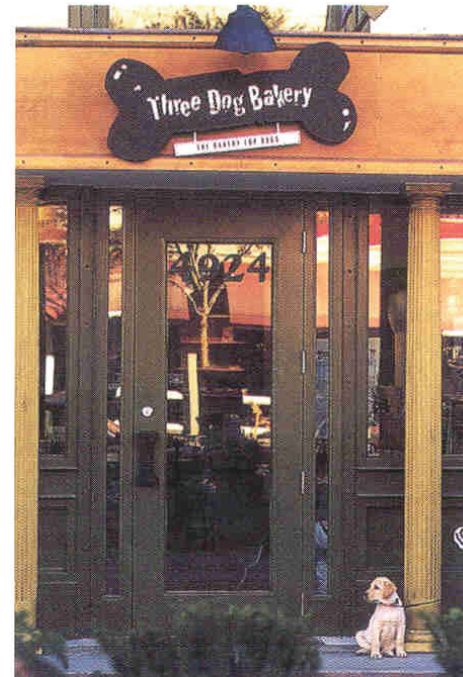
Architectural Design Guidelines

Legend

1. Base panel
2. Storefront door
3. Storefront column
4. Pier/pilaster
5. Display window
6. Transom window
7. Sign band
8. Storefront cornice
9. Sign lighting

Storefront design is critical for successful retailing and the overall appearance of the commercial core.

The redevelopment of buildings into commercial uses such as this Tim Horton's outlet and bakery incorporate the principles of creating a good store front image that are conducive to retail activity and are attractive to the overall streetscape.

7.0 Public Space Design Guidelines

7.1 Design Objectives

A vibrant and successful community core contains a hierarchy of public spaces that cater to a wide variety of users and functions. These public spaces are often directly related to activity on the street, retail and commercial establishments or as a key focus for civic activities in the community. If these spaces are adequately designed for public activity and interaction and are conveniently located, they will contribute to the street life and help to create a vibrant core.

The Binbrook Village Secondary Plan identifies the need for diverse public spaces. The various types of urban public spaces that may be included in the future development of the Binbrook Village Community Core are:

- *Village Square*
  A public square located at the north-east corner of the main intersection will provide a key civic focus for the Community Core and provide an opportunity to accom-

Open space that is associated with public buildings such as a post office are often used as casual meeting places and are vital to promoting social interaction in the Community Core.

modate a variety of landscape elements such as a water feature, public art or an architectural feature such as a gazebo or trellis.

Due to the prominent location of the village square consideration should be made to providing access to future retail and commercial businesses around its perimeter.

- *Parkette*
  The open landscape space in front of the historic Binbrook Memorial Hall is a valued attribute in the Community Core. This space has the potential to be transformed into

a desirable civic parkette with emphasis on the veterans memorial.

- *Courtyard*
  A courtyard is generally an enclosed space which is organised and designed with a relationship to an abutting public or commercial/retail building, i.e. library or post office. A court yard may also contain intimate sitting areas adjacent to the sidewalk with views to and from the street. Opportunities to establish court yards that are integral to site development throughout the village core should be encouraged.
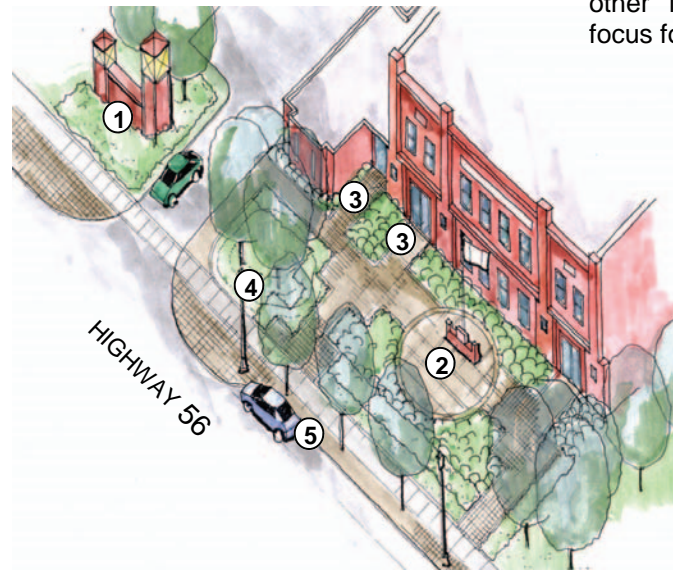
7.2 Design Guidelines

- An open space should opportunities for comfortable social interaction;
- Entrances into a public space should be inviting with multiple points of entry if possible and provide visual and barrier-free physical access from the sidewalk or adjacent buildings;
- Pedestrian access through the space should be convenient and safe;
- Public spaces will be encouraged to include other pedestrian amenities such as drinking fountains, bicycle racks, trash receptacles, etc. Landscaping, low walls and steps can be used as alternate forms of seating;
- Opportunities for public art or sculptures should enhance the promotion of cultural activity;
- A public space should be designed to reduce any onerous maintenance requirements;
- For safety and security reasons, lighting must be adequate and

there must also be visual and physical access from the street or surrounding buildings;

- Open spaces should be well buffered from moving cars;

There is a significant opportunity for introducing a parkette at the existing Binbrook Memorial Hall. This historic building located at the main entrance into the Binbrook Agricultural Fair Grounds functions today as a community centre. The existing memorial could be incorporated into a prominent ceremonial public space in the Community Core.

- Avoid blank walls around the perimeter of a space. This will create an unsafe environment and prevent interaction between the space and the surrounding buildings;
- Explore opportunities for the inclusion of water features, gazebos or other landscape structures as a focus for the space.

LEGEND

1) Improved access to Binbrook Fairgrounds with entry feature and signage
2) Create opportunities for improved public space, with focus on veterans memorial
3) Improved physical access to building where required
4) Landscape treatments to enhance building and provide continuity with public streetscape
5) Typical streetscape treatments complete with on-street parking

Public Space Design Guidelines

Courtyard Design Features and Objectives

1) Focal point of courtyard potentially consisting of water feature or public art
2) Lighting, benches and other street furnishings provide comfort convenience and safety for pedestrians
3) Plantings can be incorporated to create a visually pleasing and intimate surrounding

4) Courtyards are typically enclosed by buildings or architecturally designed fencing and screens. Pedestrian access to buildings is encouraged to promote activity and use of the courtyard
5) Courtyards should be visible from the street to encourage use and promote safety
6) Physical access from the sidewalk should accommodate all user groups
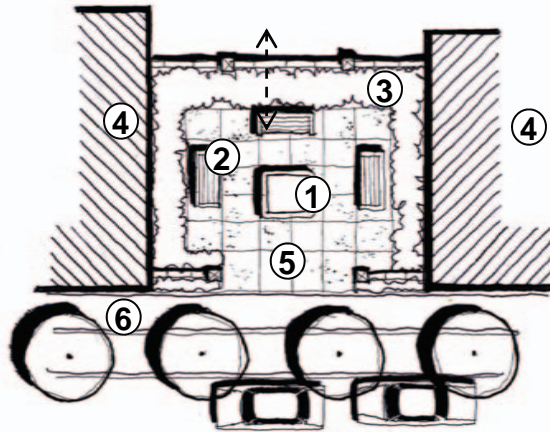
Courtyards should be encouraged as they can add to the variety of pedestrian open space in the Community Core. Courtyards can be designed as either intimate spaces or they can be designed to accommodate access to adjacent buildings. In either case the emphasis of the design should be to encourage activity and socialization in the core.
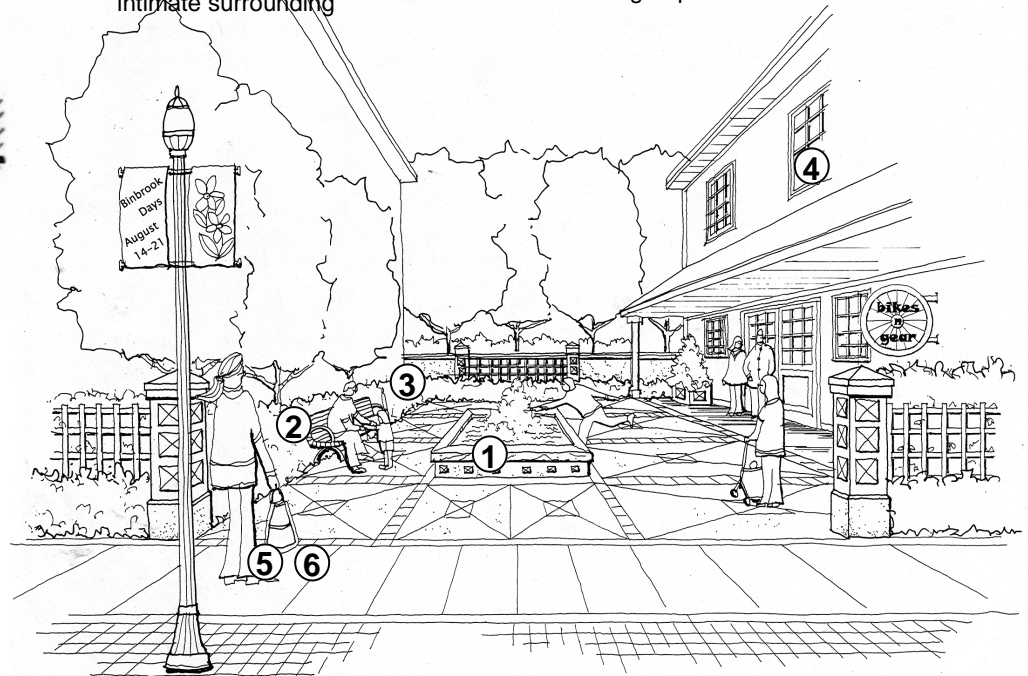
Village Square

The village square, located at the northeast corner of the main intersection is intended to serve as a focal point of the community.

Design Features and Objectives

1) The village square should incorporate a gazebo type structure to accommodate a variety of uses
2) Enhanced pavement treatment can be used as a valuable design element
3) Design elements such as raised planted columns can be used to provide accent
4) A variety of seasonal plantings can create visual interest throughout the year
5) Low walls, steps and benches can provide a variety of formal or informal seating arrangements
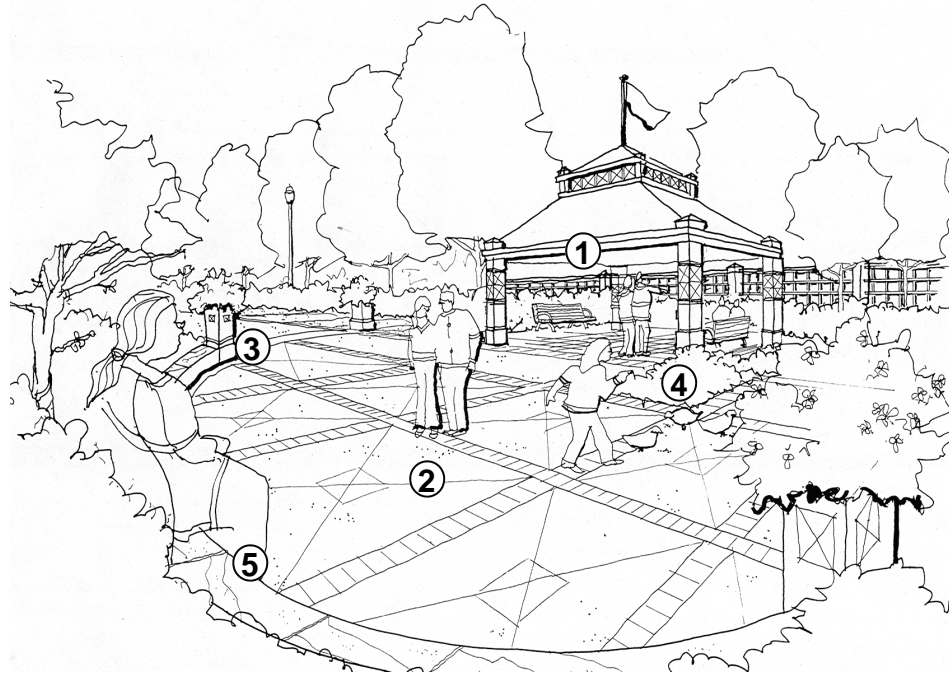
# Public Space Design Guidelines
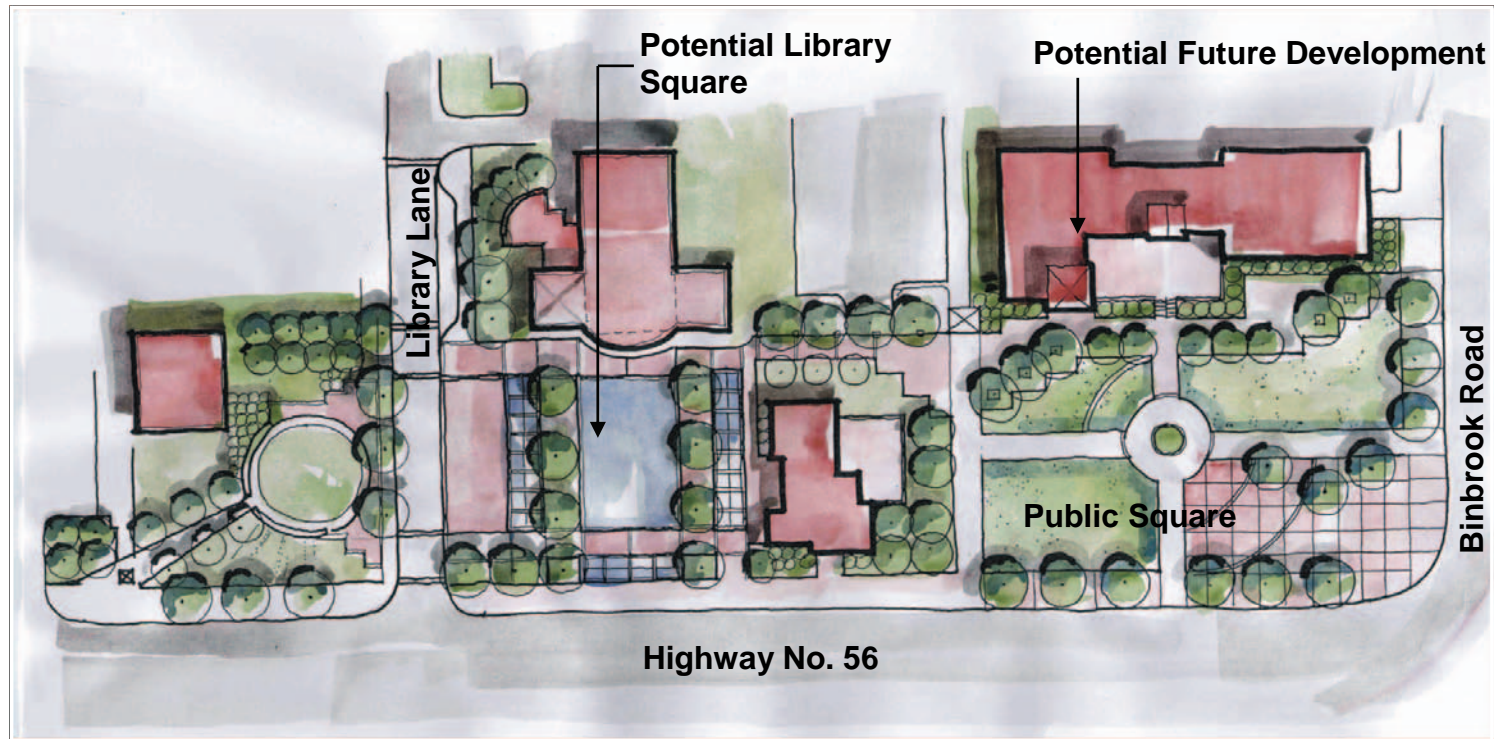
Public space development is key to creating a vibrant Community Core as illustrated in the above plan. While the Secondary Plan indicates the need for a major public square at the main intersection in the Community Core, other public spaces such as a small court yard at the library could be created to provide opportunities for alternative activities. Well designed public spaces will also help to attract users to the Community Core.

Public Space Design Guidelines

Library Courtyard

Design Features

1) Building design with an articulated front façade. Openings and activities should be oriented close to the street to allow surveillance of the square at nighttime.
2) The architectural massing and detailing to address the character of Binbrook through the use of contemporary architecture.
3) Use traditional material on the front façade (stone, brick, stucco)
4) Courtyard can be enclosed with a colonnade and glass canopies that will provide opportunities for all-weather use of the square
5) Provide sufficient lighting of the public square
6) The use of water in the design of the public square is a desirable element that will provide opportunities for four season enjoyment
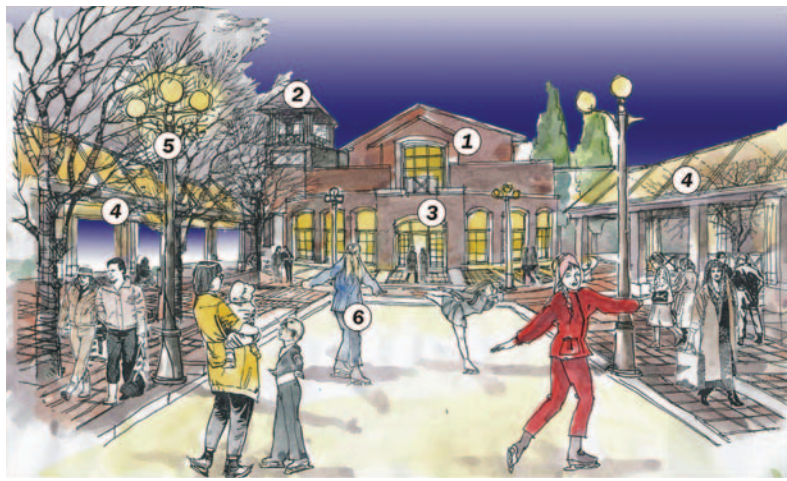

Potential Library Courtyard—a courtyard such as this could be programmed for four season use ensuring that the core is vibrant and active year round.

8.0 Gateway Design Guidelines

8.1 Design Objectives

The Binbrook Village Secondary Plan identifies the need for gateway features to be constructed at the boundary of the Community Core on Highway 56 and Binbrook Road. The primary functions of these major gateway features are to:

- serve as landmarks highlighting the arrival into the core;
- provide a transition from a rural setting into the Village Core; and
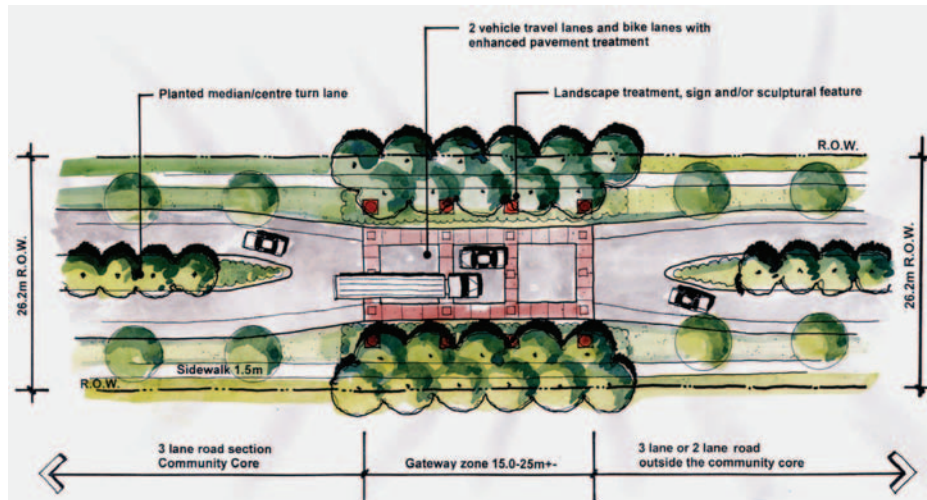- reflect the character of the Community Core area.

In addition to these aesthetic functions, gateway features may also serve as traffic calming features. As traffic calming features they perform two primary functions. The first is to slow traffic down to an acceptable speed within the Community Core and secondly increase the motorists level of awareness that they are entering an area with a greater level of pedestrian activity.
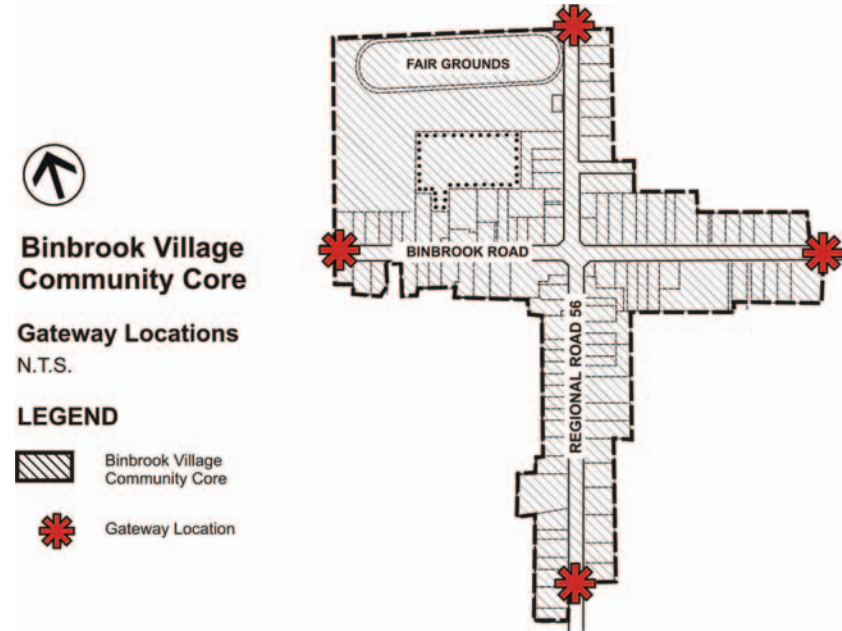
Plan of the Binbrook Village Community Core illustrating the location of the proposed gateway features on Highway 56 and Binbrook Road at the entrances into the Community Core.
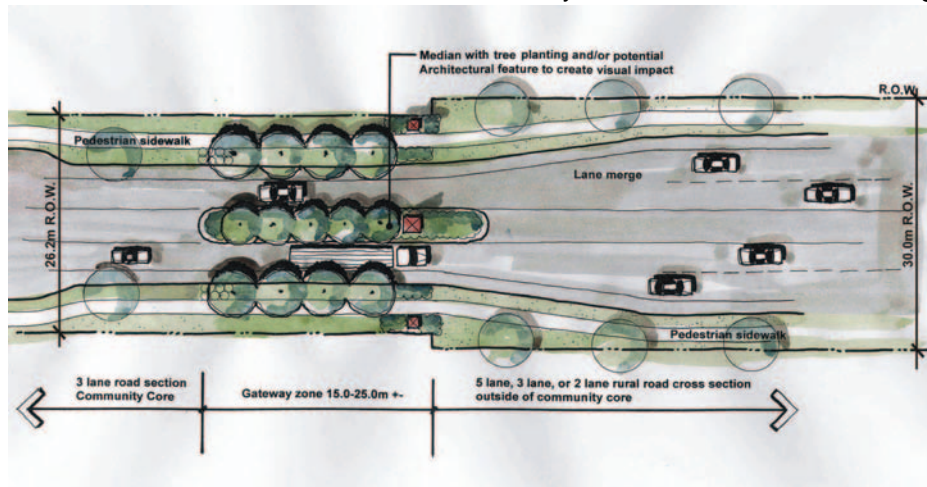
An alternative gateway design with a road narrowing. The road narrowing and planted medians are effective traffic calming elements that can be incorporated into the gateway. Other features of this gateway may include decorative pavement, tree planting and architectural features.

Gateway treatment with road narrowing

A potential gateway design with a centre median. This type of gateway is applicable at the north limit of the community core on Highway 56 where the road cross-section will change from 3 lanes to 5 lanes.

Gateway treatment with median

8.0 Gateway Design Guidelines

8.2 Design Guidelines

The design of a major gateway is subject to the function of the road and the width of the right-of-way. The imaginative use and incorporation of the following elements will help create distinctive and interesting gateways;
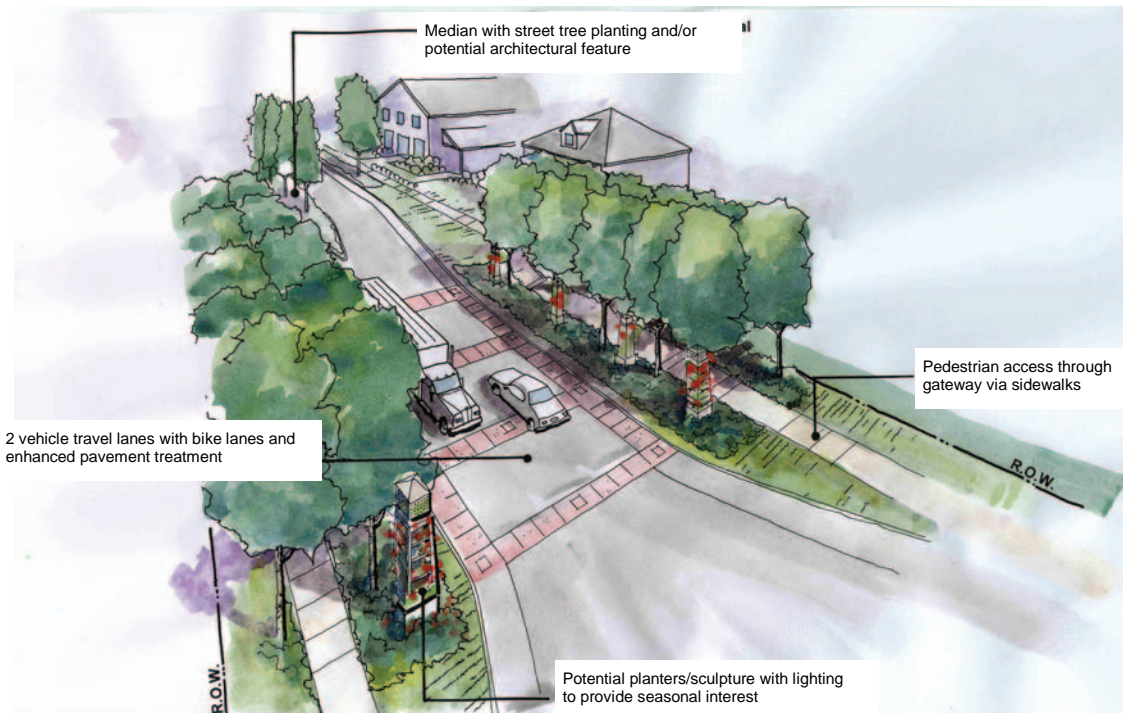
- Street trees, shrubs and ground-cover plantings to create visual interest;
- Plants selected for a gateway feature must be hardy, salt tolerant and low maintenance. Irrigation and general maintenance must be considered when designing planting areas associated with gateways;
- Where plantings or architectural features are incorporated they should consider sightline requirements for vehicular and pedestrian traffic.
- Upgraded pavement treatments to provide a contrast in colour from the asphalt road surface. These pavement treatments may consist of precast concrete unit pavers, coloured concrete or a combination of either of these materials;

- Medians as part of gateways should be designed to be accessible as refuges for pedestrians crossing the roadway.
- Materials such as masonry, natural stone, or steel may be used to construct an architectural component of the gateway. These materials should be selected to emphasize the overall design of the gateway and reflect the distinctive character of the Community Core;
- Lighting may be used to create special effects and to accentuate the feature. Careful attention must be given to the selection and placement of the lights in order to prevent interference with motorists vision;
- Signage and graphics may be used to identify the name of the community or to incorporate a community logo. The materials and colours selected for any graphics must be compatible with the overall character of the gateway and in keeping with the character of the core area;
- Sculpture or public art may be used to display a theme or something of significant cultural importance.
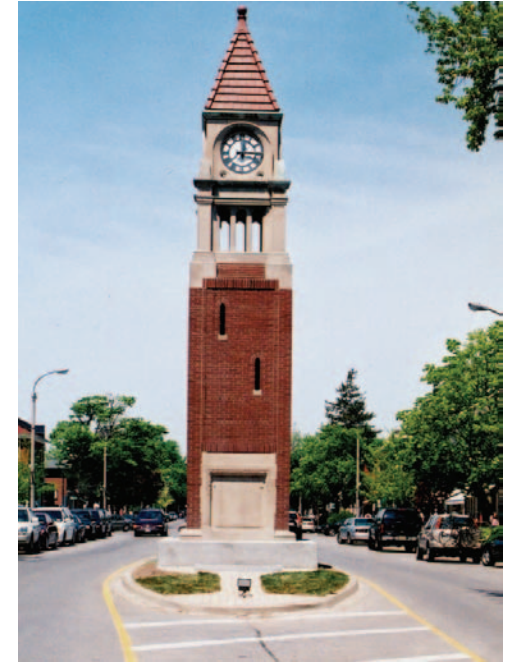
An architectural feature such as this clock tower can be used as a gateway element at a major entrance into a community core.

Elements of a Gateway Feature

A gateway feature may incorporate tree and shrub planting, pavement treatment and other landscape features as focal elements as illustrated in the layout of a potential gateway feature within the 26 metre right-of-way proposed for Highway 56 and Binbrook Road

Median with street tree planting and/or potential architectural feature

Pedestrian access through gateway via sidewalks

2 vehicle travel lanes with bike lanes and enhanced pavement treatment

R.O.W.

R.O.W.

Potential planters/sculpture with lighting to provide seasonal interest

Although the primary focus of a gateway is oriented to vehicles, pedestrian access into the community core must be accommodated in the gateway feature design where applicable.

9.0 Parking Design Guidelines

9.1 Parking Design Guidelines

Design Objectives

The Secondary Plan recognizes the need to accommodate parking for automobiles and loading areas for delivery and service vehicles. It recommends that parking be accommodated through on-street parking as well as parking lots located on-site to service individual establishments. However, the Secondary Plan also recognizes that in order to create a pedestrian friendly environment and an aesthetically pleasing core, on-site parking lots should be located behind the buildings in the core.

9.1.1 On-Street Parking

Traditionally, downtown areas have included on-street parking. There are a number of benefits that on-street parking provides: convenience, providing separation between the street and pedestrians, slowing traffic, and adding to the life of the street. On-street parking will be provided in the Community Core in strategic areas along Highway 56 and Binbrook Road.

9.1.2 Off-Street Parking

The location of off-street parking is critical in determining the overall appearance of the streetscape in the community core. Off-street parking lots located at the rear of a property will aid in removing unsightly parking and loading areas from street view.

Additional benefits of locating off-street parking to the rear of buildings are maintaining a streetscape which emphasizes a direct connection between pedestrians, buildings and the landscape. This helps maintain a sense of scale and character to the street.


Conveniently located on-street parking in the Community Core is desired.

Parking lots located at the front of the property are not acceptable. Visual and pedestrian access to the building is a key aspect of a successful street-scape in the Community Core of Binbrook. This is made difficult when the view to the building is blocked and the pedestrian route is secondary to vehicles.

AVOID
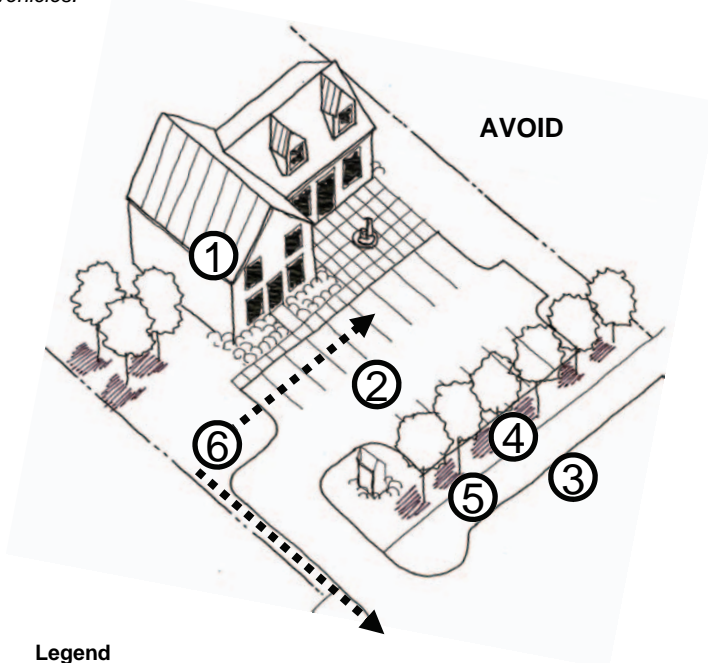
Legend

1. Existing renovated or new building
2. Required parking (size of lot to be determined as permitted by zoning)
3. Proposed on-street parking
4. Landscape buffer to screen parking also screens building from view of the street
5. Pedestrian access from sidewalk is prohibited by parking lot
6. Pedestrian access to the building is secondary to vehicle access

Parking located directly in front of retail buildings detracts from the char-acter of the buildings and is unsightly due to the expanse of asphalt. In this example there is little opportunity for any landscape improvements including well defined pedestrian access from the sidewalk as a result of the space required for automobile parking.

9.0 Parking Design Guidelines

Parking lots located behind the building encourage a pedestrian friendly street with the focus on the architectural character of the building and the streetscape.

Parking lots that are located behind the building should provide convenient pedestrian access to the building or street via a common walkway or rear entrance.
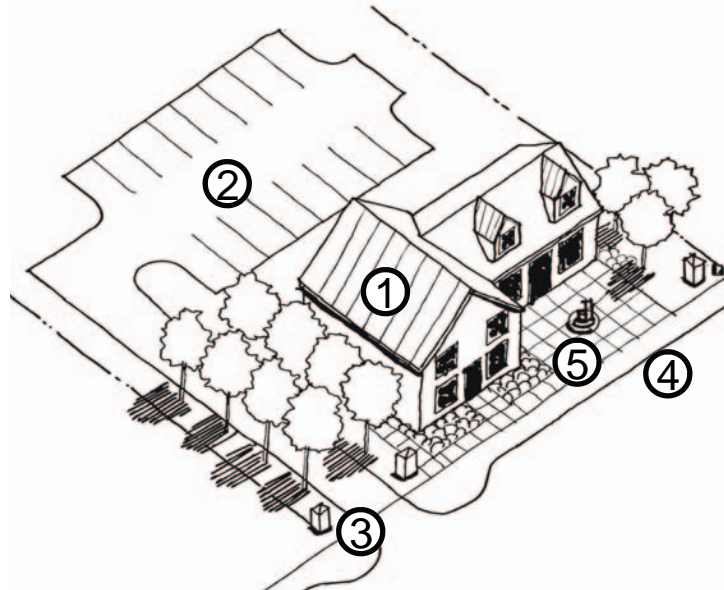
Legend

1. Existing or new building with a strong physical connection and visibility to the street
2. Required parking (size of lot to be determined as permitted by zoning)
3. New 6.0 metre wide driveway with potential for shared access with adjacent site
4. Proposed on-street parking
5. Landscape opportunity with direct access from the sidewalk

Parking for this retail business is located directly behind the building. The character of the building, pedestrian access, signage and landscape treatments are highly visible from the street. The net result is a greater aesthetic value for the character of the street.

9.2 Consolidated Parking Lots

Parking lot consolidation may be one component of a parking strategy. There are a number of potential benefits such as better organization and movement of vehicles, (including service and delivery vehicles), and an aesthetic streetscape that favours pedestrian activity and movement.

Legend

1. Existing or new building
2. Proposed parking with connection to adjacent property (size of lot to be determined as permitted by zoning)
3. New 6.0 metre wide shared access driveway
4. Potential connection to future development
5. Area for on-street parking in front of retail /commercial buildings

⋮ Pedestrian access from parking area to building or street
▼ (opportunity for landscaped walkways)
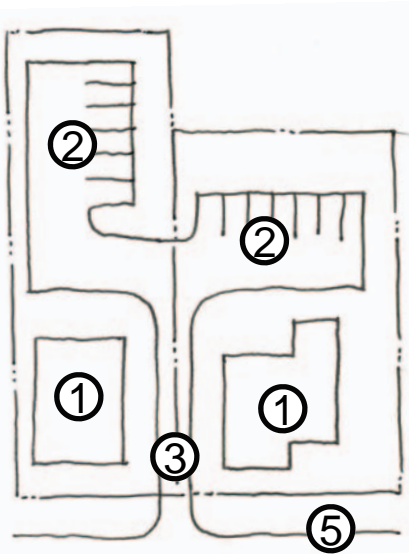
Parking lots located at the rear of the property with shared driveway access can be successfully achieved for two properties as illustrated. However in order to maximize the area for on-street parking the location of the driveway must be considered carefully.
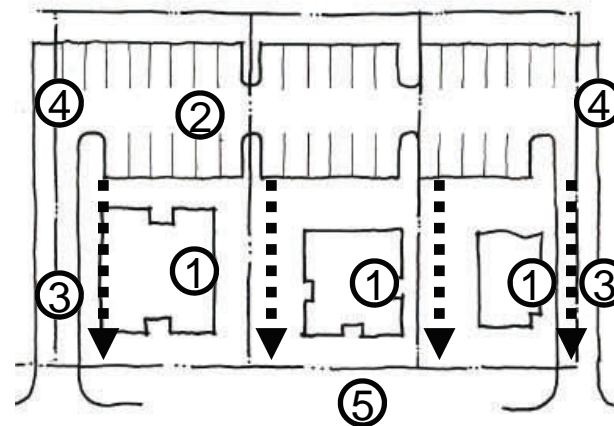
The benefit of shared parking and access for multiple lots will result in fewer driveway entrances along the street. This will provide an opportunity to have uninterrupted on street parking bays , help to minimize potential pedestrian and automobile conflicts and allow for a larger unobstructed landscaped streetscape including planted medians.

9.0 Parking Design Guidelines

Design Guidelines

- Parking lots should be located behind a building or situated within the interior of a complex of buildings;

- Shared entrances for parking lots should be encouraged in order to minimize vehicular traffic across the sidewalk and to minimize potential conflict with pedestrian movement;

- Surface parking lots should be designed to avoid large expanses of asphalt. The use of landscaped islands is an effective way to reduce the amount of asphalt and make the parking lot comfortable and aesthetically pleasing;

- Pedestrian movement through a parking lot should be safe, convenient and clearly demarcated with upgraded pavement or pavement markings;

- Planted islands should be incorporated to provide separation for vehicles and create a visually pleasing environment. The layout of a parking lot should consider the so-lar orientation and incorporate high branching deciduous trees in the islands as a way to provide shade for the vehicles;

- Landscape buffers consisting of plantings in conjunction with walls or fences should be constructed to provide separation from adjacent land uses or establishments;

- Lighting for safety and security purposes should be included in the design of parking lots. The type of lighting should be in conformance with lighting standards that are establish for the entire core area, i.e. pole style and colour, bulb type, mounting height, etc. and adhere to the "dark skies" design principles.

www.pedbikeimages.org/danburden

On-street parking and streetscape treatments can coexist in the Community Core. The level of design as illustrated in this photograph will contribute to the aesthetic character of the core and maintain the functional requirements of on-street parking.

10 Implementation of Design Guidelines

10.1 Introduction

Implementation of these guidelines will rely on a number of public and private initiatives including changes to municipal by-laws and careful consideration and review of development applications. These initiatives are described more fully in this section.

Private sector development in the core will be subject to requirements under the Planning Act including the site plan review process to ensure compatibility with these Urban Design Guidelines.

City policies and by-laws that are to be reviewed or created in conjunction with development in the core are:

Sign By-Law
• A new comprehensive sign by-law is required for the Binbrook Village Commercial Core area and thus will be implemented as part of the City's new Sign By-Law Study.

Comprehensive Commercial Core Zoning
• A new comprehensive zoning by-law is required for the commercial core as stated in the Binbrook Village Secondary Plan. This new by-law has been included in section 10.4 of this report.

Site Plan Control
• Private property improvements subject to the Planning Act will be implemented through Site Plan control to ensure compatibility with these guidelines.

Heritage Designation
• The designation of heritage properties under the Ontario Heritage Act in the Binbrook commercial core is essential to ensure that heritage buildings are protected. The Urban Design Guidelines recognize that the preservation of key heritage buildings is fundamental to retaining and emphasizing the character of Binbrook. These guidelines will be forwarded to the Municipal Heritage Committee for consideration.

Public Art Policy
• Current policy directives elsewhere in the City require that one percent of public project costs are directed to ensure that public art will be incorporated into site development projects. In Binbrook, development of public projects will provide opportunities for the inclusion of public art in the Community Core. The City shall implement appropriate Official Plan amendment policies to address this in Binbrook and on a City-wide basis.

Urban Braille
• The City of Hamilton Urban Braille system has become the standard model for the Hamilton Downtown and is being considered for inclusion in other commercial areas within the City. The Urban Design Guidelines have included Urban Braille as a component of the Community Core streetscape.

10.2 Capital Project Implementation

A time frame for capital projects has been established. These public realm streetscape projects are directly linked to the implementation of the Binbrook Village Transportation Master Plan, which in turn is based in part on the completion of development phases one and two as identified in the Binbrook Village Secondary Plan.

The following table is a summary of key public streetscape projects that have been identified for implementation as outlined in these guidelines. The information in this table includes the ownership, timeframe for construction, priorities, and budget implications associated with the recommended actions.

Timeframe
Short Term   1 to 5 years
Medium Term   6 to 10 years
Long Term   more than 10 years

Priority
The public elements included in this table are to be implemented in conjunction with the phasing and implementation of the Binbrook Village Transportation Master Plan. They have been identified as Low, Medium or High priorities.

Budget Implications
Since these projects are municipal projects, they will require capital project funding. The anticipated budget implications for each of the listed projects has been estimated as having Low, Medium or High impact on future capital budget forecasting.

Item No.	Municipal Streetscape Improvement Projects	Responsibility (Primary)	Time-Frame	Priority	Budget Impacts
1.1	Road reconstruction / streetscape improvements	Public Works	Medium-Long Term	Medium - High	High
1.2	Public square at the main intersection	Planning and Development Public Works	Medium Term	High	High
1.3	Community Core Gateways (Highway 56)	Public Works	Medium -Long Term	Medium	High
1.4	Community Core Gateways (Binbrook Road)	Public Works	Short Term	Low	Medium
1.5	Landscape improvements at Binbrook Hall/Fairground entrance	Planning and Development Public Works	Short Term	High	Medium

10 Implementation of Design Guidelines

10.3 Zoning

One of the keys to implementing the Urban Design Guidelines lies within the ability of the proposed guidelines to be implemented through the former Township of Glanbrook Zoning By-law. At present, the existing zoning in the area designated as Community Core, within the Binbrook Secondary Plan, would not permit the implementation of the guidelines. It is recommended that through the development of the new City of Hamilton Zoning By-law, the existing Community Core zoning be modified to reflect and permit the implementation of the Design Guidelines, within the work program when Commercial zoning is revised.

The zoning will create consistency in the Community Core for all properties. The intent would be that existing uses shall be permitted to remain but the long term vision can be maintained.

Key elements that will need to be reviewed and incorporated into the new zoning regulations should include the following:

- Provide for a range of uses consistent with the Community Core Secondary Plan policies.
- Permit existing dwellings to remain.
- Provide for the opportunities for mixed use buildings and restrict new stand alone residential buildings.
- Require buildings to be brought closer to the front property lines to improve the pedestrian streetscape.
- Restrict the construction of flat roofs on new buildings
- Require rear yard parking, ability to share driveways for access to parking and provide for ability to access parking to through easements to gain access to public streets.