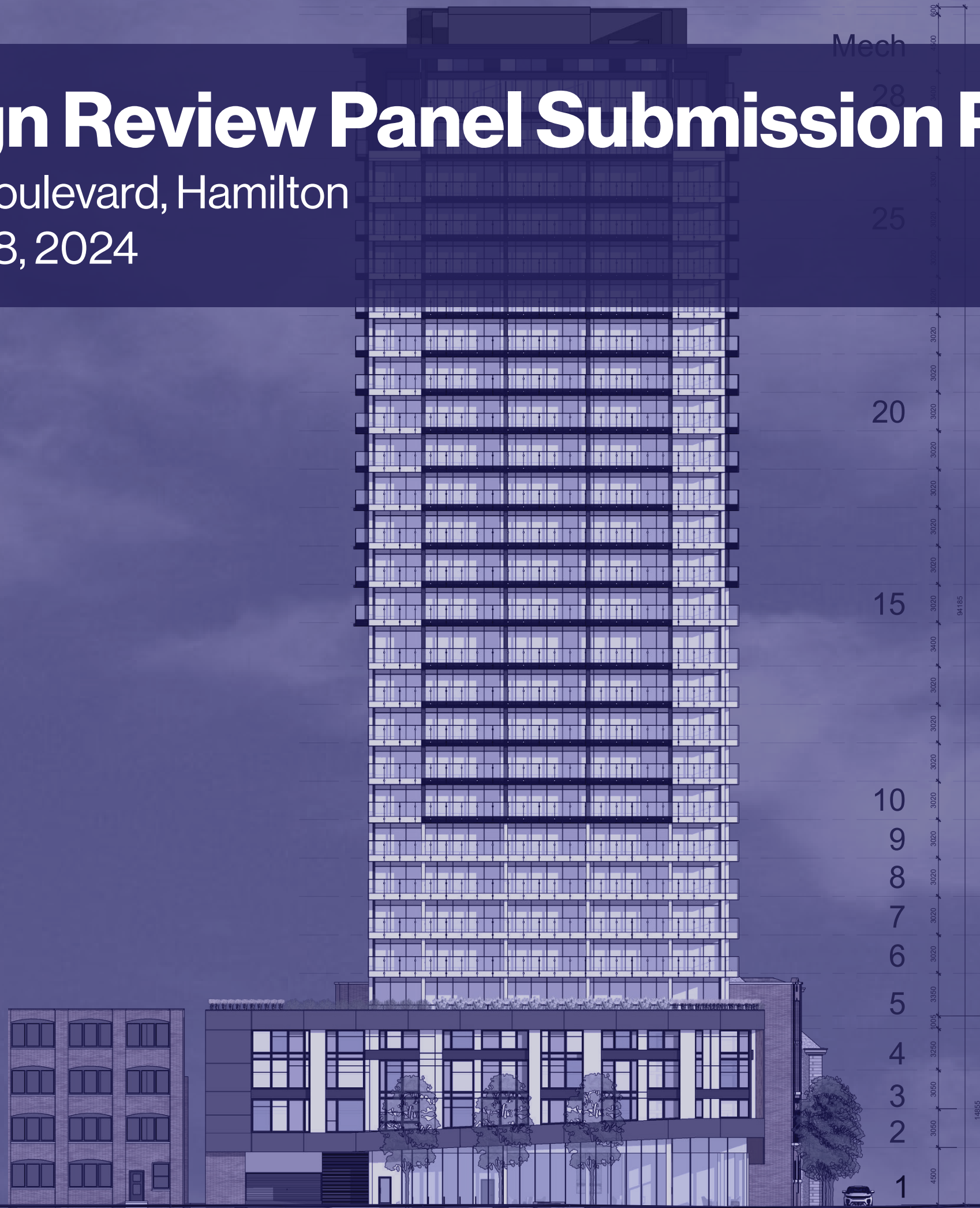


Design Review Panel Submission Package

58 York Boulevard, Hamilton

February 8, 2024



Hamilton DRP Submission Requirements

58 York Table of Contents:

Completed applicant project summary sheet.	Introduction	3
Photographs of the surrounding streetscape and adjacent lands to illustrate existing site conditions and surrounding context, along with a map identifying where each photo is taken.	Site conditions and surrounding context	
	York Blvd before Hamilton, and today	5
	Immediate Context	6
A design brief (approximately 4 pages in length) that includes a:	Urban Design Context	7
	Design Brief	
concise summary and response to the applicable City urban design policies and guidelines;	Response to Tall Building Guidelines	9
contextual analysis including:		
transportation networks for cars, cyclists and pedestrians	Contextual Analysis : Transportation Networks (Mobility)	10
focal points/nodes	Contextual Analysis: Urban Structure	11
gateways		
parks/open spaces		
topography		
views towards the site		
the urban pattern (streets, blocks)		
future and current proposals		
public art		
heritage resources		
height analysis of the surrounding context for all tall building proposals	Height Analysis & Surrounding Proposals	12
	Proposed Development	13
	Design Vision	14
	Project Summary	16
	Built Form & Massing	17
Models and/or illustrations that show the project massing and relationships in its urban context. Massing illustrations to provide the Panel with an understanding of the overarching approach to the design of the site		
Detailed perspective drawings or computer models, placing particular emphasis on the first few storeys and how the project responds and relates to its urban context. Perspective drawings should be set within the existing streetscape, indicating compatibility with development on both sides of the street. The intent is to show the Panel how the proposed development’s design and scale relate to the existing context.	Tower Design	18
	Podium Design Evolution	19
	Building Base Design	21
Site Plan with all dimensions in metric as well as adjacent properties and buildings.	Conceptual Site Plan	23
Landscape Plan	Landscape Design & Amenity Areas	26
A plan showing existing and proposed servicing, if requested by City staff during formal consultation.	Not included in this submission	
Section and floor plans.	(See Appendix A)	
Building elevations detailing proposed materials and colours.	Building Elevations	27
Sun/shadow and wind studies (if requested by City staff during formal consultation).	Summary of Sun Study, and Wind Study	29
	<i>Full studies sent as separate documents</i>	
Cultural Heritage Impact Assessment (if requested by City staff during formal consultation).	Summary of Cultural Heritage Impact Assessment:	29
	<i>Full study sent as separate document</i>	

Introduction

SRM Architects + Urban Designers is pleased to present this Design Review Panel Submission Package on behalf of Belmont Equity (58 York) Hamilton Inc. (the “applicant”) in support of a Site Plan application and removal of Holding Provisions for 58 York Boulevard, Hamilton (the ‘Site’). The applicant is proposing the development of a 28-storey (94m) mixed-use building with ground floor commercial and residential above. The building has a 4-storey podium. The proposal has 344 residential units, 120 square meters of commercial space, and a total gross floor area of approximately 23,102 square meters.

The Site is located on York Boulevard, at the corner of Park Street N, in Downtown Hamilton. The property is almost square in shape and is approximately 0.19ha, with 43.3m of frontage on York Blvd and 45.57m on Park St N. Like much of downtown Hamilton, the Site is currently being used as a parking lot. The Site contains no topographic constraints.

The Proposed Development is in keeping with the overall intent and purpose of the Official Plan, and the Downtown Hamilton Secondary Plan, with a change in the required side-yard setback to the east of the property:

Regulation	Permitted	Proposed
Side Yard (east)	12.5m	6.65m

In order to effectively review and evaluate the Proposed Development, this Submission Package includes the following information:

- An analysis of the urban design policy and immediate context within 400m;
- An overview of the design vision of the Proposed Development;
- A detailed summary of the Proposed Development, including site organization & circulation, built form & massing, building base design, tower design, potential impacts & mitigation, amenity areas, sustainable design;
- Appendices containing the relevant architectural and landscape drawings, and supporting Pedestrian Wind Assessment, Heritage, and Shadow Impact Study.

Of note, the immediate site context does not include any open spaces. It is to the north of the official view corridors to the Niagara Escarpment. As such, the context analysis will not address these elements.



Site conditions and surrounding context

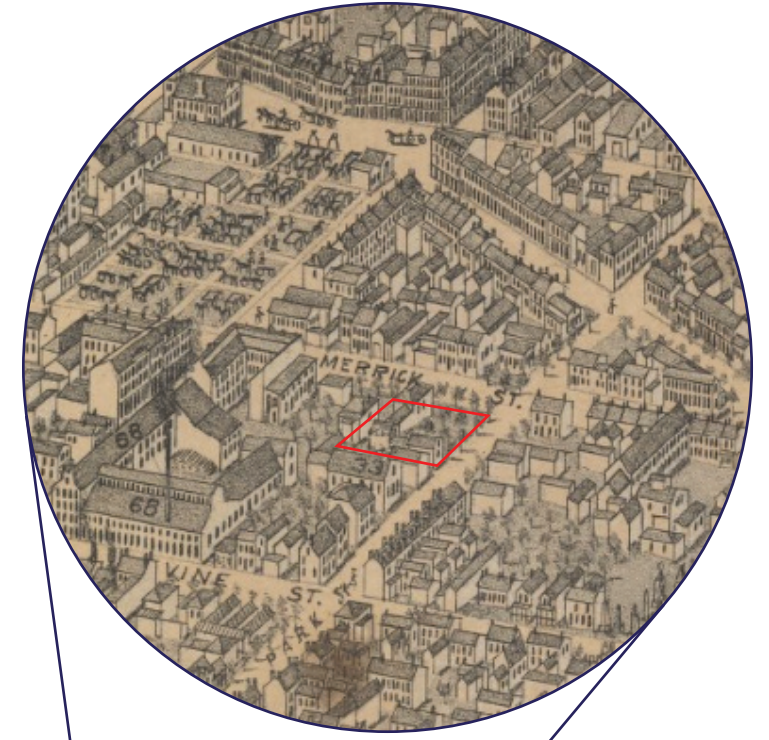
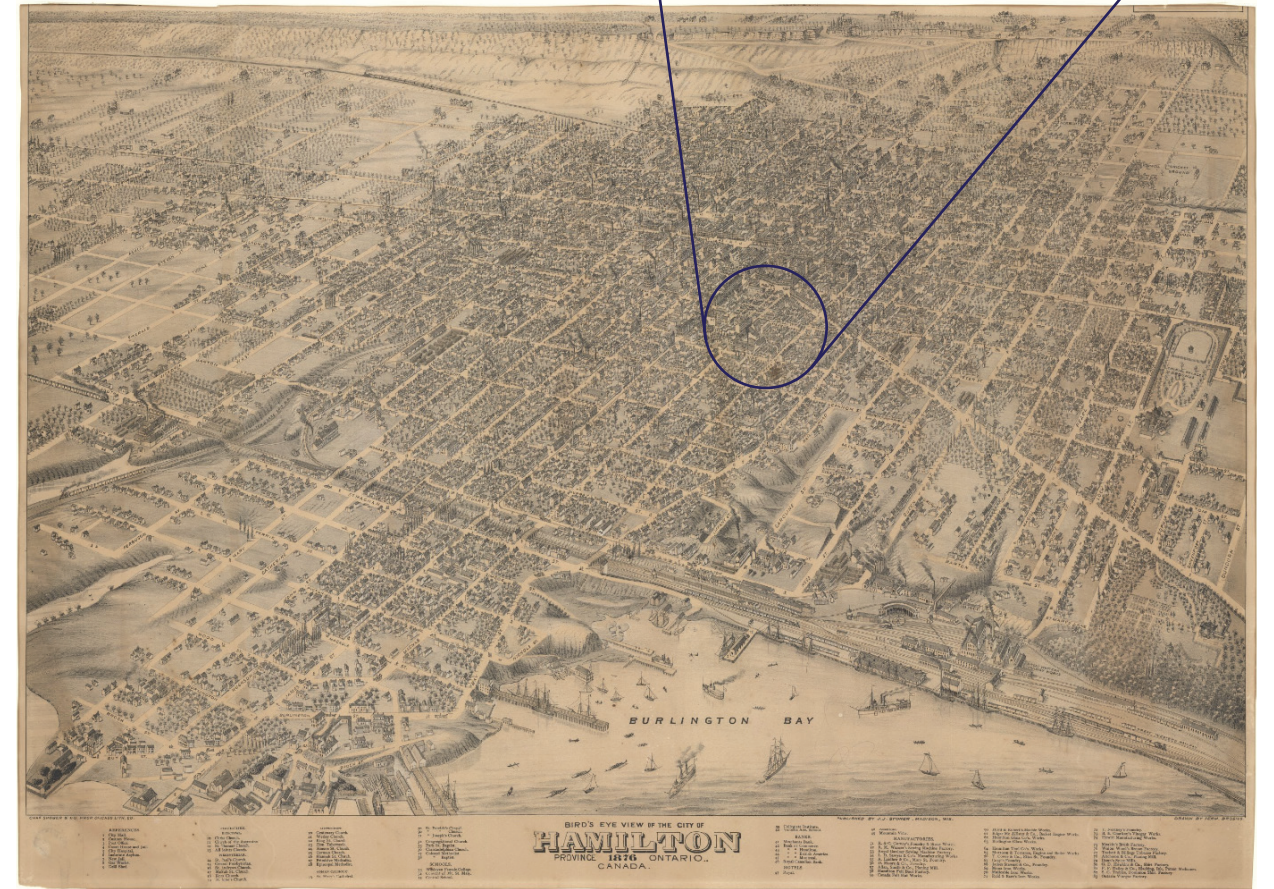


York Blvd before Hamilton, and today

York Boulevard is an ancient indigenous trail that preexists the founding of colonial Hamilton. This ancient trail was important route along the shore of Lake Ontario, evolving into the main road between Kingston and Niagara during colonial times. This is why York does not run parallel to the grid layout of Hamilton.

The site is located on modern day York Boulevard. When Hamilton was part of the colony of Upper Canada, York Blvd continued straight at Bay St, bisecting the current First Ontario Centre and Jackson Square Mall. In the 19th century, the Site would have been one block north of York, on Merrick St. This area was once a thriving centre of the city, with a mix of factories, shops and houses. Today the public realm has been taken over by the automobile. The vast majority of all surface area surrounding the site is covered in asphalt for cars, and used either for parking or driving.

The 58 York Blvd project represents an opportunity for the city to recover what was once a thriving heart of the city.



Site conditions and surrounding context

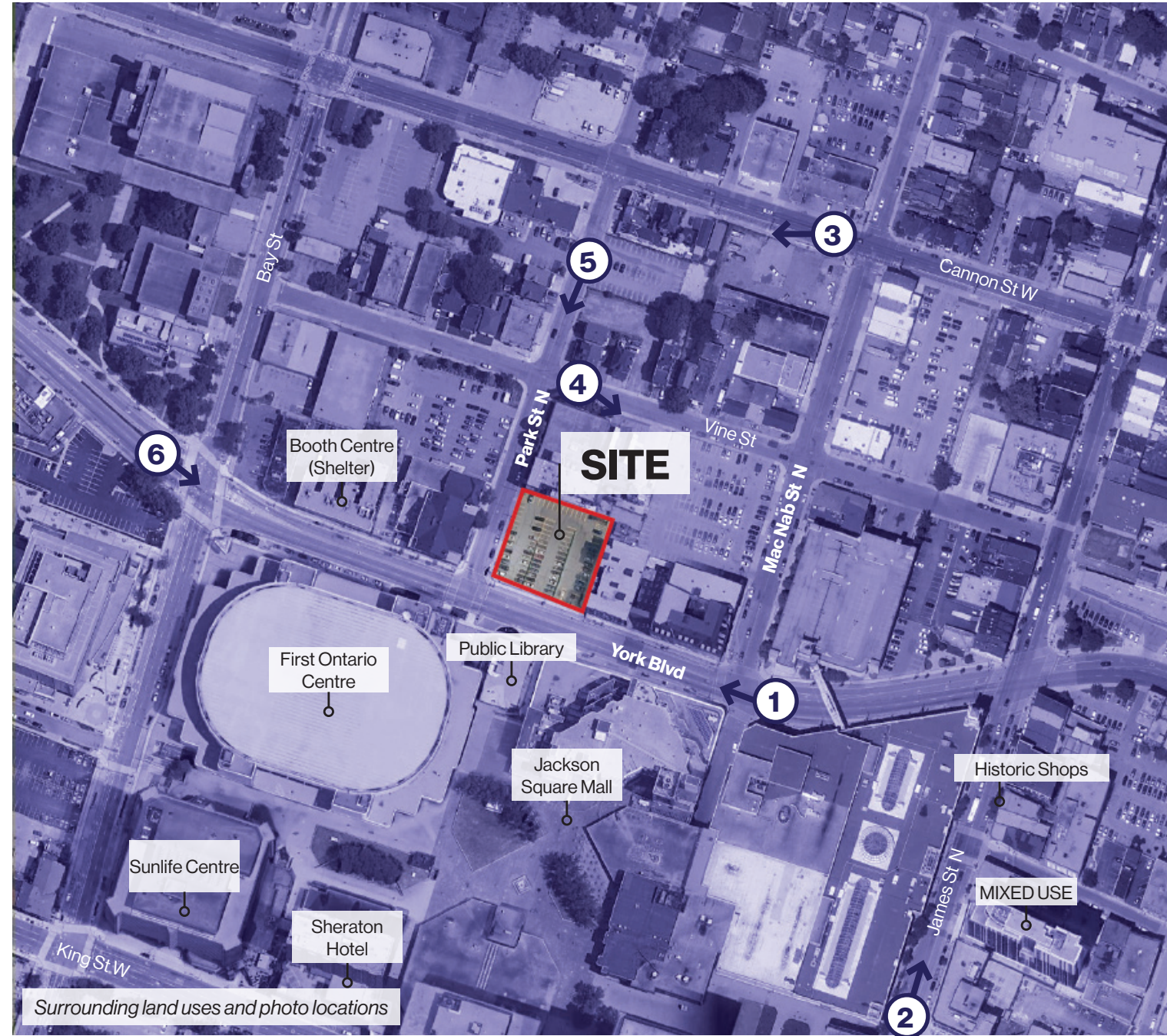
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Immediate Context

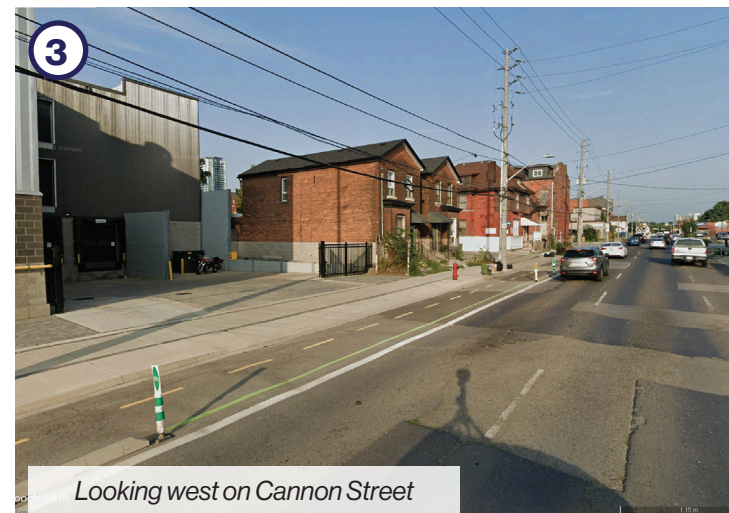
The Site is located within the central business district of Hamilton, diagonally across York Blvd and Park St from First Ontario Centre, and opposite the Jackson Square Mall and Public Library. The site is largely severed from the commercial uses of King and James street by the large inactive frontages of the mall and First Ontario Centre. Residential uses are located to north of the site. Overall, the surrounding neighbourhood has an auto-oriented character, with the majority of private land dedicated to parking.



Looking west on York Blvd



Looking north on James St N



Looking west on Cannon Street



Looking East on Vine St



Looking south on Park St N



Looking east on York Blvd

Site conditions and surrounding context

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Urban Design Context

From an urban design perspective, the local context includes a diverse mix of architectural styles. The Coppley Building was built by Scottish stone masons over a decade prior to Confederation. Dating from the 19th century, it was built in a style characteristic of the Scottish Regency period with rounded windows along the ground floor, and rectangular windows on the upper floors.

Other buildings in the area reflect the contemporary style of when they were built. Marca College, and the Market Library are both contemporary in style, and the modern First Ontario Centre is about to undergo a major renovation to bring its architectural style up to date.



Site conditions and surrounding context

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Design Brief

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28

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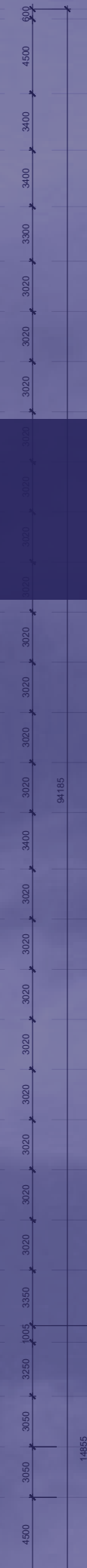
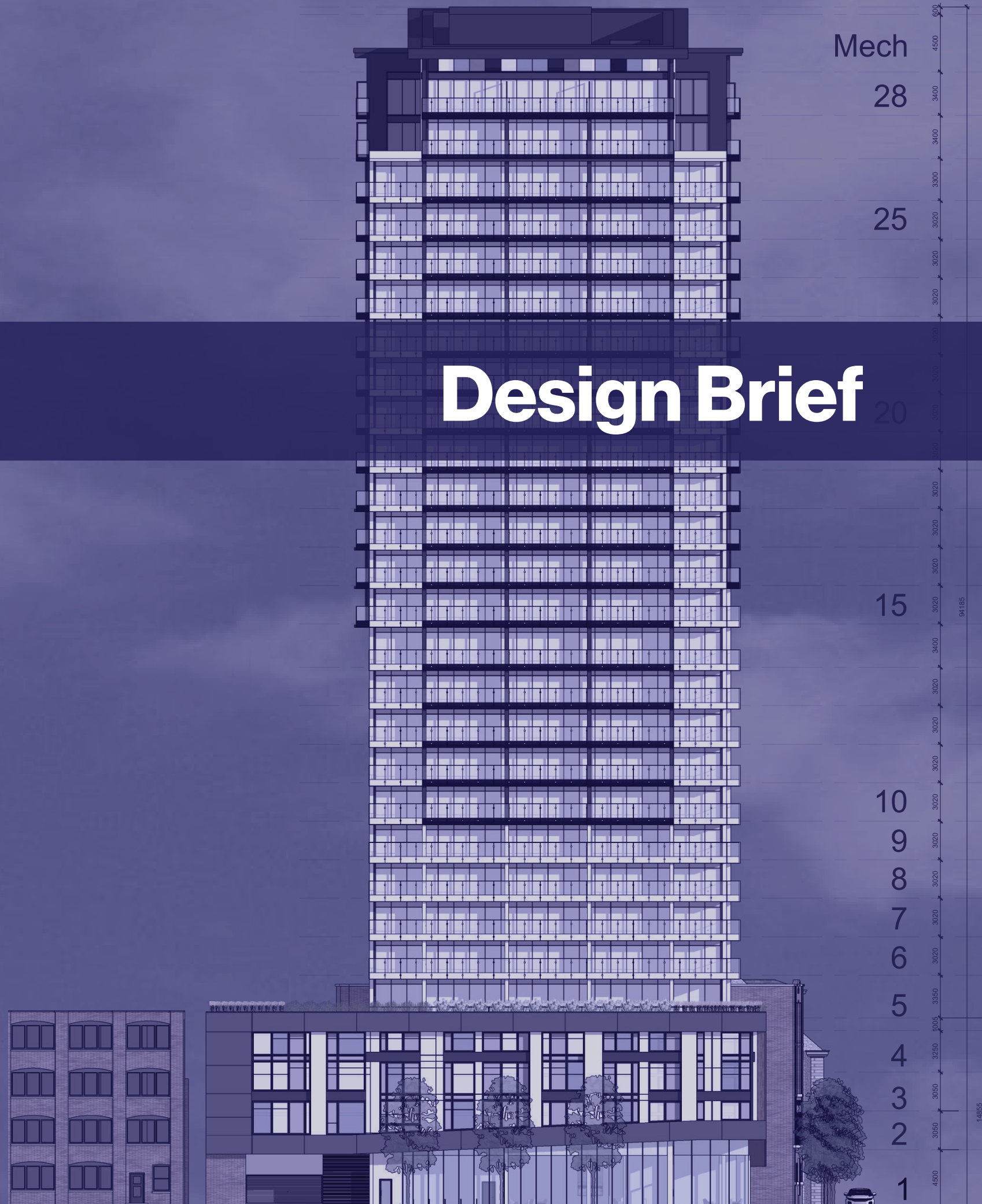
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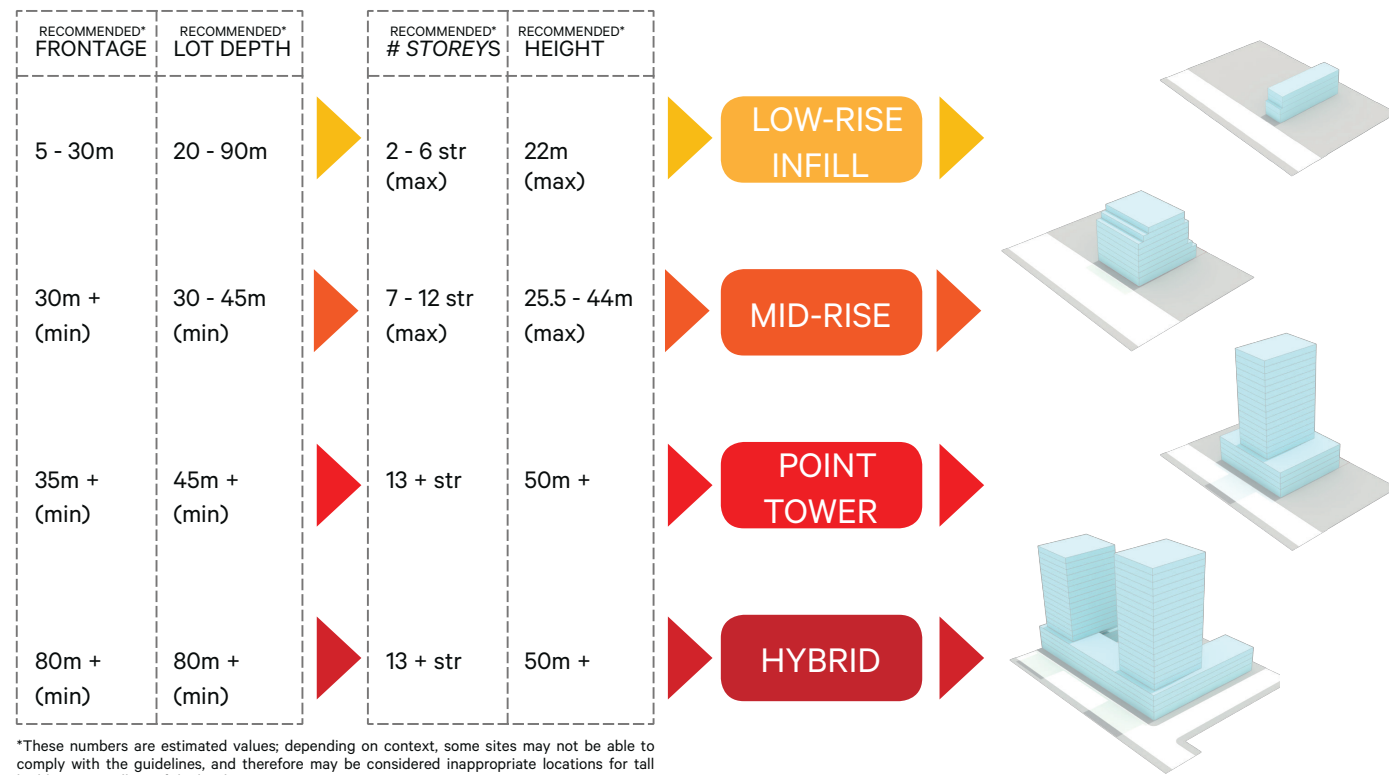
Response to Tall Building Guidelines

The Downtown Hamilton Tall Building Guidelines implement the design direction for buildings over 12 storeys in Downtown Hamilton. The Guidelines have been referenced as a set of best practices and expectations for this proposal.

The Guidelines provide direction related to contextual considerations, building form and articulation, and public realm relationships in the arrangement and design of tall buildings. The following sections of the Guidelines are relevant and referenced as part of the proposed design: Neighbourhood transition (3.2), Vibrant streets (3.4), Transit proximity (3.5), Site organization and building base (4.2), Building tower (4.3), Streetscape and landscape design (5.1).

The Guidelines also provide the following recommended minimums for point tower development:

Point Tower Site Dimensions 35m x 45m
 Point Tower Height 13+ storeys, 50m+



Site Dimensions for Tall Buildings (City of Hamilton)

York Boulevard Corridor

The Downtown Hamilton Tall Building Guidelines also establish the vision for York Boulevard as a corridor. While the site is located on the north side of the boulevard and not considered part of the York Boulevard corridor area, the proposal has built on the Tall Buildings Guidelines relating to the York Boulevard Corridor as best practice. This sets out a series of design priorities including:

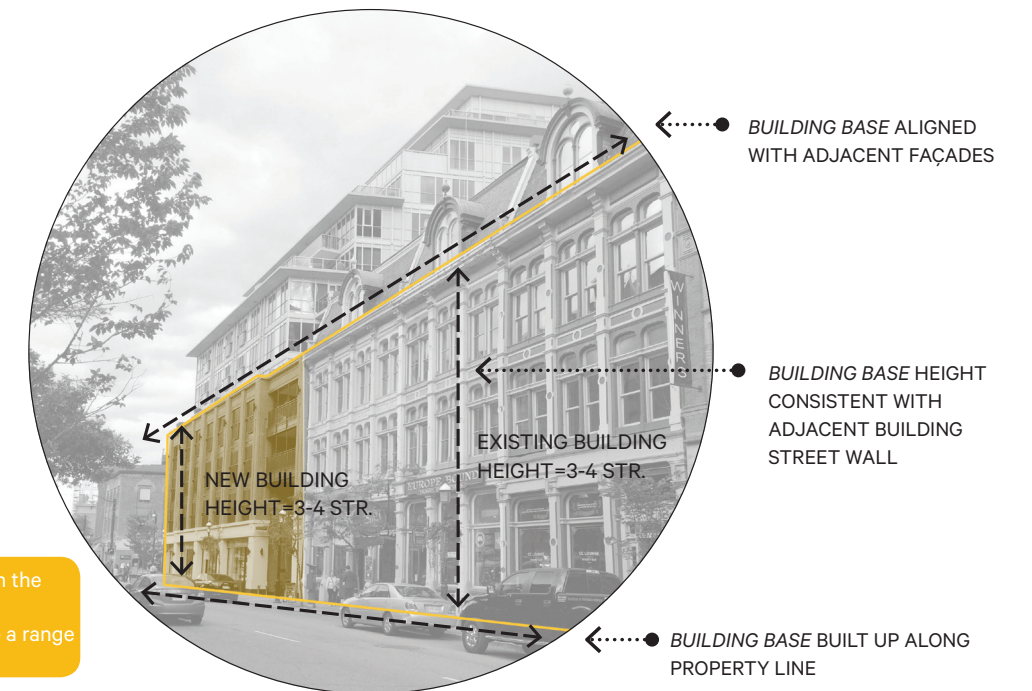
- Build to the street line
- Surface parking should not abut York Blvd
- Street-oriented uses at grade
- Compatibility with neighbouring typologies
- Taller buildings to create gateway feature

Encourage:

- **building base heights** that are in keeping with the scale of adjacent buildings and uses
- **higher ground floor heights** to accommodate a range of uses over time

Avoid:

- **base heights** that exceed the height of the *right-of-way*



Building Base to align with Context (City of Hamilton)

Design Brief

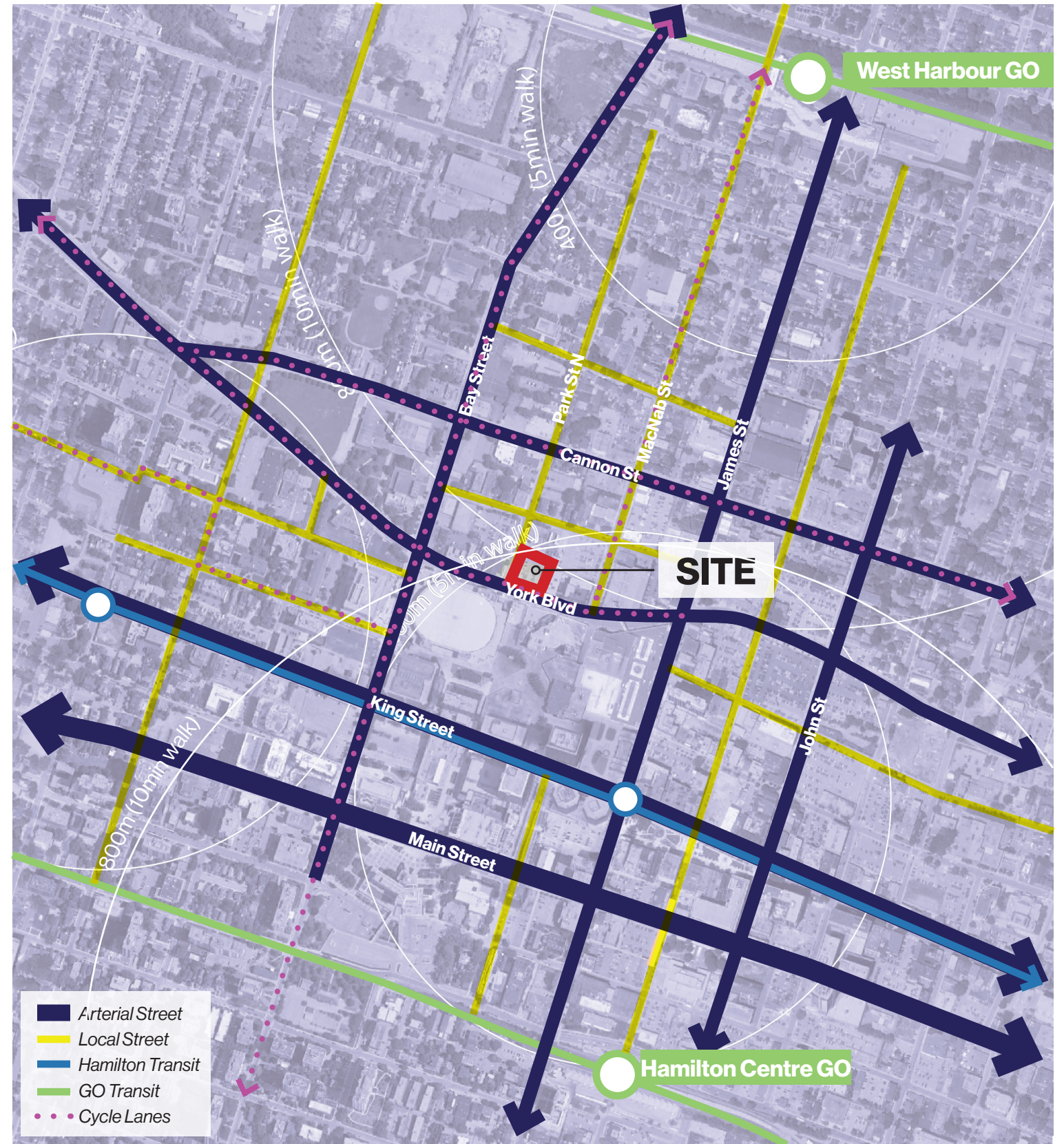
58 York Boulevard, Hamilton

D2021
 February 8, 2024
 Design Review Panel Submission Package

Contextual Analysis : Transportation Networks (Mobility)

The site is well located to mass transit. It is within an 800m (10 min) walk from both the Hamilton Centre, and the West Harbour GO Stations. It is within a 400m (5min) walk of the future LRT stop at King and James. The site is well connected to the road network. York Boulevard is an arterial road, as are Bay and James streets.

The local street network is based on a fine-grained grid network and is highly connected by foot to all of the downtown area. The site is also highly connected to the cycling network, with dedicated cycle lanes on York, Bay, and Cannon, and a shared cycle route along MacNab Street.



Contextual Analysis: Urban Structure

The Site is located within the Downtown Hamilton Secondary Plan Area (OPA 102) and is designated:

Official Plan

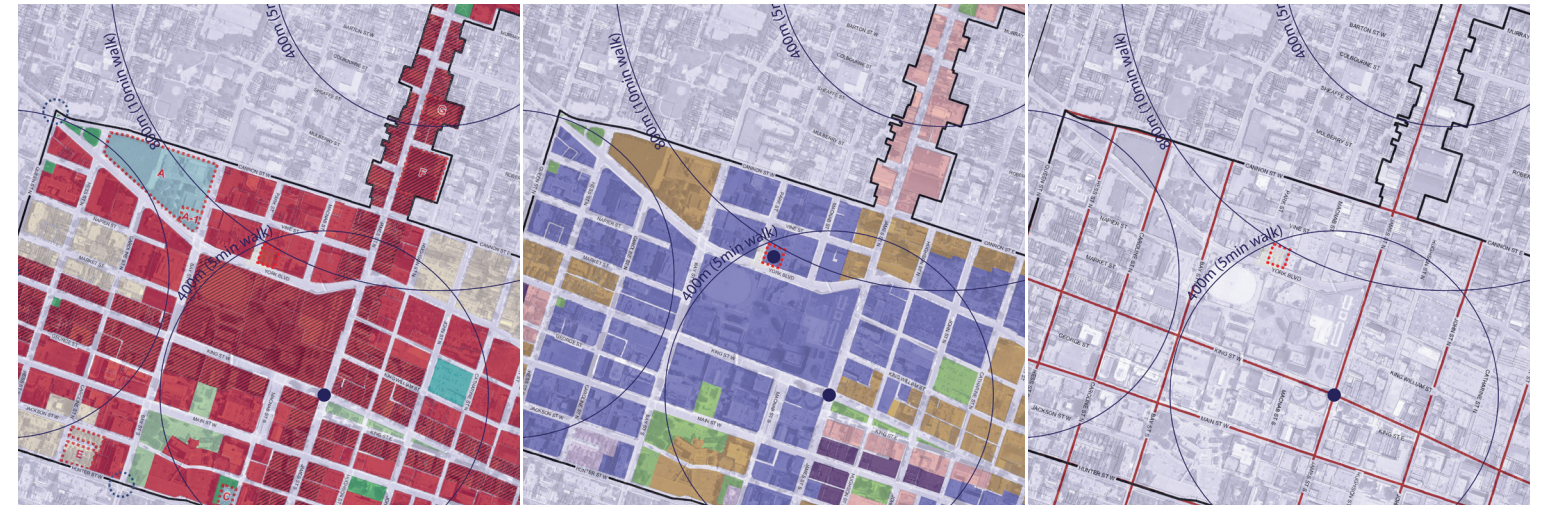
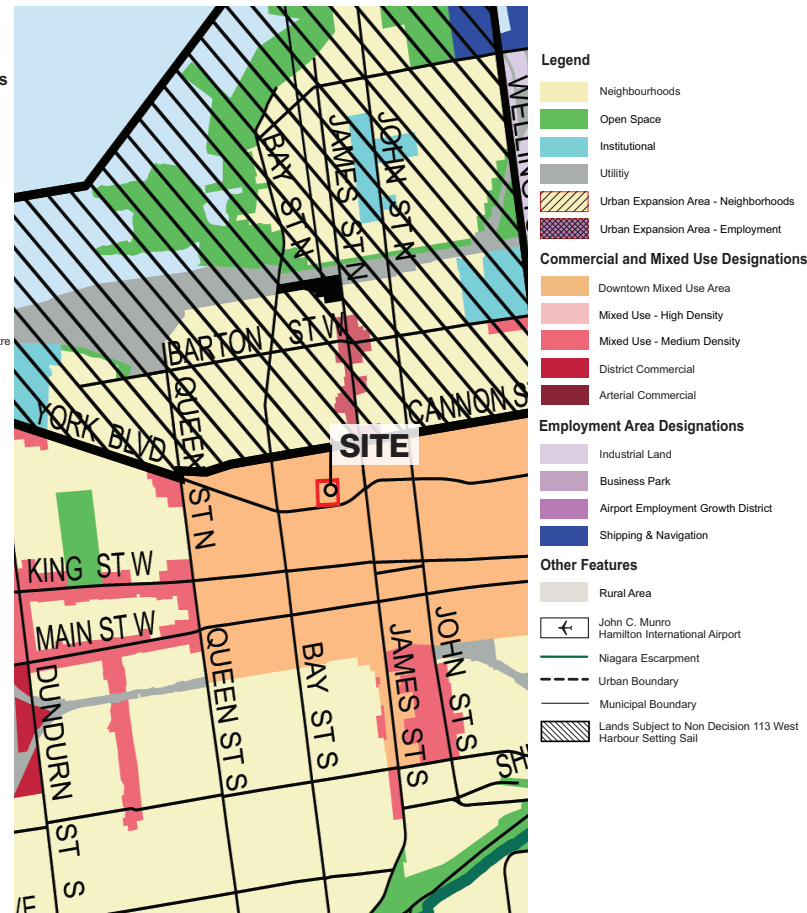
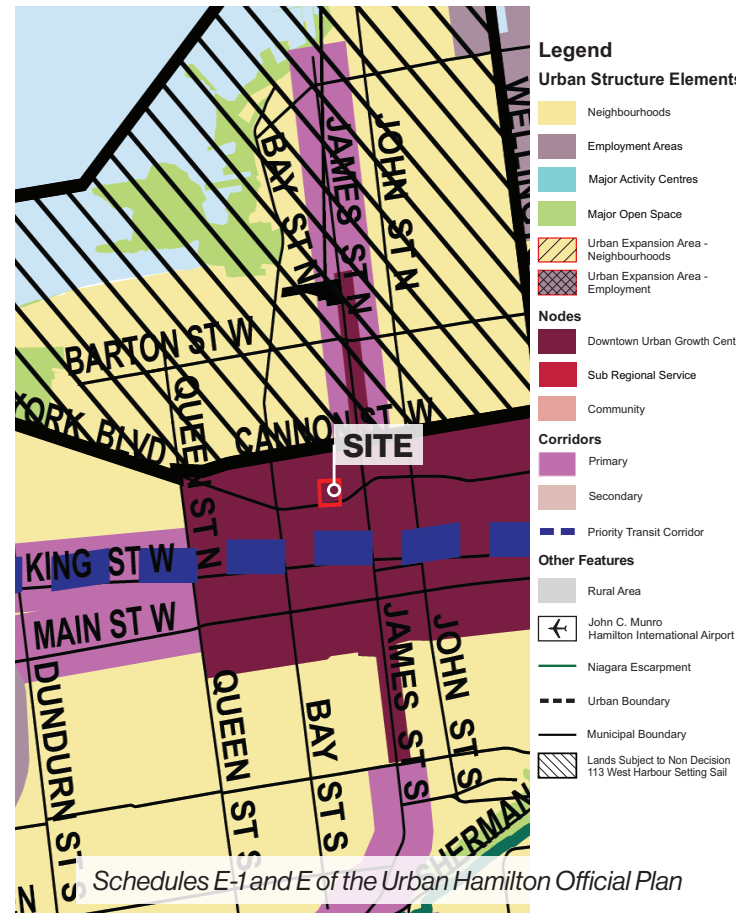
- Downtown Urban Growth Centre
- Pre-eminent node of Hamilton
- Major employment centre
- Area to accommodate 30% of new population
- Major transit hub with 2 GO stations and LRT
- Mixed Use High Density

Downtown Hamilton Secondary Plan

- Preeminent node for scale, density, range of uses and functions
- Location for tall buildings

These designations combine to create the following performance standards:

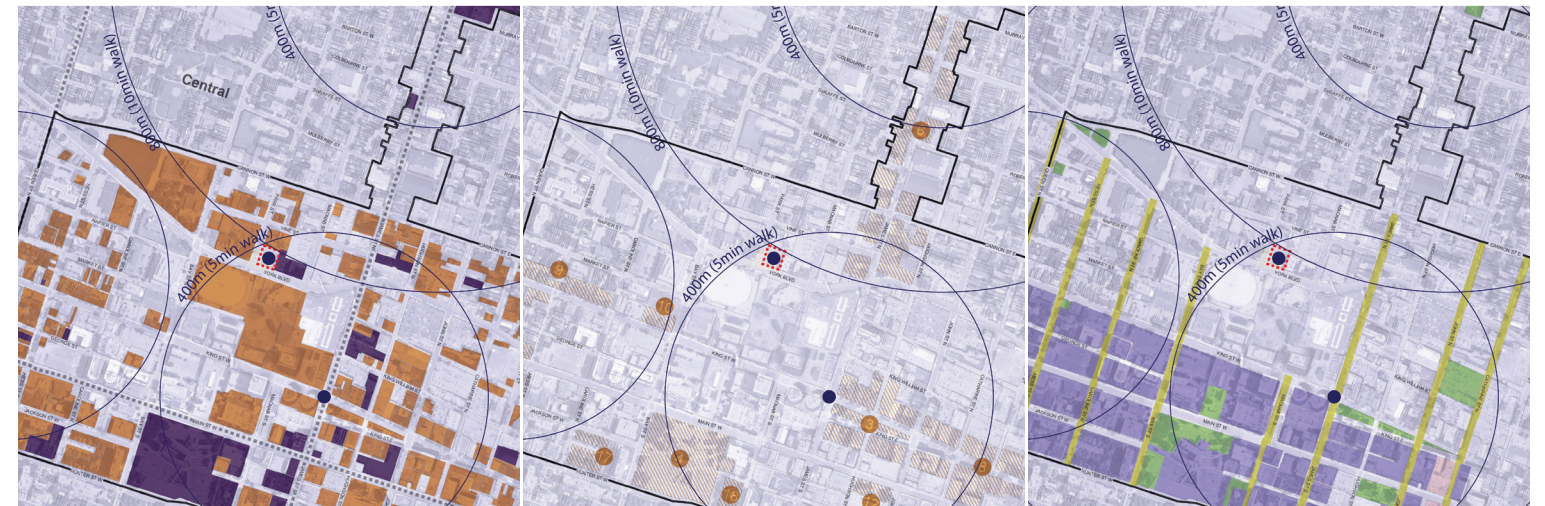
- Maximum Height - 30 storeys, and not taller than 191.2m (the Niagara Escarpment)
- Building base to respect the scale and built form character of the existing context
- Minimum of 500 persons and jobs per hectare.



Land use: Site is designated mixed use, high density development

Building Heights: Site is designated for 30 storeys

Height of escarpment = 191.2m or 28 storeys for 58 York Blvd..



Heritage: Site is adjacent to a Heritage designated property (56 York) and a registered non-designated property (84 Park)

The site is located away from the important cultural heritage landscapes within Hamilton.

The site is located to the north of any blocks where views to the Niagara Escarpment might be impacted by development.

Downtown Hamilton Secondary Plan Maps in relation to site

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D2021
February 8, 2024
Design Review Panel Submission Package

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Height Analysis & Surrounding Proposals

The context to the north is characterized by low-rise commercial, light industrial, and residential development. The area to the south, around the corner of King and James, contains the primary cluster of tall buildings in Hamilton. This area contains a diverse mix of low-, mid-, and high-rise buildings. Existing heights range from 1 storey industrial buildings, to 43 storey apartment towers.

89 Park Street N, located to the west of the site, has a proposal for two 30 storey towers with a 5 storey podium.



15 storey Offices James Street



43 storey apartments on Main Street E



41 Wilson (Source IBI Group)



Proposed 30-storey tower at 89 Park St N
SOURCE: <https://www.iconarchitects.ca/>



77 James (SRM Architects)



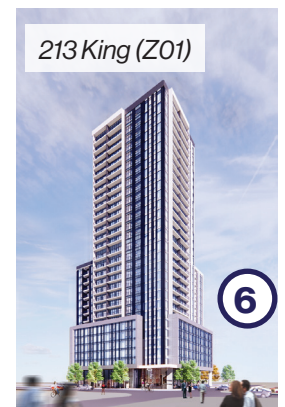
80 John (McCallum Sather)



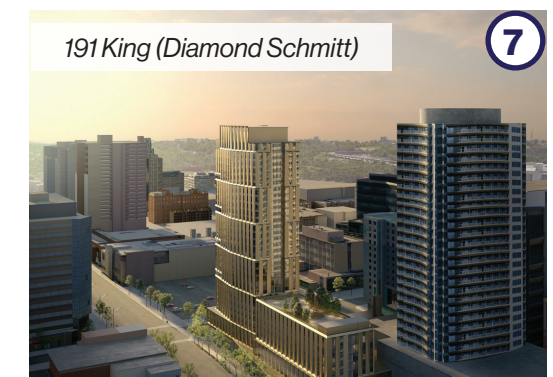
Tower cluster: 19, 26 and 16 storey towers On King St W.



108 James (MacCallum Sather)



213 King (Z01)



191 King (Diamond Schmitt)

Design Brief

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Design Vision

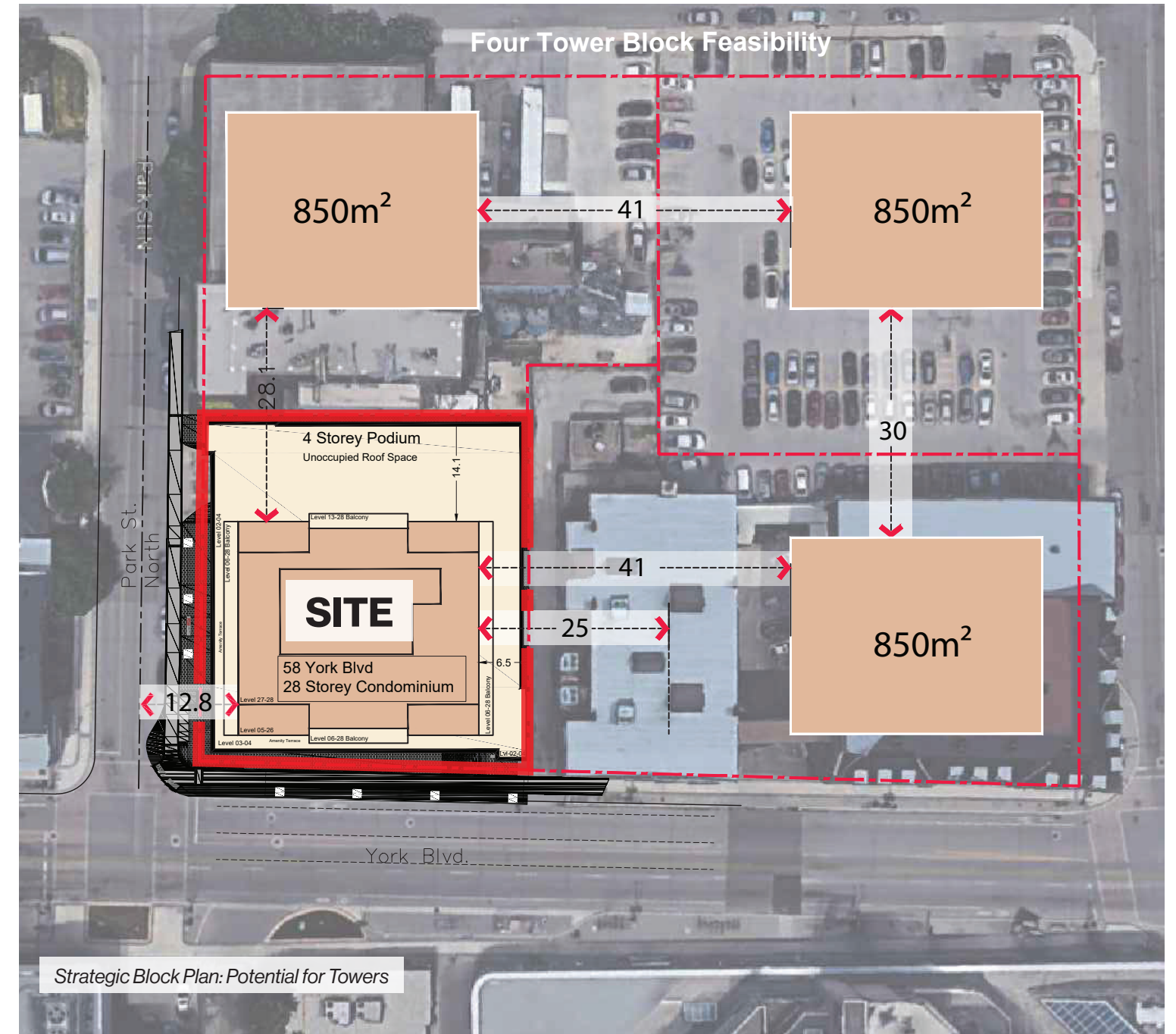
In response to the location and planned context of the Site, SRM and the consultant team developed a set of design principles to guide the site plan's development. The guiding principles are as follows:

Build on the Downtown Secondary Plan, incorporating high density, tall buildings within existing historic context

Create a tower and podium form that align with the Secondary Plan height limits with relation to the Niagara Escarpment.

Design starts at the block level, building on surrounding context

Use of 850m² floor plate based on potential for large (40 m & 30m) setbacks between towers, and limited potential for tower development within the block.



Design Vision

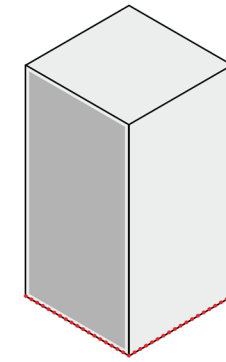
The guiding principles continued:

Develop a point tower form with setbacks from Park St and York Blvd to create base that aligns with adjacent heritage buildings

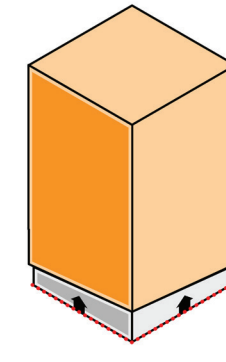
Create active urban streetscape character along Park St, and York Blvd through a continuous street wall and ground floor commercial. Lift “fabric” of podium facade to emphasize main entrance and commercial use.

High ground water table, and ground water contamination constrain underground parking to one level. Parking incorporated in upper podium. Above ground parking is screened to appear as occupied base, using 1m depth of design elements to create character

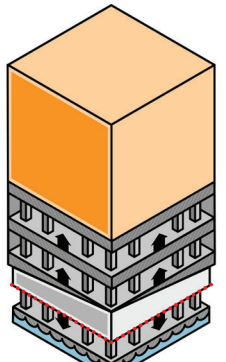
Build on contemporary architectural character of York Blvd through use of contemporary materials and architectural language that act as a complimentary counterpoint to neighbouring heritage buildings.



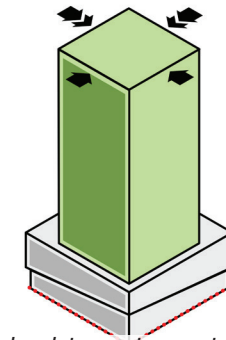
1. Work within height limits



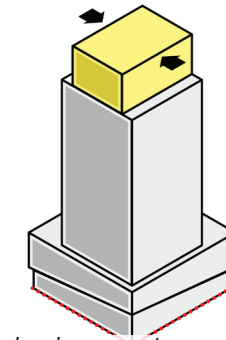
2. Lift fabric at corner to improve pedestrian environment



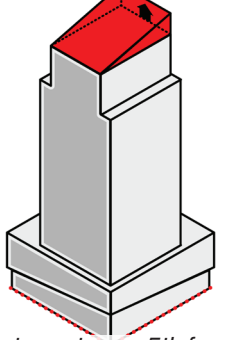
3. Underground parking limited by high water table. Parking in podium



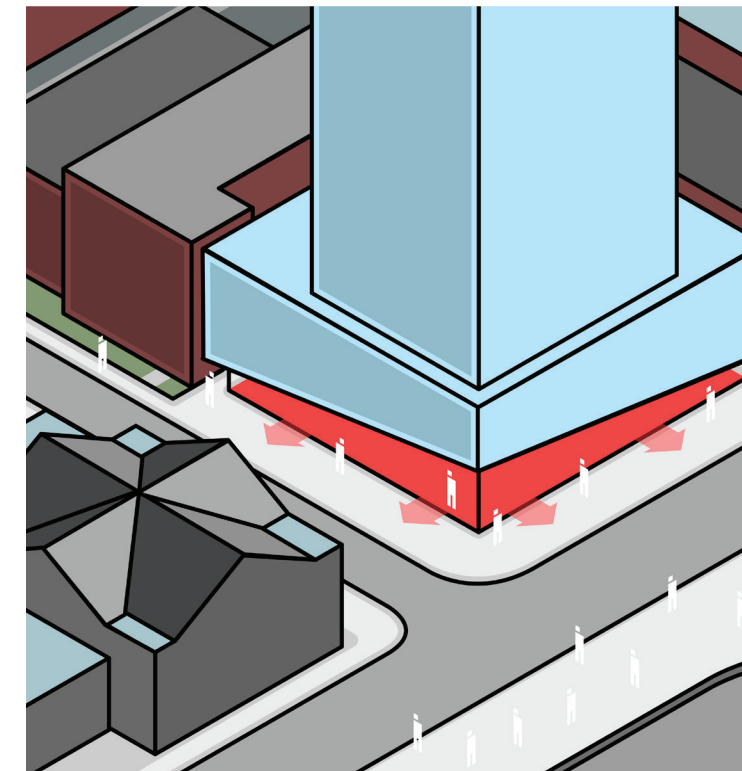
4. Step-back tower to create continuous street-wall



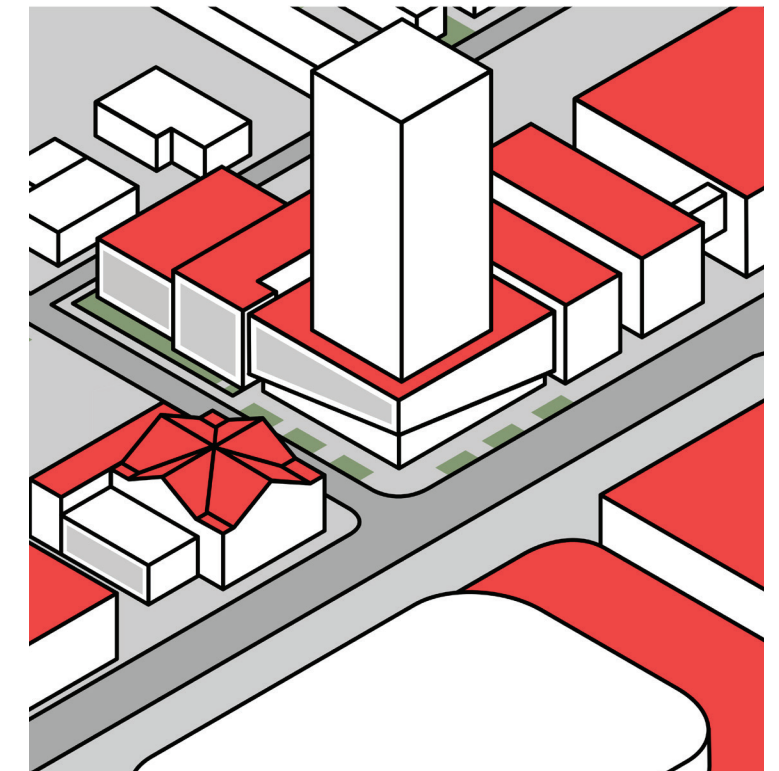
5. Step-back upper storeys to create a feature



6. Design tower top as 5th facade



7. Ground floor provides active street frontage and passive surveillance


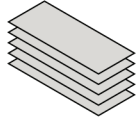




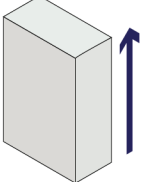



8. Podium height aligns with surrounding context

Project Summary

The Proposed Development combines the principles of the design vision to create a 28-storey mixed use building with a 4-storey podium base. The Proposed Development contains 344 dwelling units, 120 square meters of ground floor commercial. 122 vehicle parking spaces and 186 bicycle parking spaces are provided for residents and visitors, as well as 3440 square meters of amenity space for resident use.

The Proposed Development will contribute to the transformation of Downtown Hamilton, helping to revitalize the area as a vibrant urban city centre, accessible for pedestrians and by mass transit.

	58 York Blvd		23,102m² GFA 120m² Commercial GFA
	0.19ha (site) 43.28m (York frontage) 45.57m (Park St frontage)		122
	344 units		172 Long term cycle parking spaces 10 Short term cycle parking spaces
	28 Storeys (94m) 4 Storey Podium		3440m² private balconies 137m² outdoor amenity area



Proposed Development
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

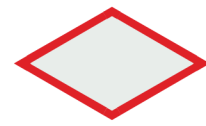
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Built Form & Massing

A podium and point tower form is utilized to create a massing that fits within the Site as well as the existing and proposed context.

The Site's size meets the minimum recommended dimensions for a point tower, as prescribed in the Tall Building Guidelines. The tower footprint of 850 m² exceeds the Guidelines' of 750 square meters, however the accompanying shadow and wind studies demonstrate a minimal impact on the surrounding properties. Given the density targets for the area, and the limited potential for tower development in this block (see strategic block plan) the design team sees a larger tower floor plate as beneficial in this case.

The proposed height of 28 storeys conforms with the Downtown Hamilton Secondary Plan and the Tall Building Guidelines. The tower height is 2 storeys less than the two proposed towers on 89 Park Street North.



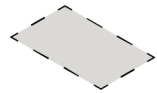
Site Area

Guideline Min.

35m x 45m

Proposed

43.28m x 45.57m



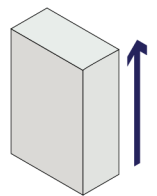
Tower Footprint

Guideline Max.

750sqm

Proposed

850m²



Tower Height

Guideline Min.

13 storeys & 50m

Secondary Plan Max.

30 Storeys

Proposed

28 Storeys



Podium aligns with heritage context to create street wall along Park St N

Proposed Development

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Tower Design

The design builds on the downtown secondary plan, incorporating a high density, tall building within the site. The tower height (28 Storeys) is within the maximum height permitted for the site, based on the site's relation with the Niagara Escarpment, as set out in Downtown Hamilton Secondary Plan Policy 6.1.4.14.

The tower is stepped back from the podium edge on all four sides. The placement on site is based on an analysis of the context at the block scale. A point tower form is utilized, with a 850 square meter floor plate. This size of floor plate is justified on the limited potential for future tower development within the block because of the inheritance of heritage designated buildings, and the ability to maintain large (40 m & 30m) setbacks between potential future towers on neighbouring properties.

The white and black panels on the tower create a lighter design to reduce the perceived size. The towertop is set-back from the roof edge and designed to comply with the Hamilton Tall Buildings Guidelines policy 4.4, creating a 'fifth façade', with a distinctive presence on Hamilton's skyline. The tower top references elements from the podium design, repeating the sloped roof, and incorporating a similar materials pallet.



Proposed Development

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

SRM[↑]
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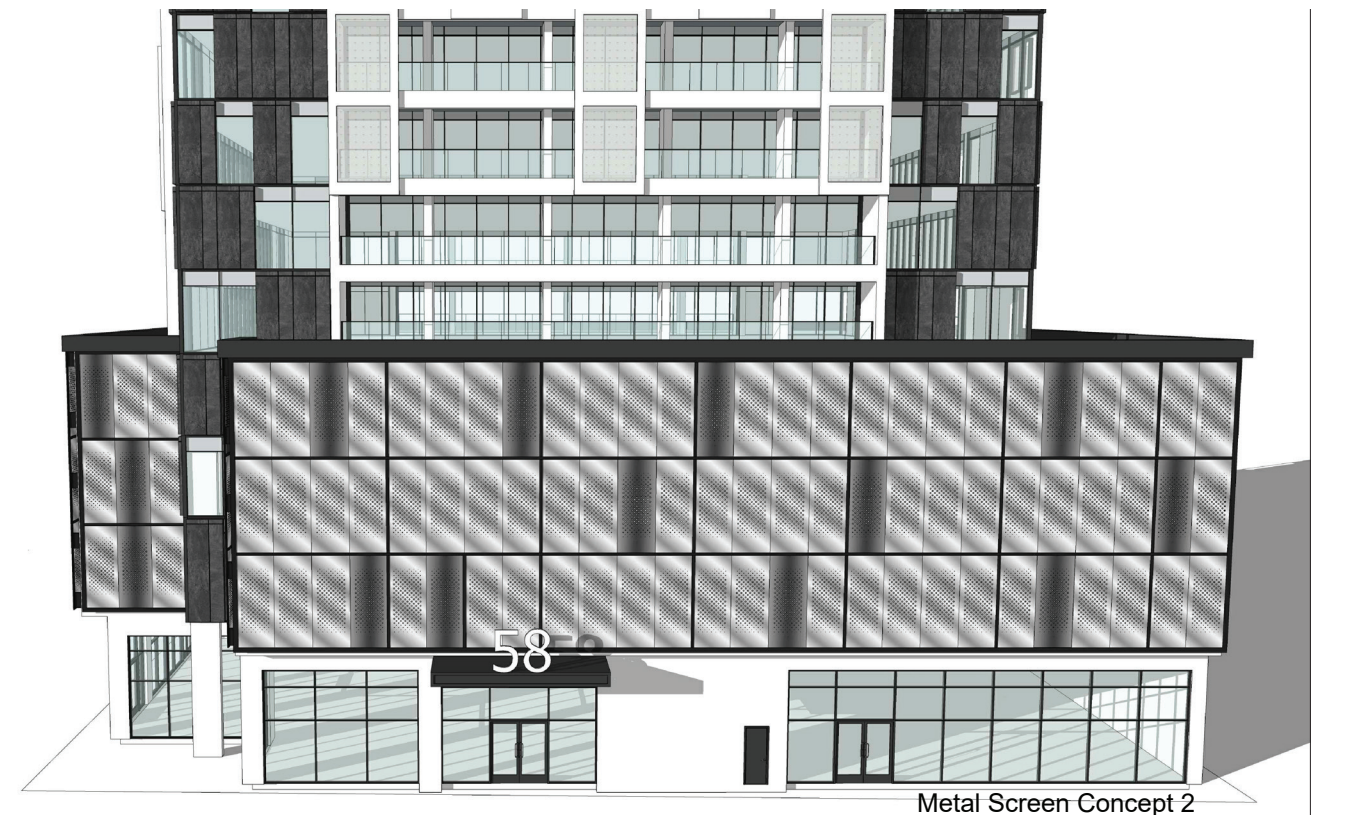
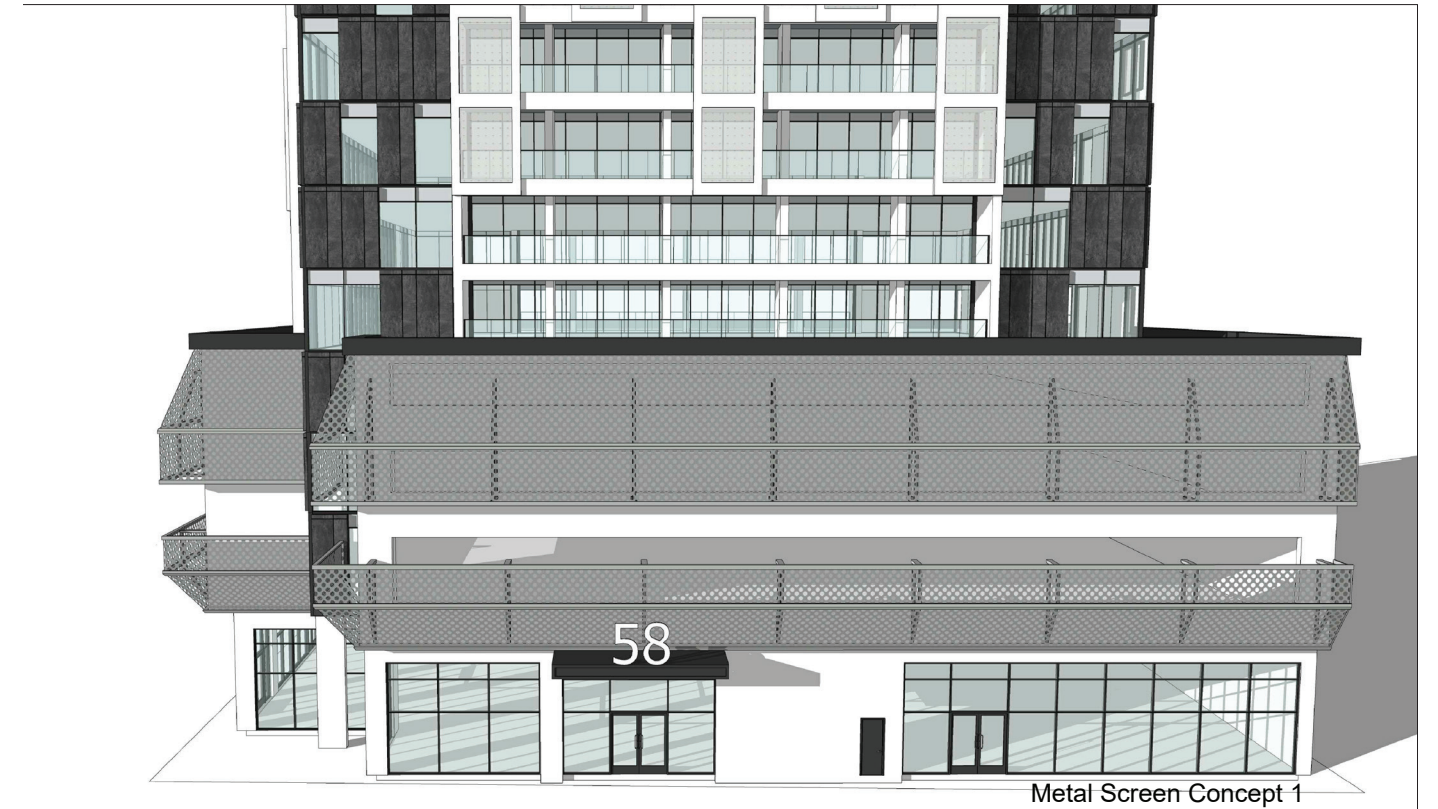
Podium Design Evolution

The massing of the Proposed Development remained consistent throughout the design process. The design for the podium has evolved in an effort to improve the urban street-wall condition by wrapping the podium level parking. The design began with strategies to wrap the above ground parking with a metal screen, similar to a number of well known architectural precedents.

After discussions with the city, this changed to create a facade with translucent glass panels that create the image of active uses on floors 2-4. These panels are back-lit at night to look like residential or commercial space, and help provide a perception of safety along the street.

The use of contemporary materials means the building is of its time, while fitting in with the surrounding context by aligning the podium height with the neighbouring heritage building on Park St N.

Finally the design evolved to provide an overhanging roof at the corner of Park St N and York Blvd. This detail helps to shelter the main entrance to 58 York from the rain and snow, and improves the legibility of the public realm.



Proposed Development

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

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Masonry Concept



Opaque Glass Screen



Floating Tilted Frame Base



Translucent Glass Panels with Dark Frame

Building Base Design

The podium is designed to create an active streetscape along York Blvd. The project proposes direct commercial and residential entrances from the sidewalk, creating a finer grain of uses and entrances along the street. A ground floor height of 4.5m and large windows allow passive surveillance (eyes on the street) from the commercial space, and communal ground floor spaces. The primary residential entrance is recessed to provide legibility and weather protection, while contributing to a degree of safety.

The ground floor of the podium along is recessed along, creating a protected streetscape, with the upper podium cantilevered over the exterior space. This allows greater visibility between the parking garage entrance and the street, improving pedestrian safety and reducing potential conflicts with vehicles.



Podium along York Blvd



Podium along Park St N



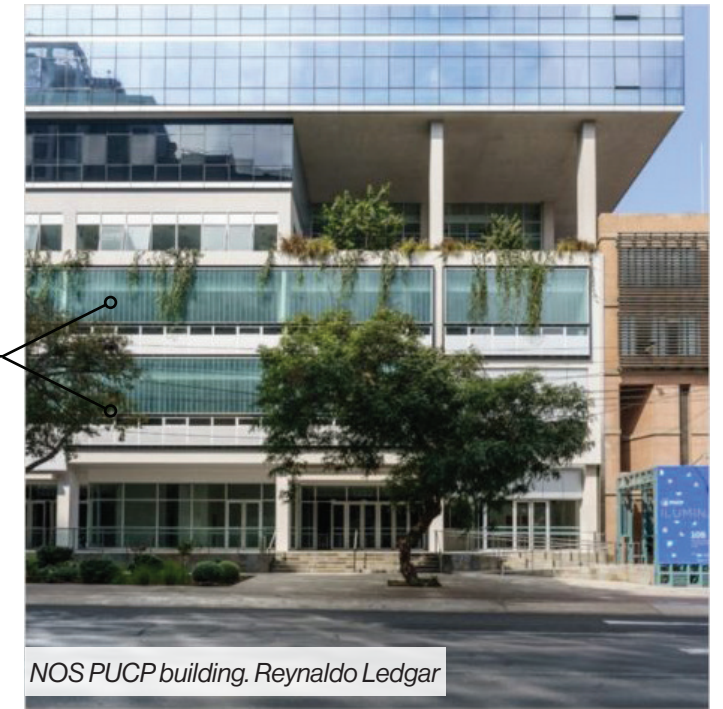
Podium along York Blvd

Building Base Design

The upper podium, levels 2-4, house above ground parking. This program is wrapped with a facade treatment that provides depth, uses materials that hide the parking, and give the sense that the upper podium is made up of active uses.

The podium height aligns with the adjacent heritage building at 80 Park St N, creating a continuous street wall.

Example of parking in podium made attractive by design.



Proposed Development

58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

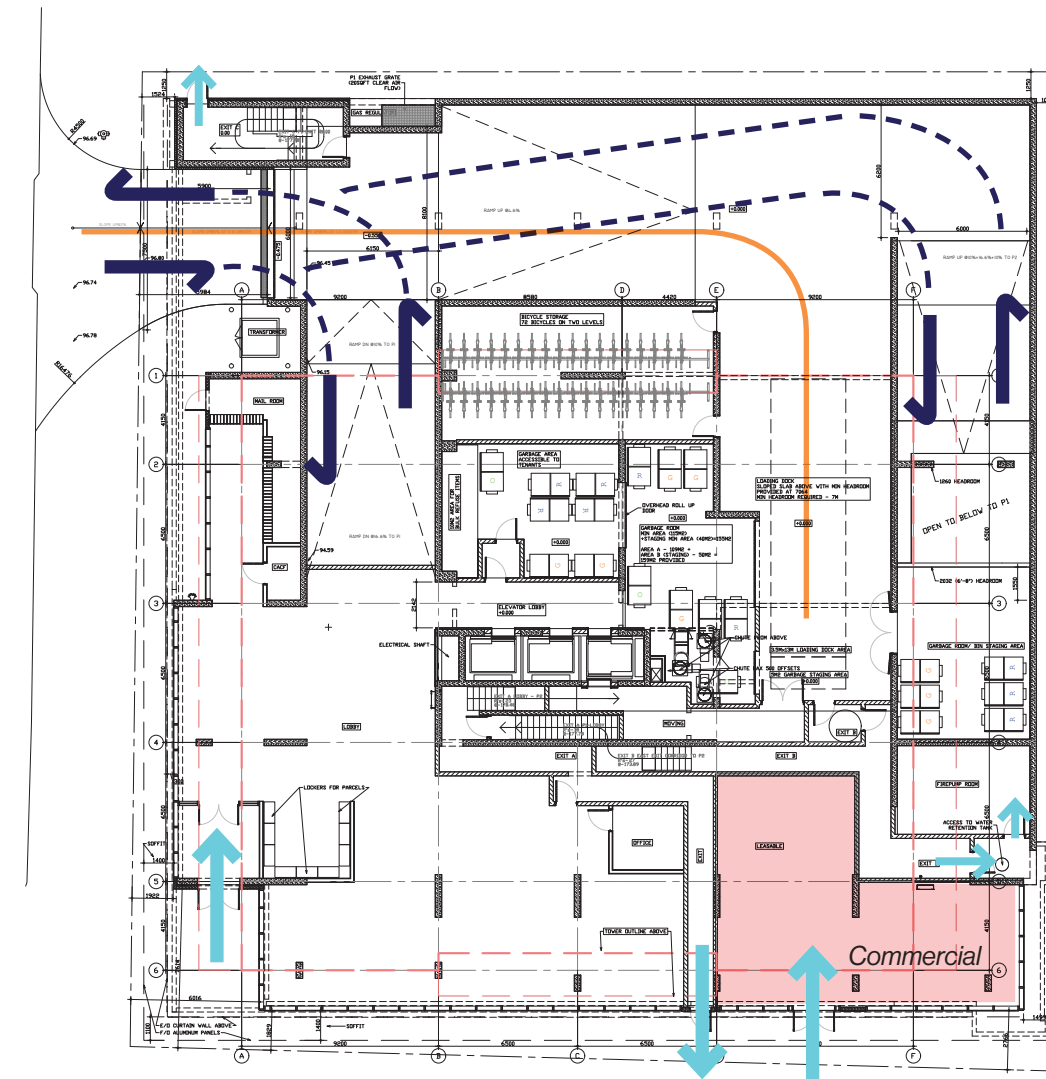
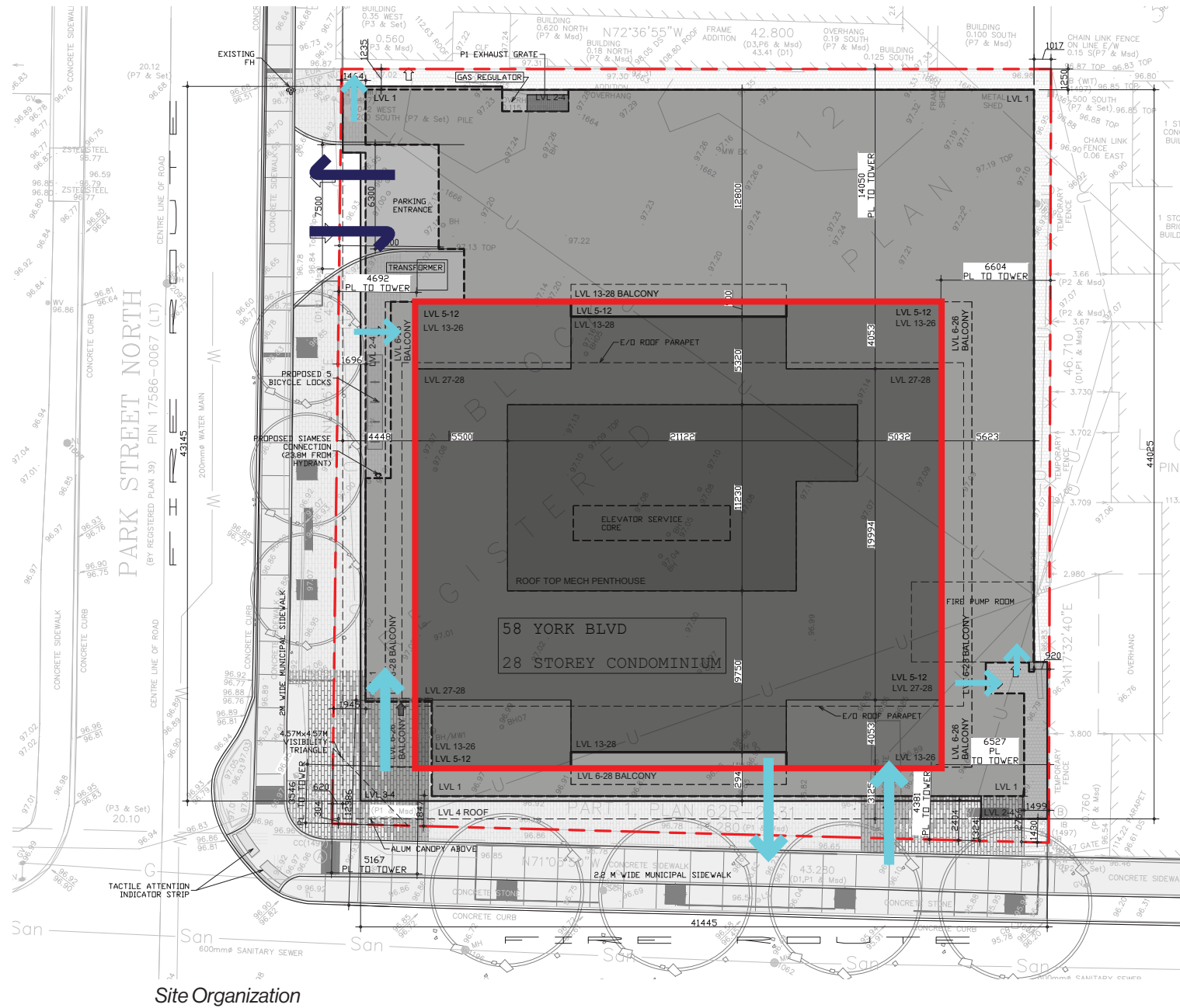
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Site Organization & Circulation

The proposed building is located on the corner of York Blvd and Park St N with an uneven ground-floor setback that ranges from 1.46m to 1.94m along Park, and 1.43 to 2.9m on York. The podium creates a strong streetscape, running along the majority of the street frontage, leaving a 0.92m side-yard setback to the east on York, and a 1.24m rear yard setback to the north off Park. The pedestrian entrance to the building lobby is located at the corner of York and Park with secondary entrances / egress routes off of both Park Street and York. The primary entrance is emphasized by recessing the facade and lifting the corner of the building fabric. The building scale is broken up at the pedestrian level by the presence of the commercial area, which has a separate entrance off York. Short-term bicycle

parking is provided near the primary entrance with long-term parking located securely within the building envelope.

Servicing is accessed via the northwest corner of the site off Park Street N, with waste storage located inside the building. Resident parking is also accessed from this point. Inside, passenger vehicles go to underground and podium parking.



- Pedestrian
- Vehicle
- Service & Loading

Proposed Development
58 York Boulevard, Hamilton

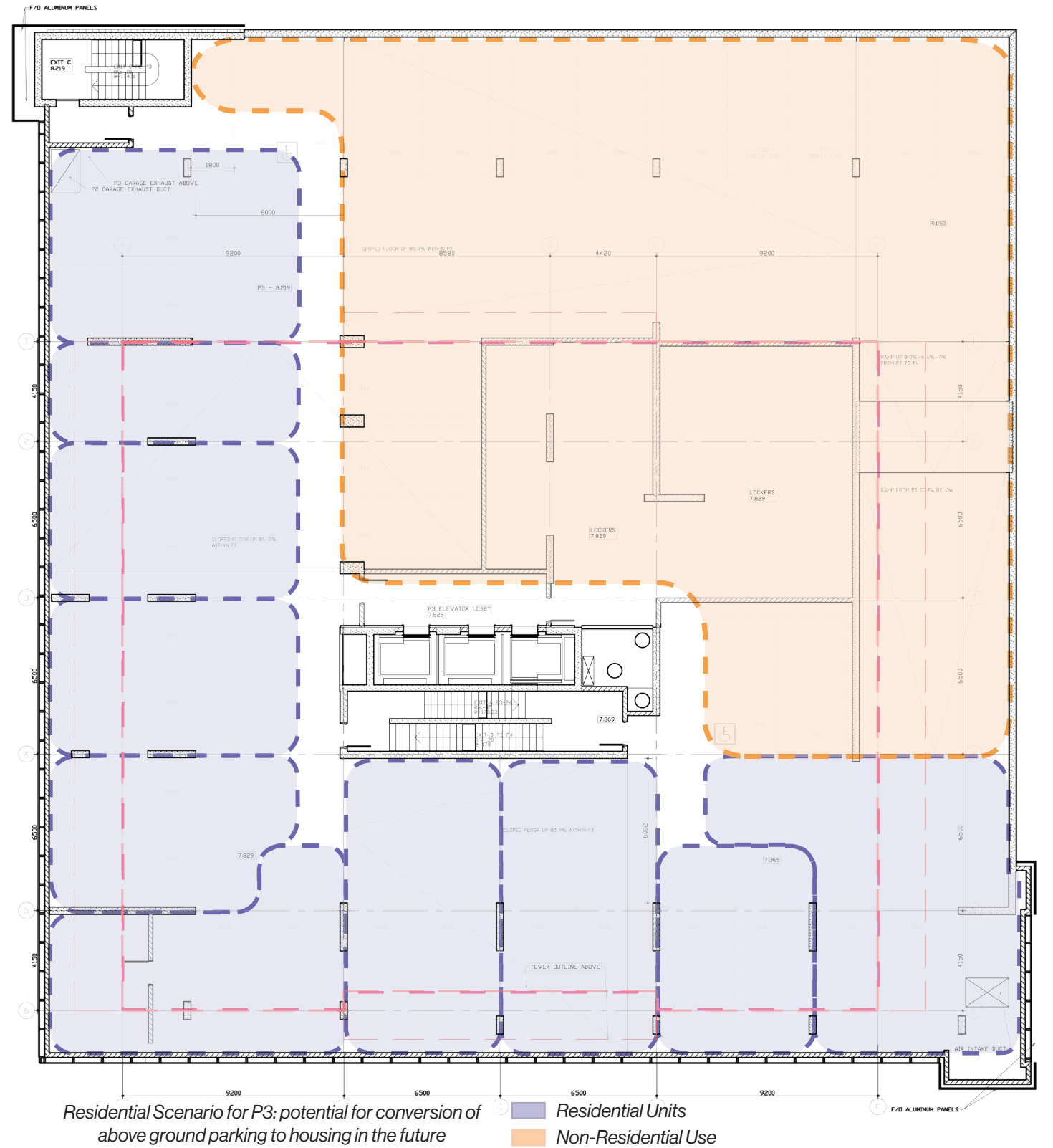
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An Adaptable Solution

The parking areas in the podium have been designed to allow for their future conversion to residential use in the potential scenario that parking is no longer desirable on site, either because of the availability of autonomous vehicles, or because of future regulations in relation to the climate crisis. In this long-term scenario, the 3 above ground parking levels could be converted to residential units, following the diagram shown on the right.

This possible future scenario would create an additional 10 units per floor on levels 2-4. As the underground parking level (P1) is below grade, it would not be suitable for conversion.



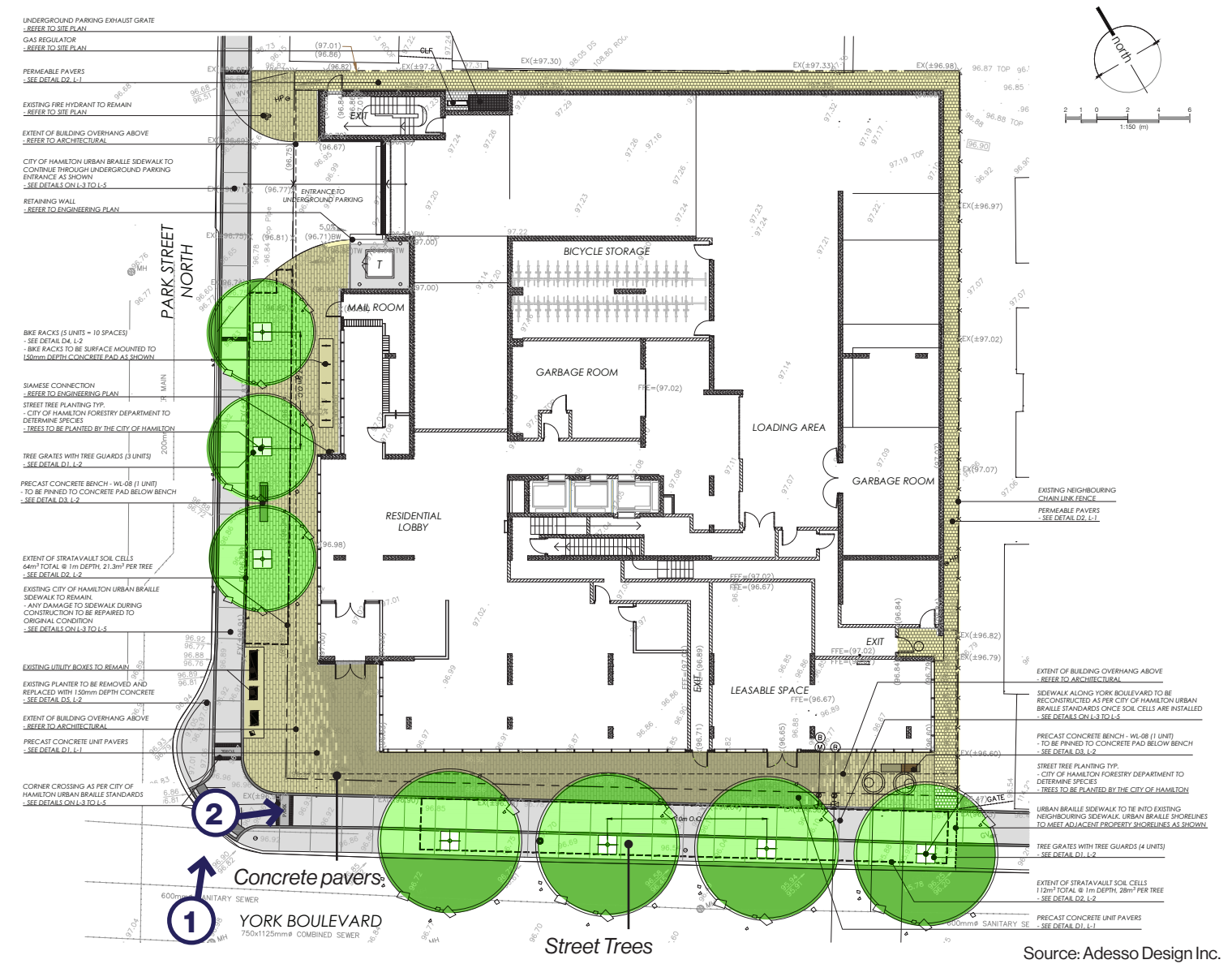
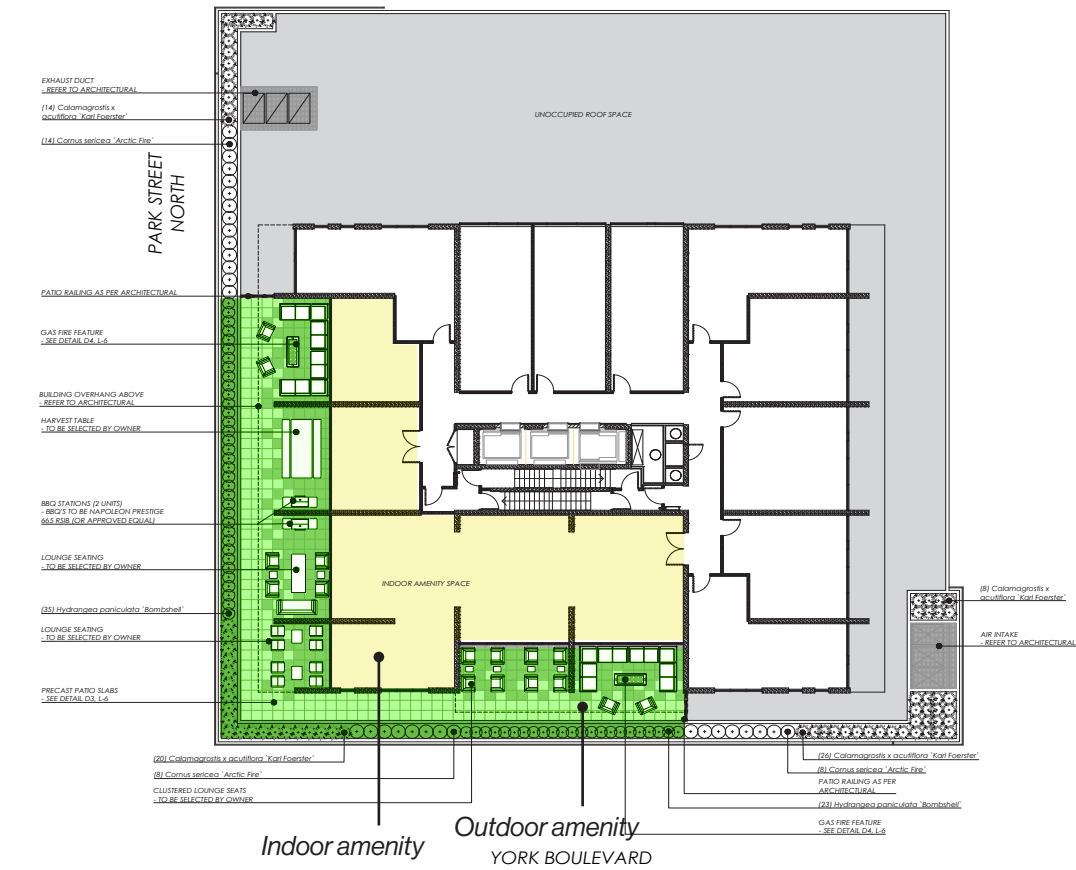
Landscape Design & Amenity Areas

The proposed Conceptual Landscape Plan provides direct sidewalk access for the residential lobby and ground floor commercial units, with hard wearing concrete pavers that act to widen the usable sidewalk area parallel to the York Blvd and Bay St N. The sidewalk marks the high traffic pedestrian circulation area along York Blvd and Park St N and drop-off area. Public benches, trees, and street furniture run in a line parallel to the sidewalk, creating a legible and attractive public realm for people.

Further landscaping details will be discussed in the landscape plan at the Site Plan stage. The design incorporates tree grills and space for 7 city planted trees along the street. Currently, there are no trees on site. The site also makes use of tactile pavers to highlight the entrance to the parking garage, and the pedestrian crossing at the corner of Park Street N and York Blvd.

Each unit has its own balcony with 10m² of amenity space. In addition, the 5th floor offers a shared outdoor rooftop amenity area, in conjunction with an indoor amenity area with access to common seating, outdoor tables, BBQ stations, and landscaping. The proposal meets the required amenity space requirement as per the zoning bylaw.

Additional concept landscape drawings are included in a separate document.



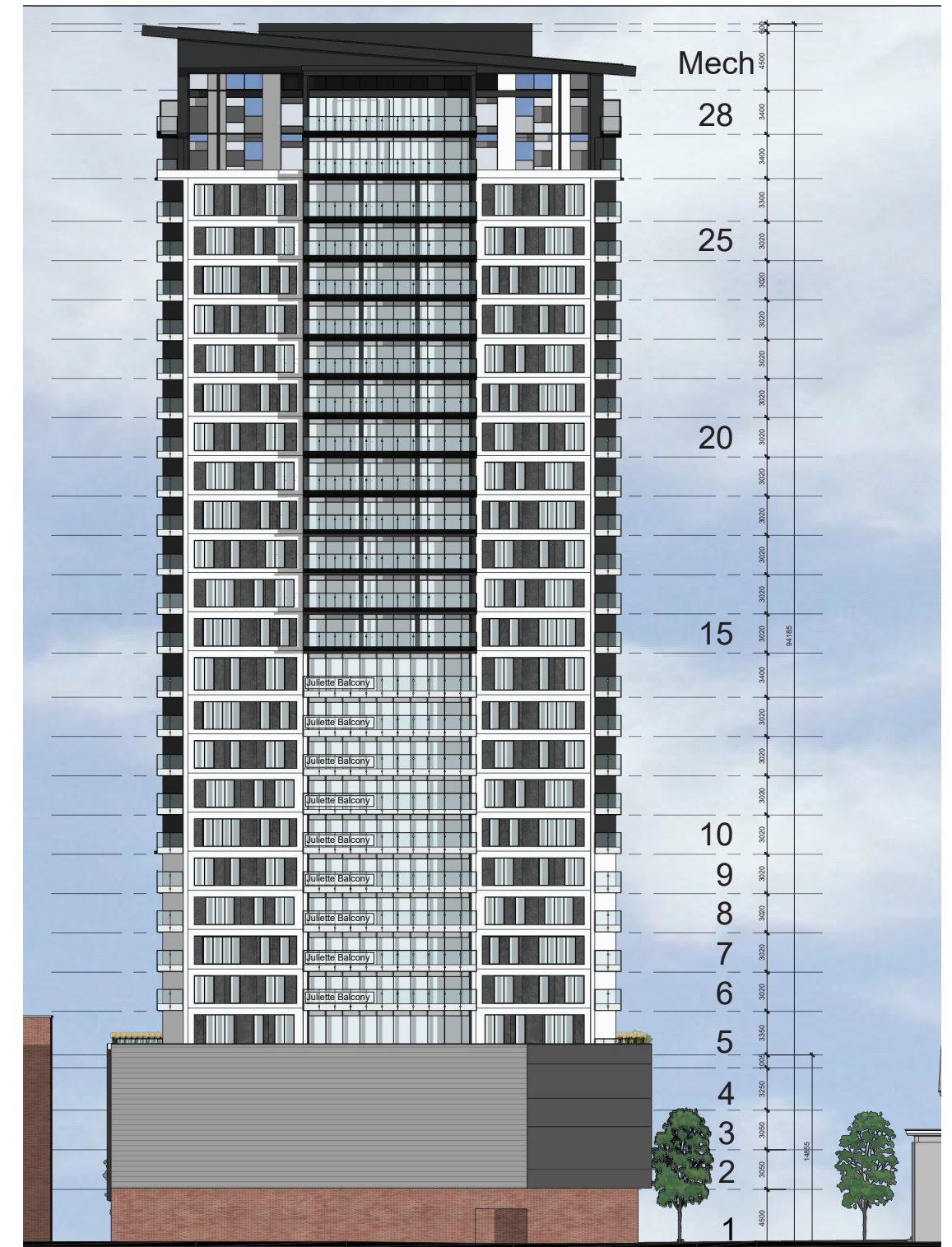
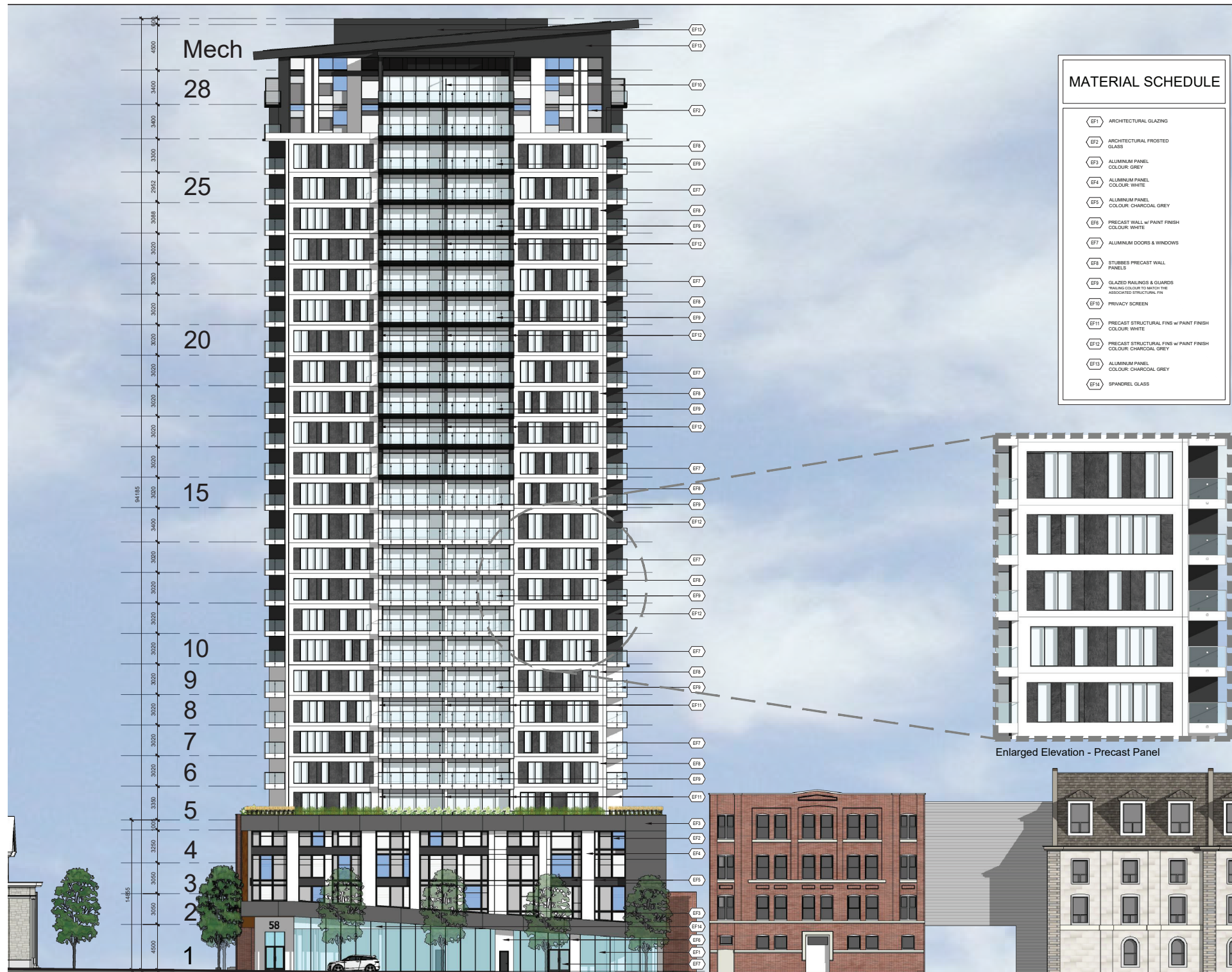
Source: Adesso Design Inc.



Proposed Development
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

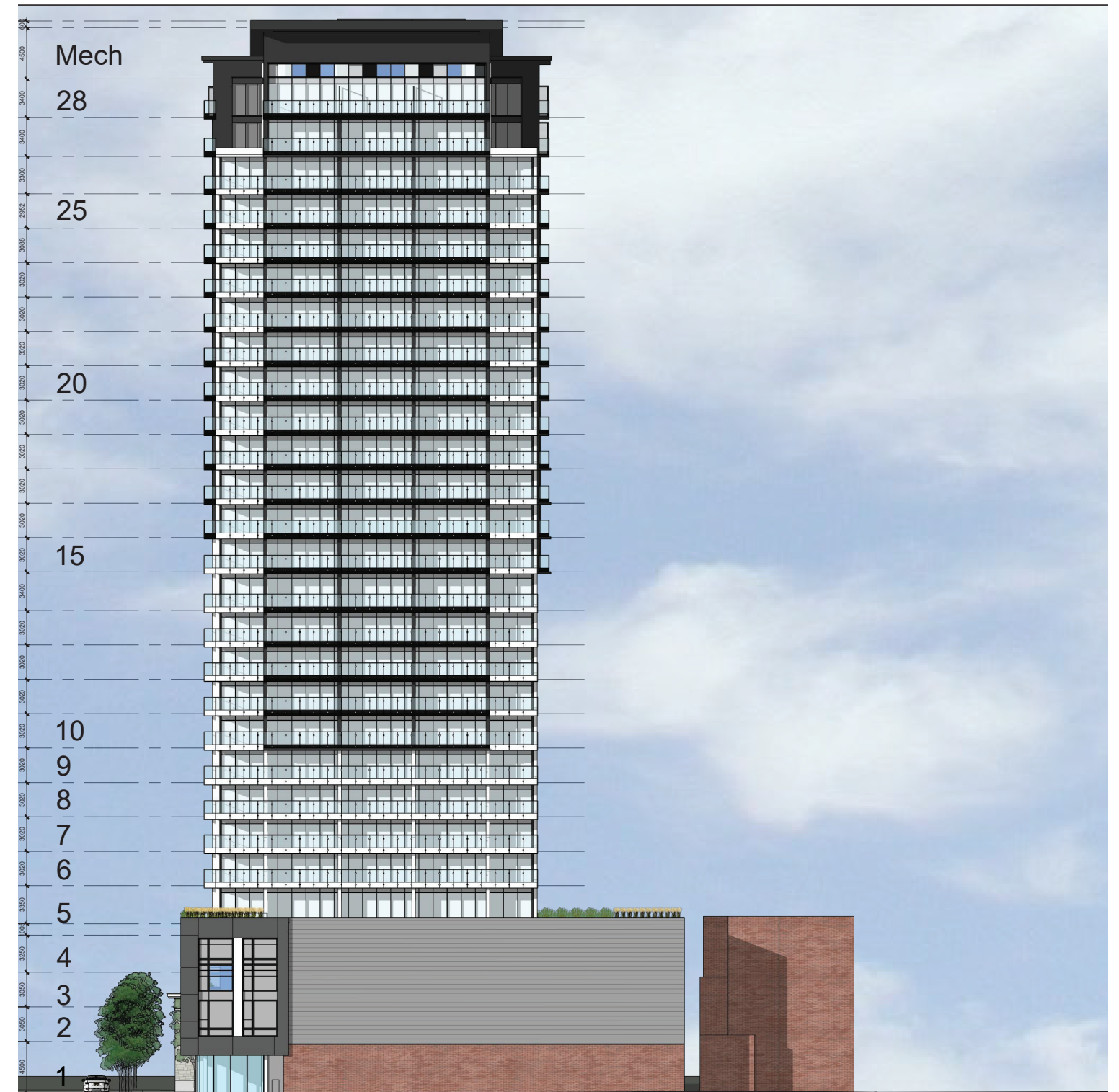
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Source: SRM Architects (December 14, 2022)

Elevations - South (L) & North (R)
 58 York Boulevard, Hamilton

D2021
 February 8, 2024
 Design Review Panel Submission Package



Source: SRM Architects (December 14, 2022)

Elevations - West (L) and East (R)
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package



Findings: Shadow Impact Study

The Shadow Impact Study prepared by GSP Group concludes that the proposed development will have minimal shadow impact as per the guidelines referenced in the City of Hamilton's Development Application Guidelines: Sun Shadow Study.

The Shadow Impact Study is included as a separate document.

Findings: Pedestrian Wind Assessment

The Pedestrian Wind Assessment prepared by SLR Consulting (Canada) Ltd concludes that Wind conditions on the sidewalks surrounding the proposed development, and on site, including entrances and outdoor amenity spaces, are expected to be suitable for the intended use year-round.

The Pedestrian Wind Assessment is included as a separate document.

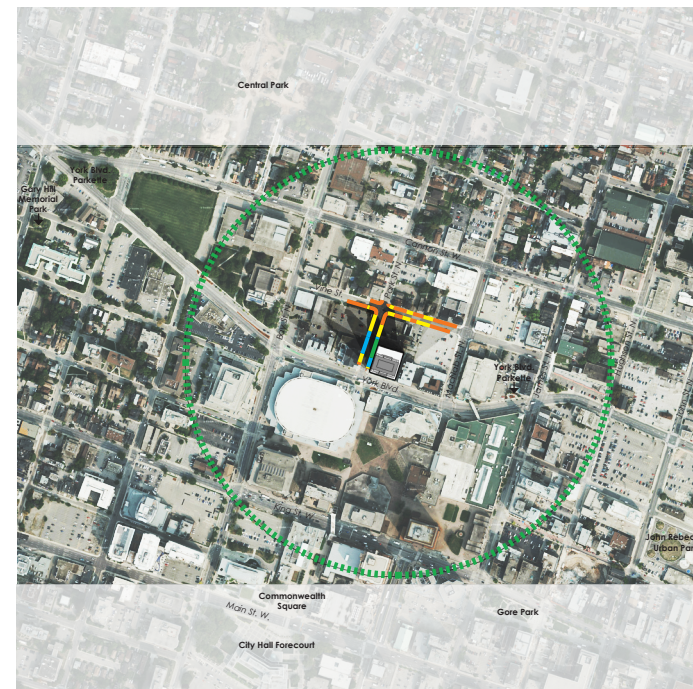
Sunlight Hours | Between 10am to 4pm



w Study | 58 York Blvd., Hamilton

Sun Shadow study findings, March (L) and September (R), GSP Group

Sunlight Hours | Between 10am to 4pm



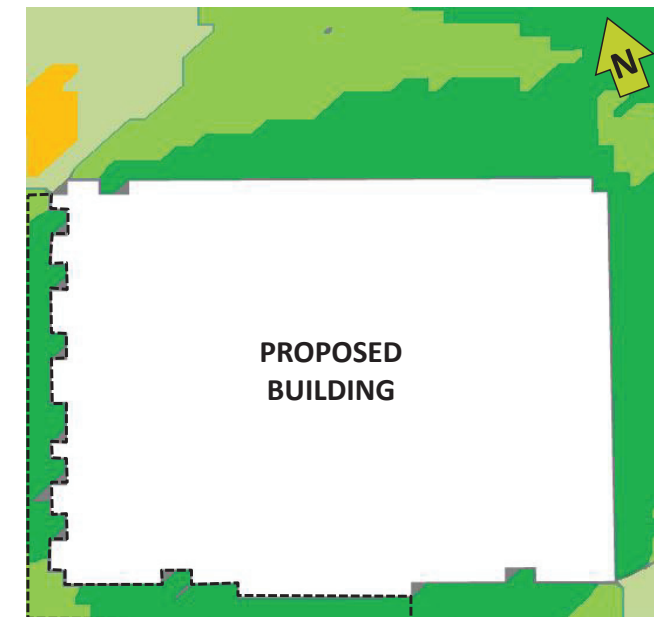
w Study | 58 York Blvd., Hamilton

Cultural Heritage Impact Assessment

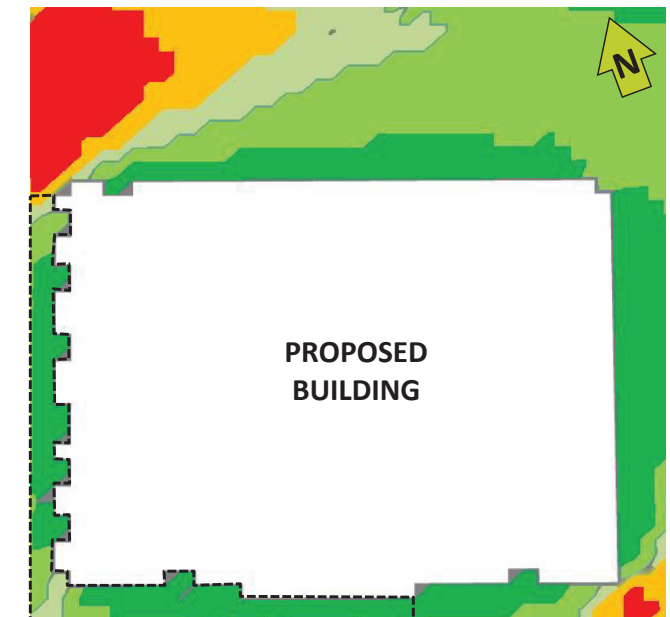
The Cultural Heritage Impact Assessment prepared by MHBC Planning, Urban Design and Architecture concludes the following:

1. The subject property is currently vacant and contains no cultural heritage resources, therefore, the proposed development will have no impact on cultural heritage resources on site.
2. Contiguous to the subject property are two heritage properties, the designate property at 56 York Boulevard to the east, and the listed property at 80 Park Street to the north. Recommendation for Temporary Protection Plan that would include a Vibration Monitoring Plan.
3. The CHIA concludes that the contemporary building design is compatible with the surrounding area and conforms to the cultural heritage policies of the Official Plan.
4. The CHIA concludes that shadowing is minimal and will not affect heritage attributes.
5. The listed heritage resources at 89 Park Street, 55 York Boulevard, and 101 York Boulevard are located across the street from the subject property. It is not expected that the proposed development will result in adverse impacts to these listed heritage properties.

The Cultural Heritage Impact Assessment is included as a separate document.



Sitting Walking Amenity Terrace
Standing Uncomfortable
Strolling



Sitting Walking Amenity Terrace
Standing Uncomfortable
Strolling

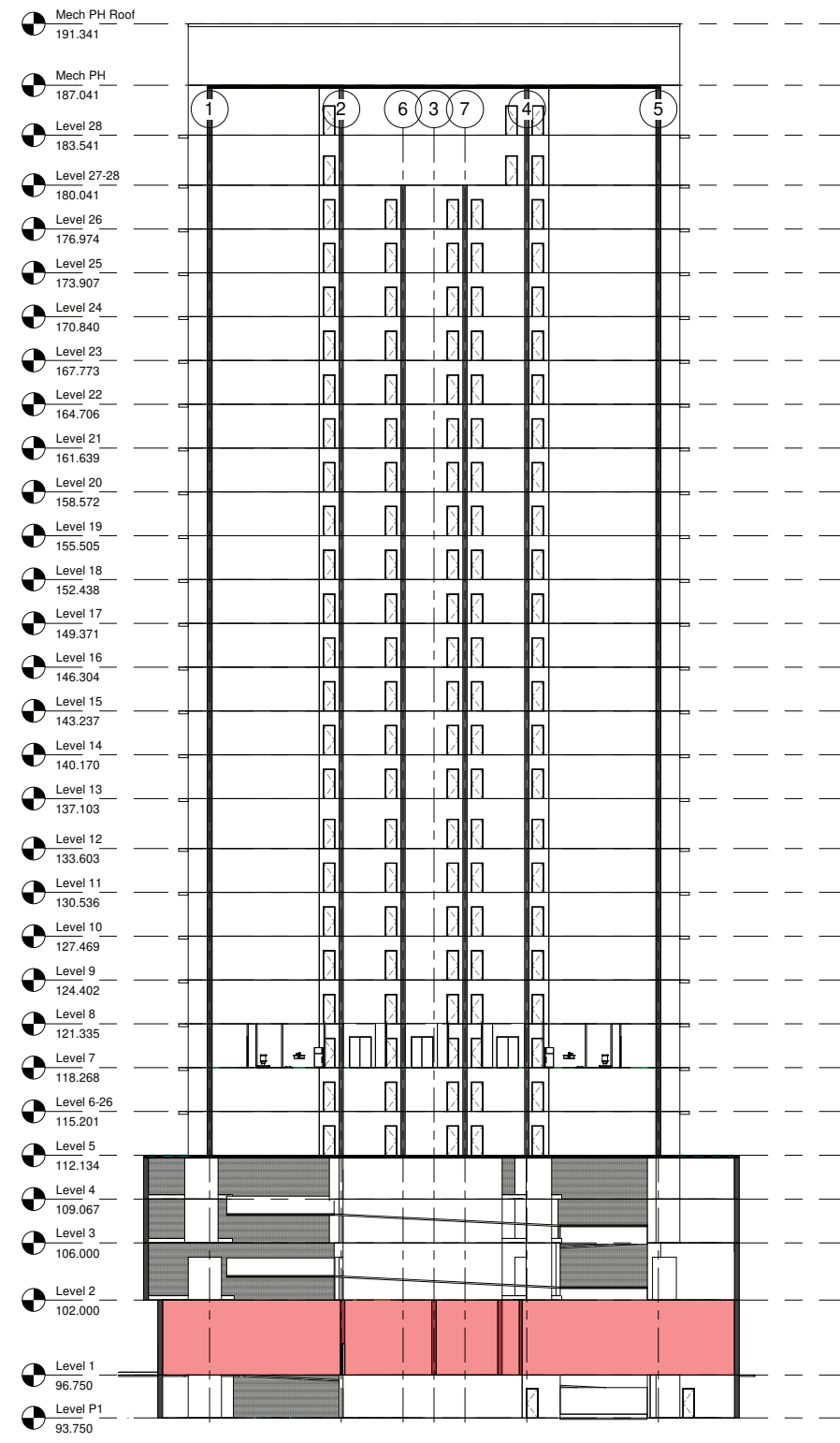
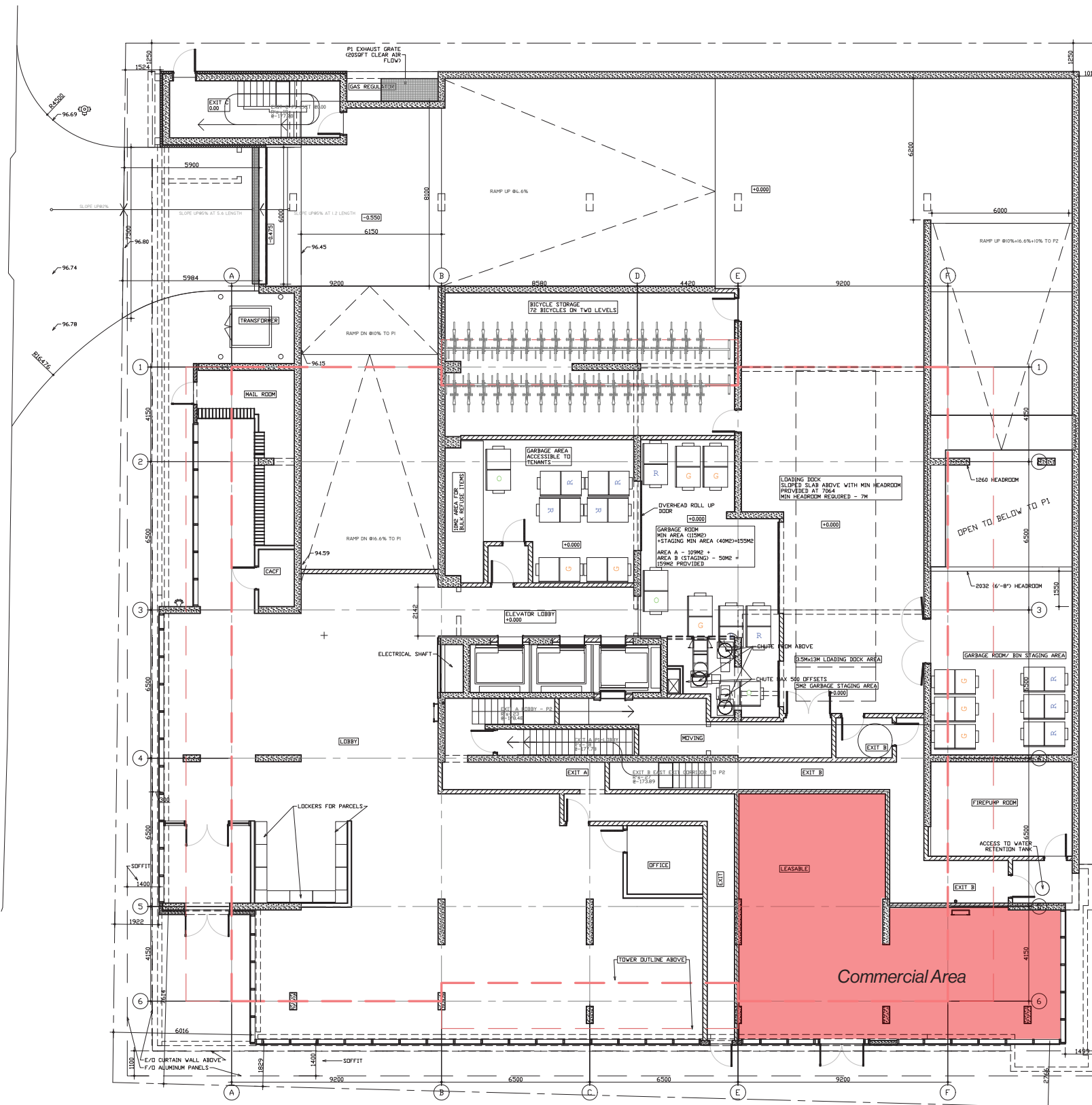
Wind study findings, summer (L) and winter (R) (SLR Consulting (Canada) Ltd. Dec 16, 2022)

Proposed Development

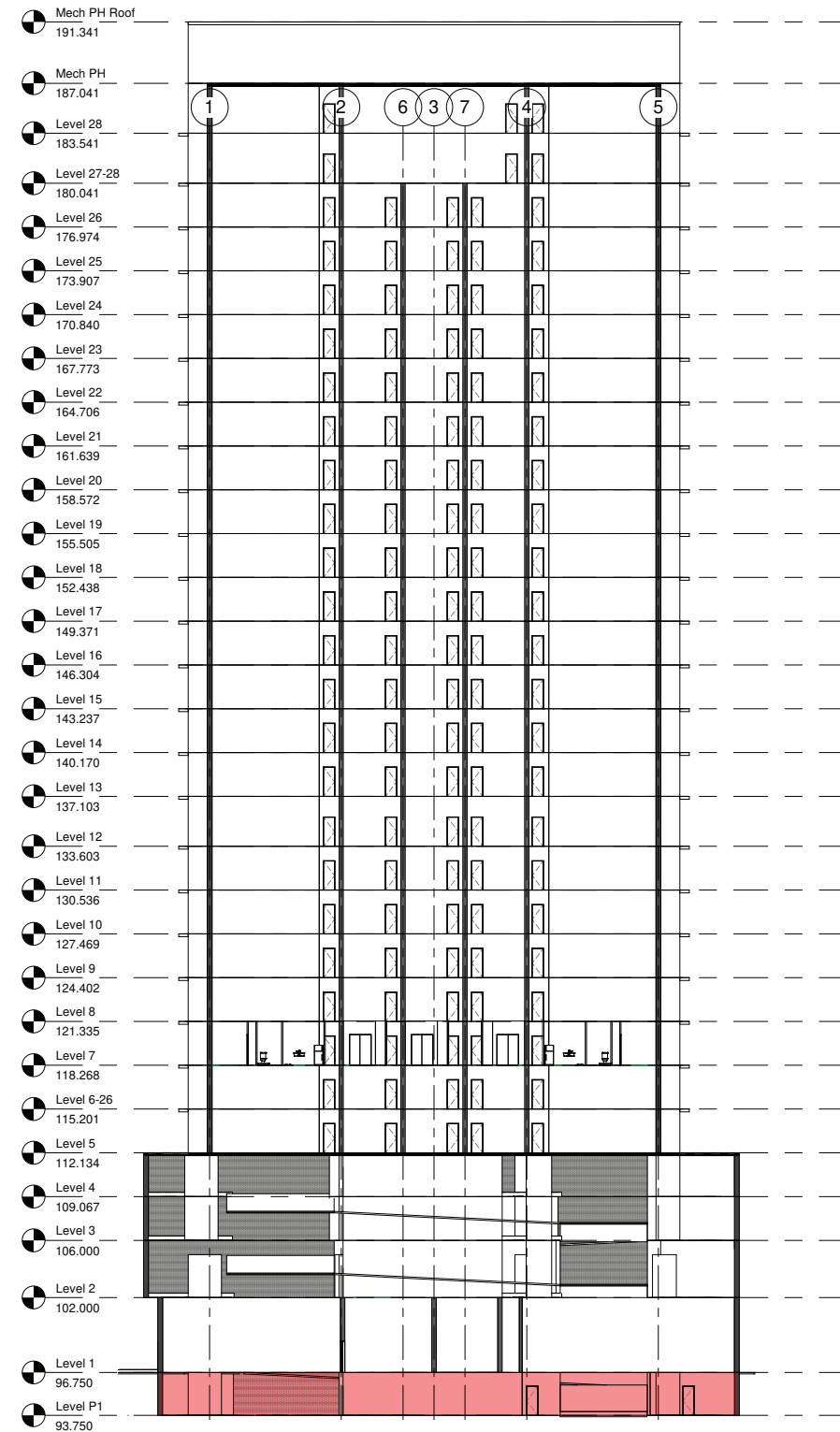
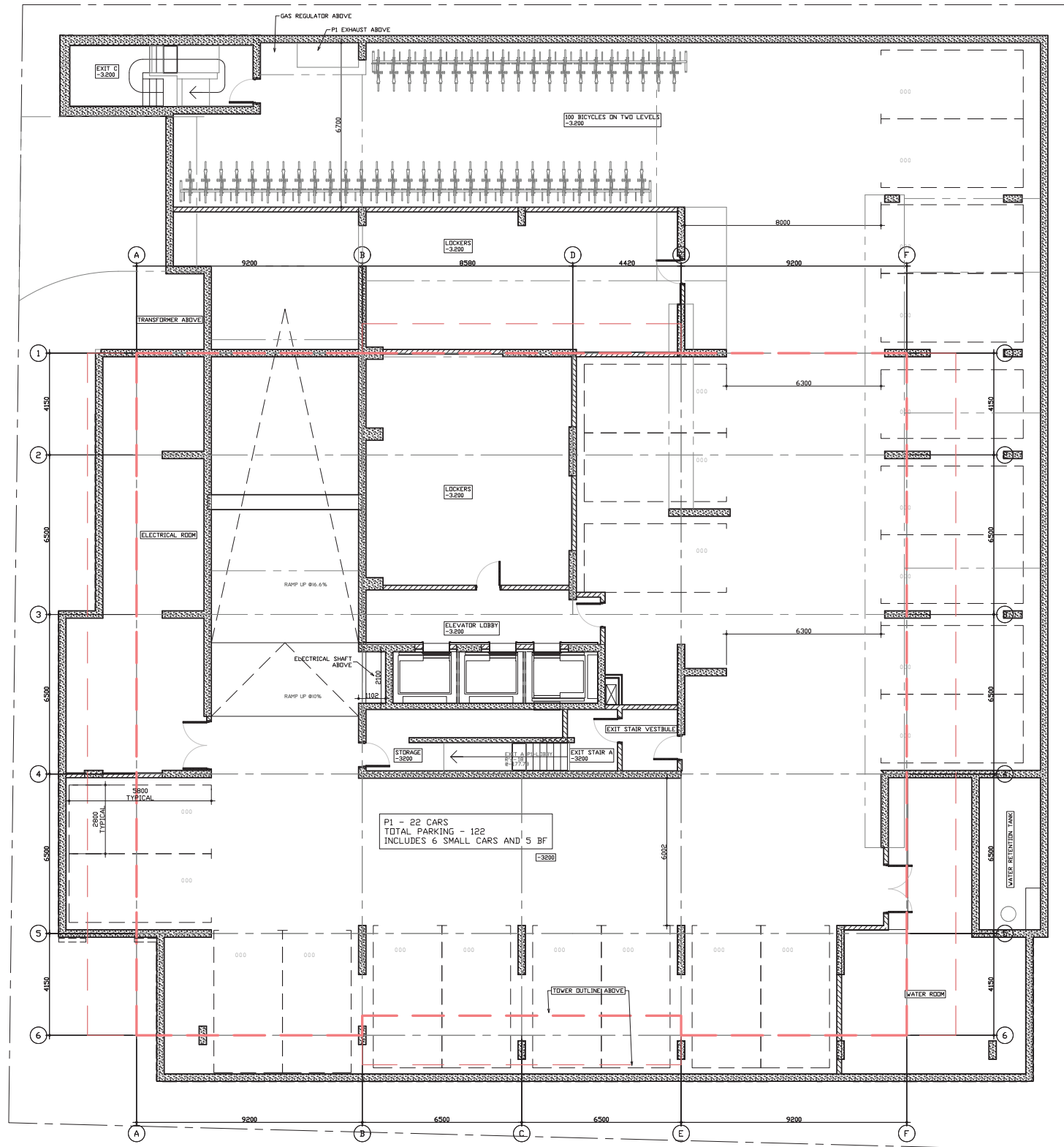
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February 8, 2024
Design Review Panel Submission Package

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Additional Floor Plans and sections are included in Appendix A

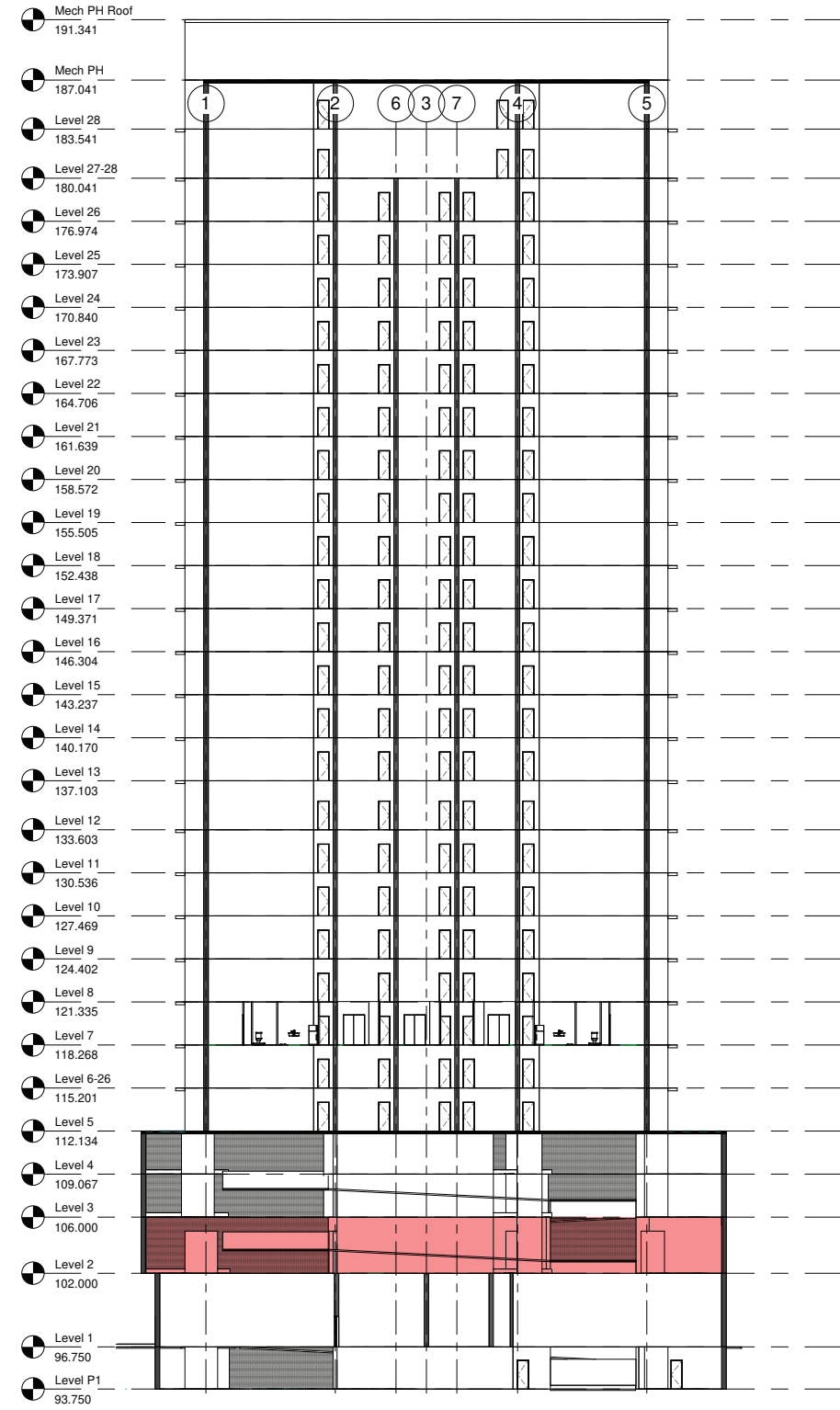
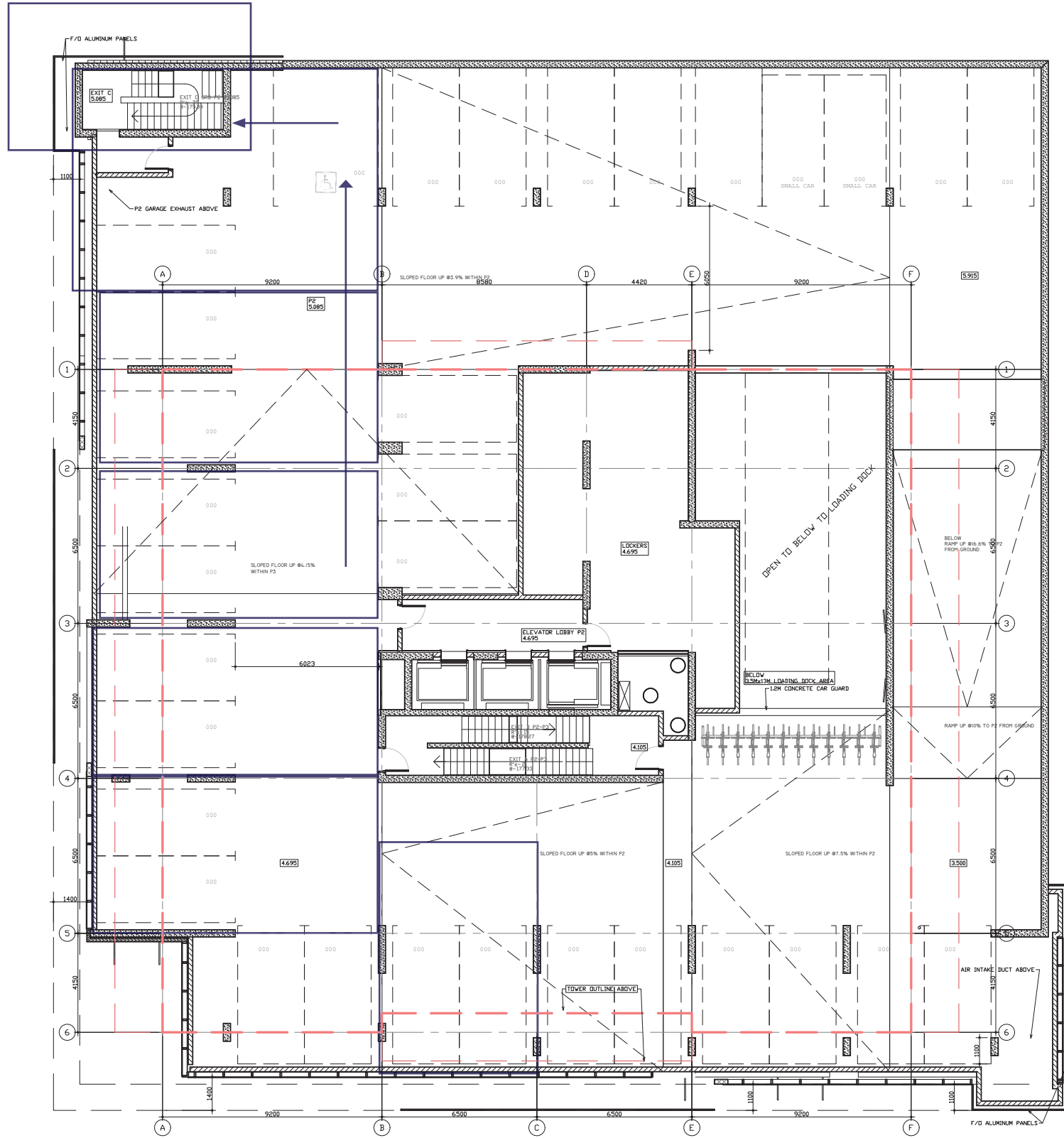


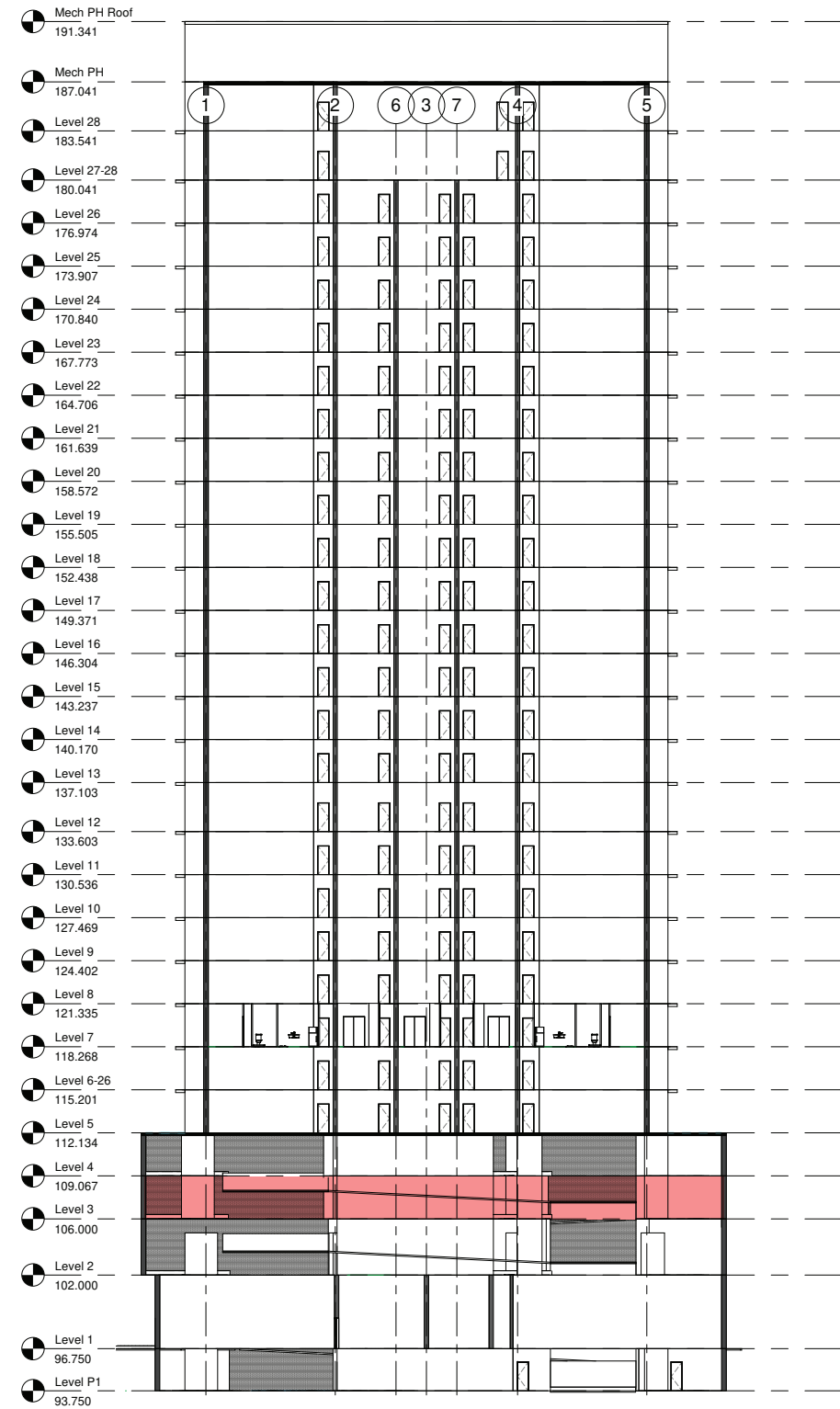
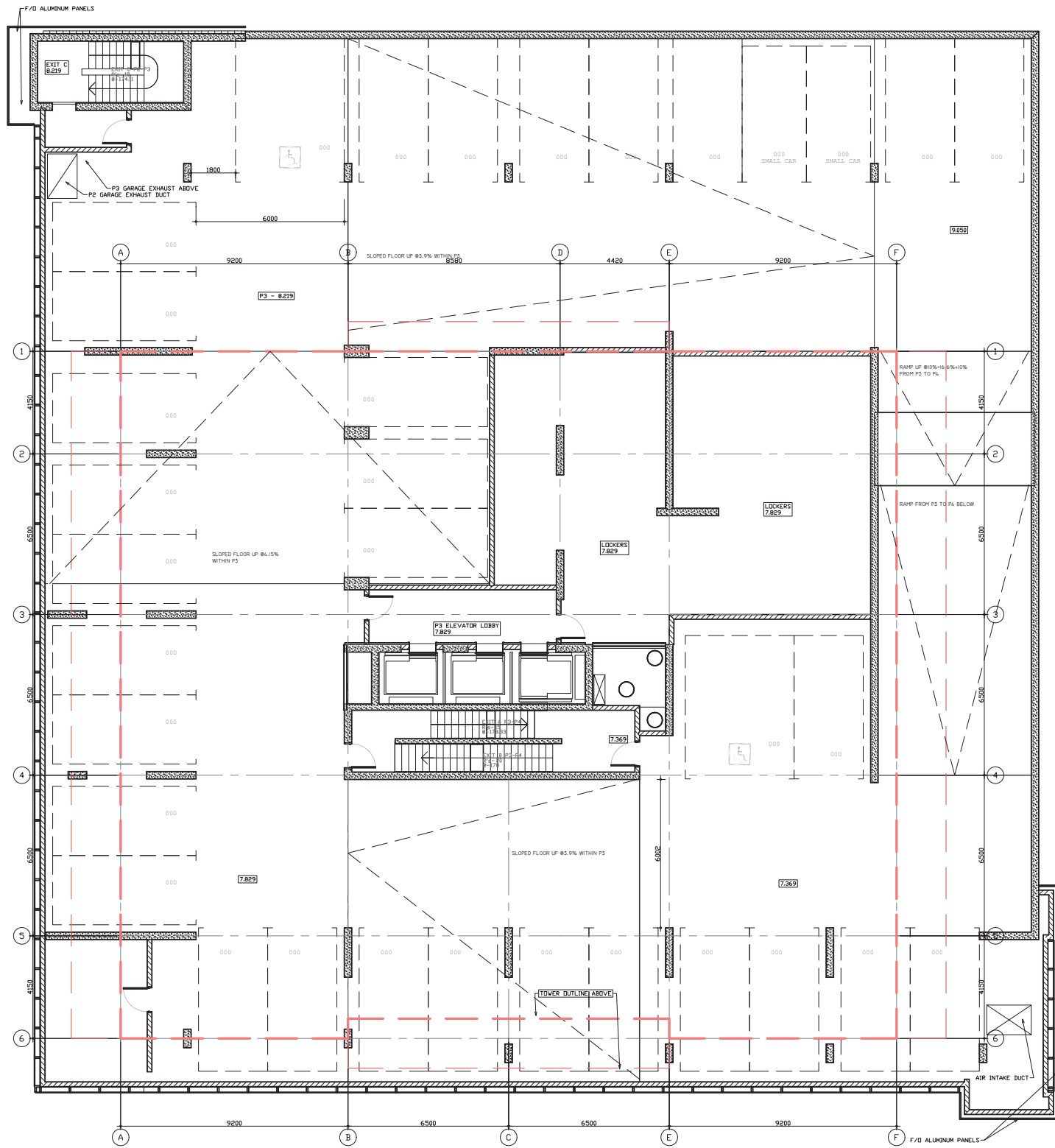
Source: SRM Architects (Dec 14, 2022)

Floor Plans - Level -01 (P1)
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package





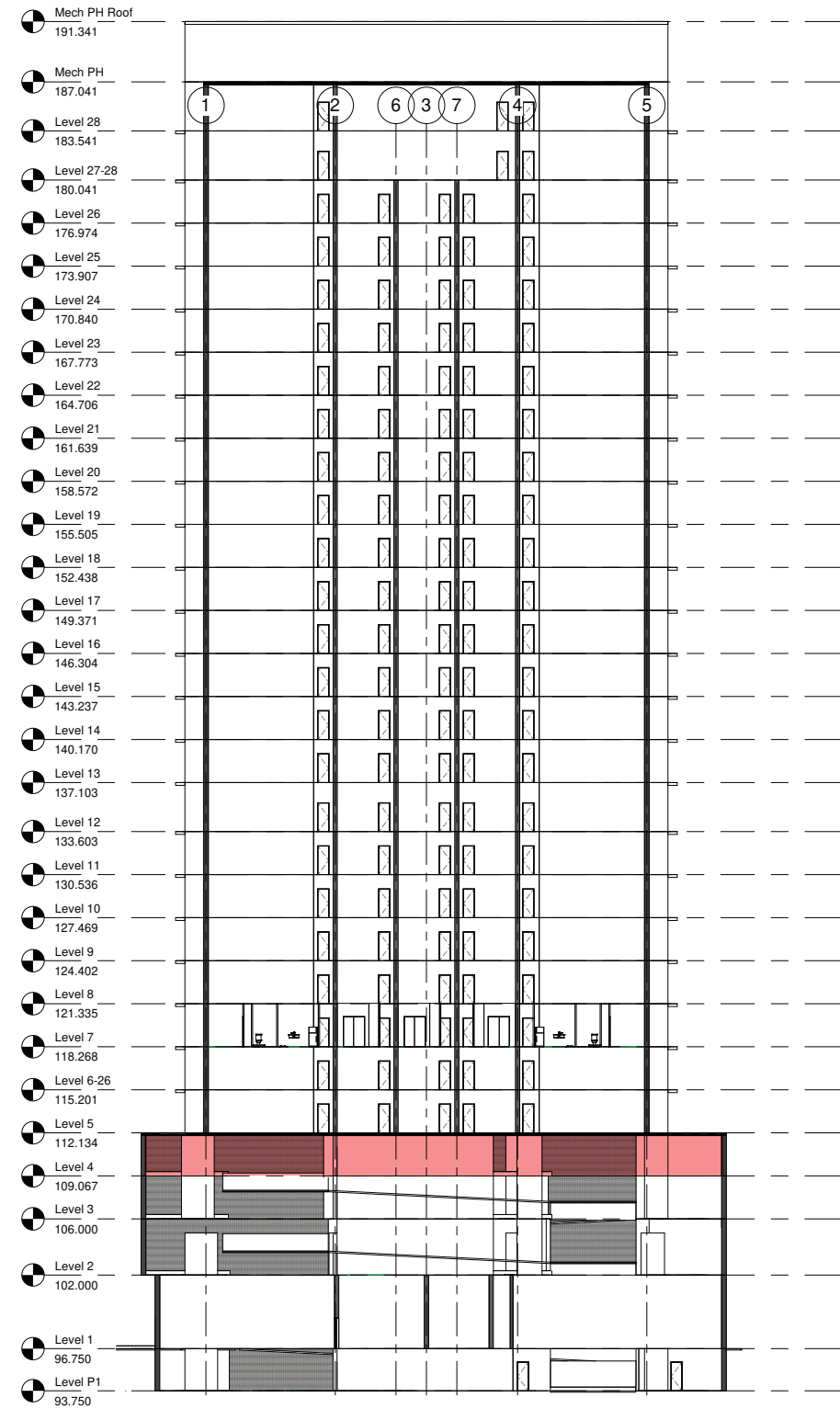
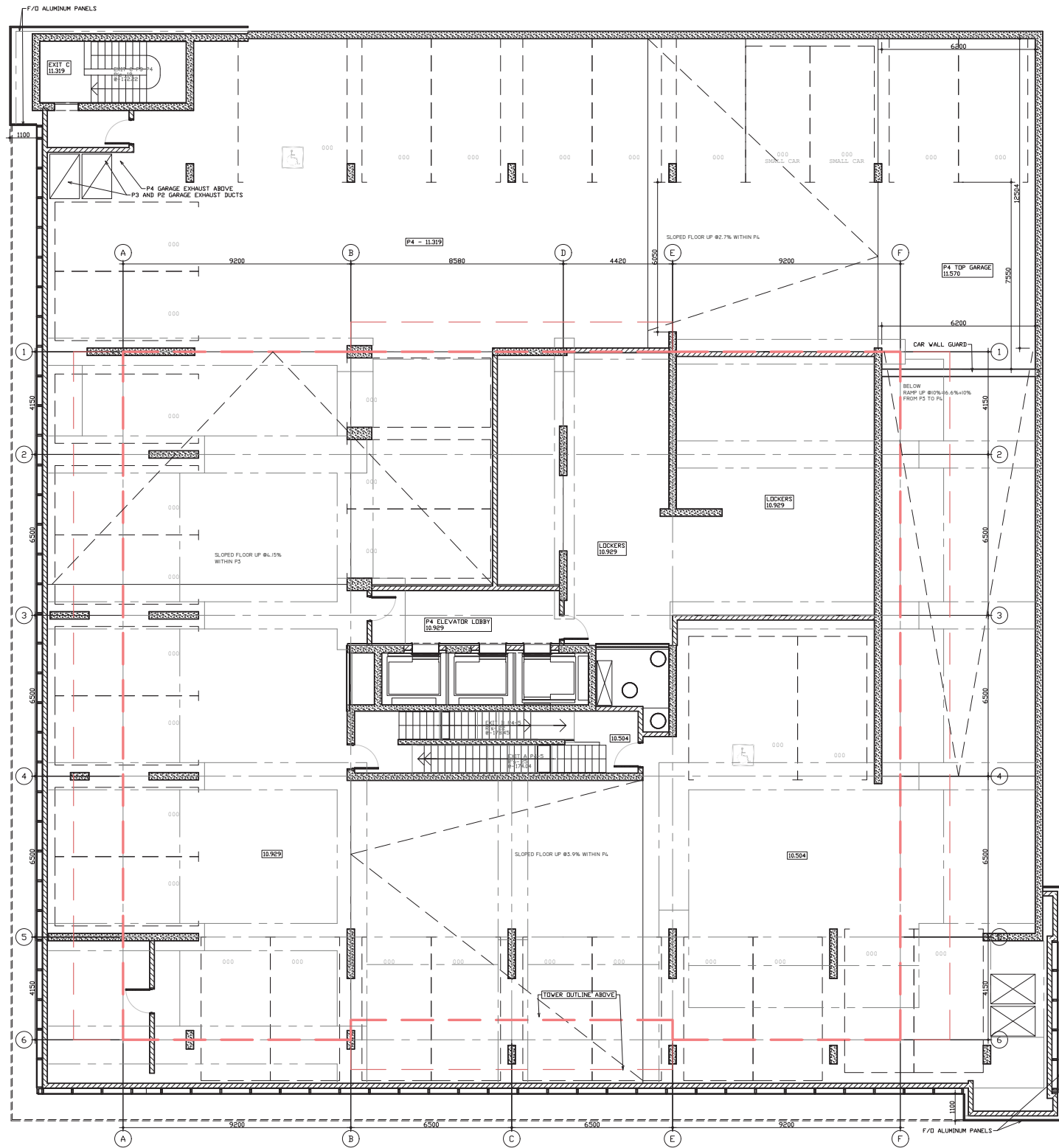


Source: SRM Architects (Dec 14, 2022)

Floor Plans - Level 3 (P3)
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package



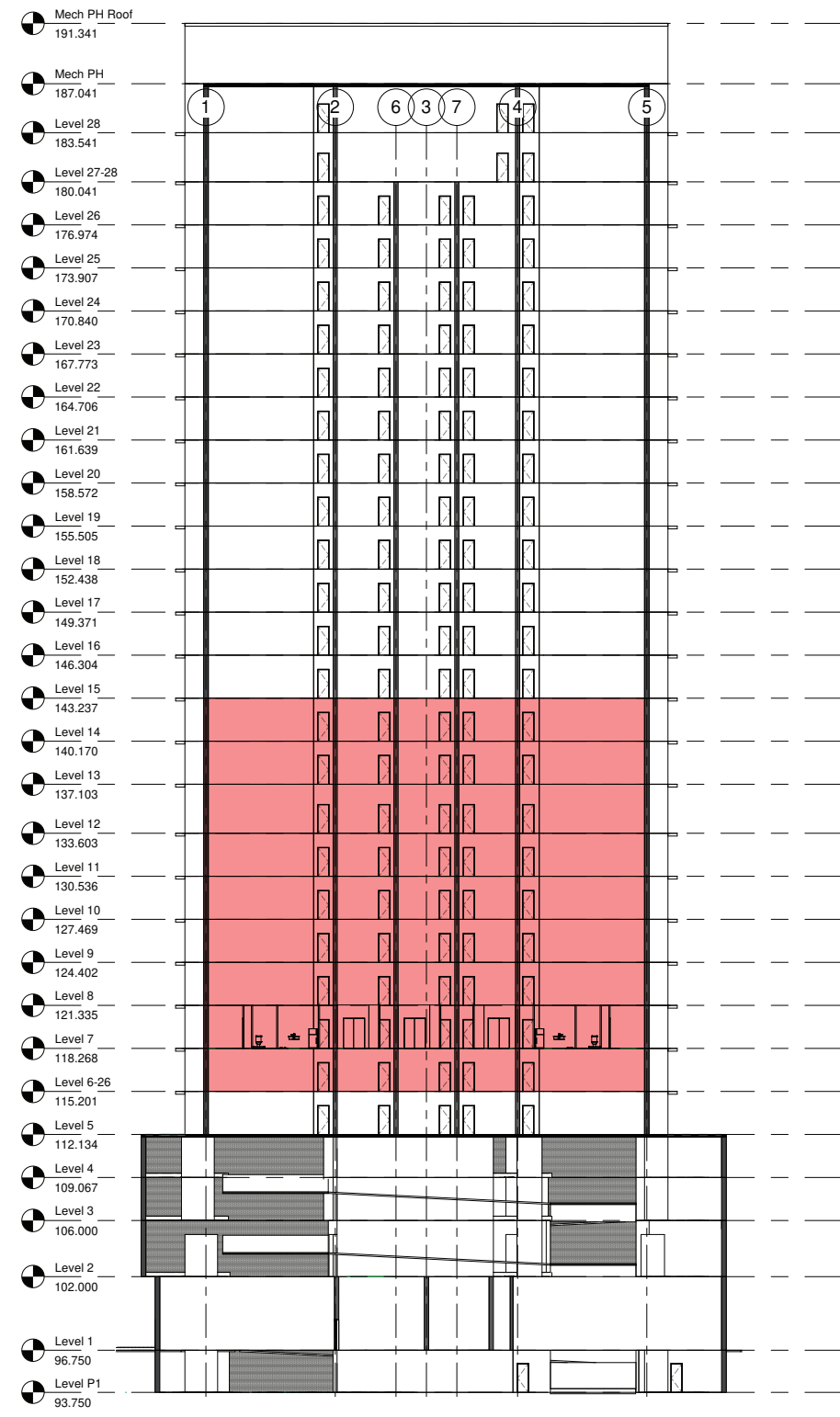
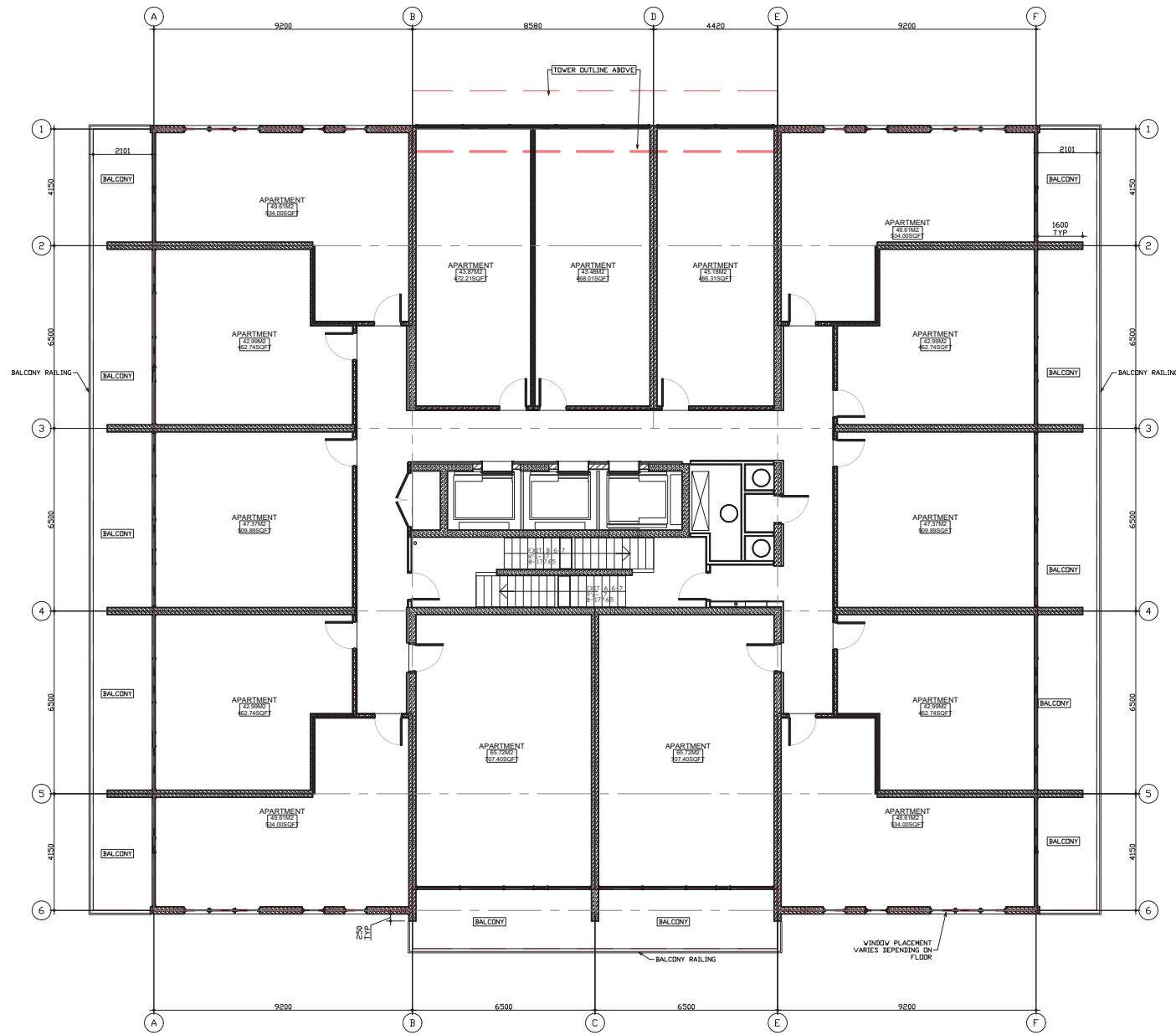


Source: SRM Architects (Dec 14, 2022)

Floor Plans - Level 4 (P4)
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package



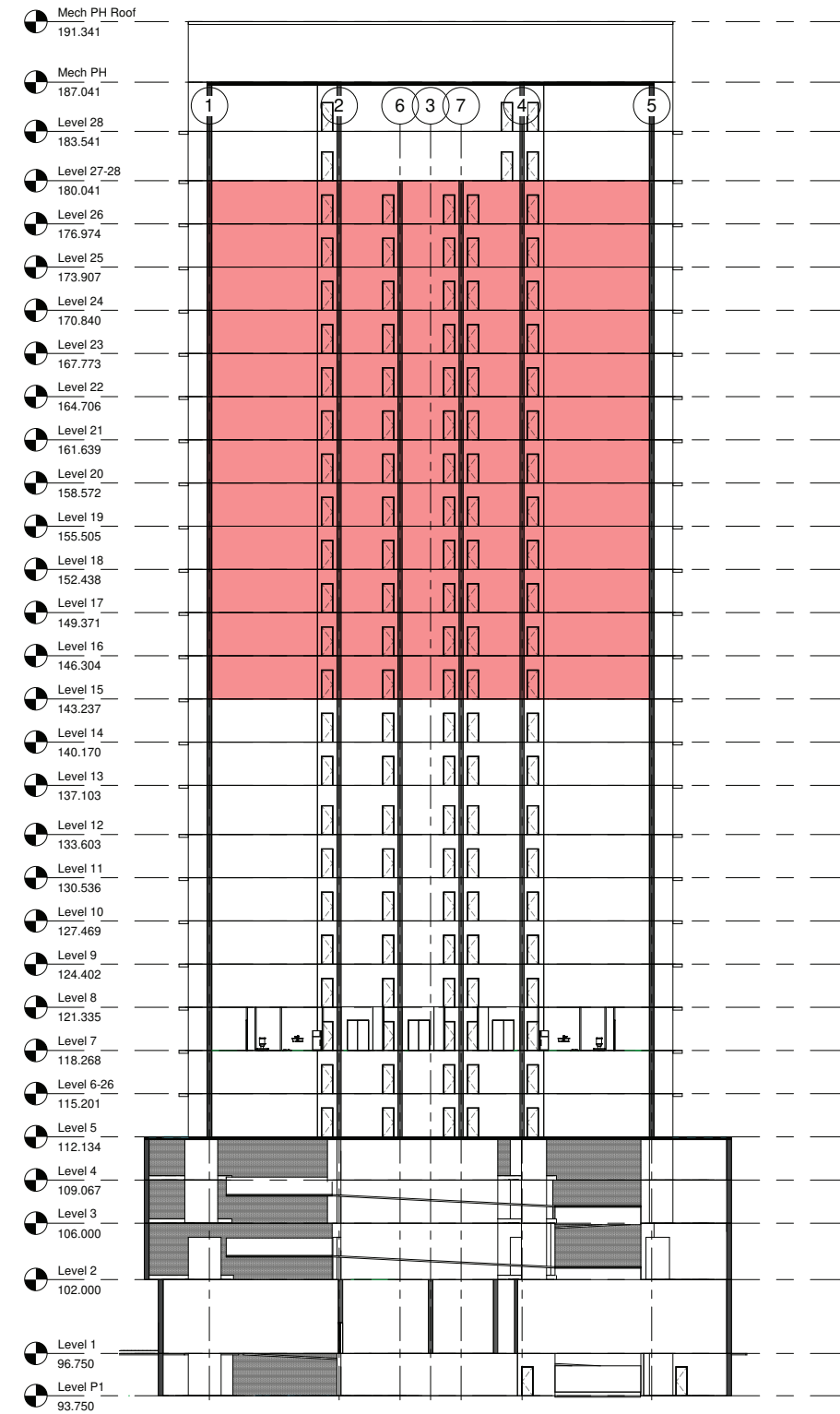
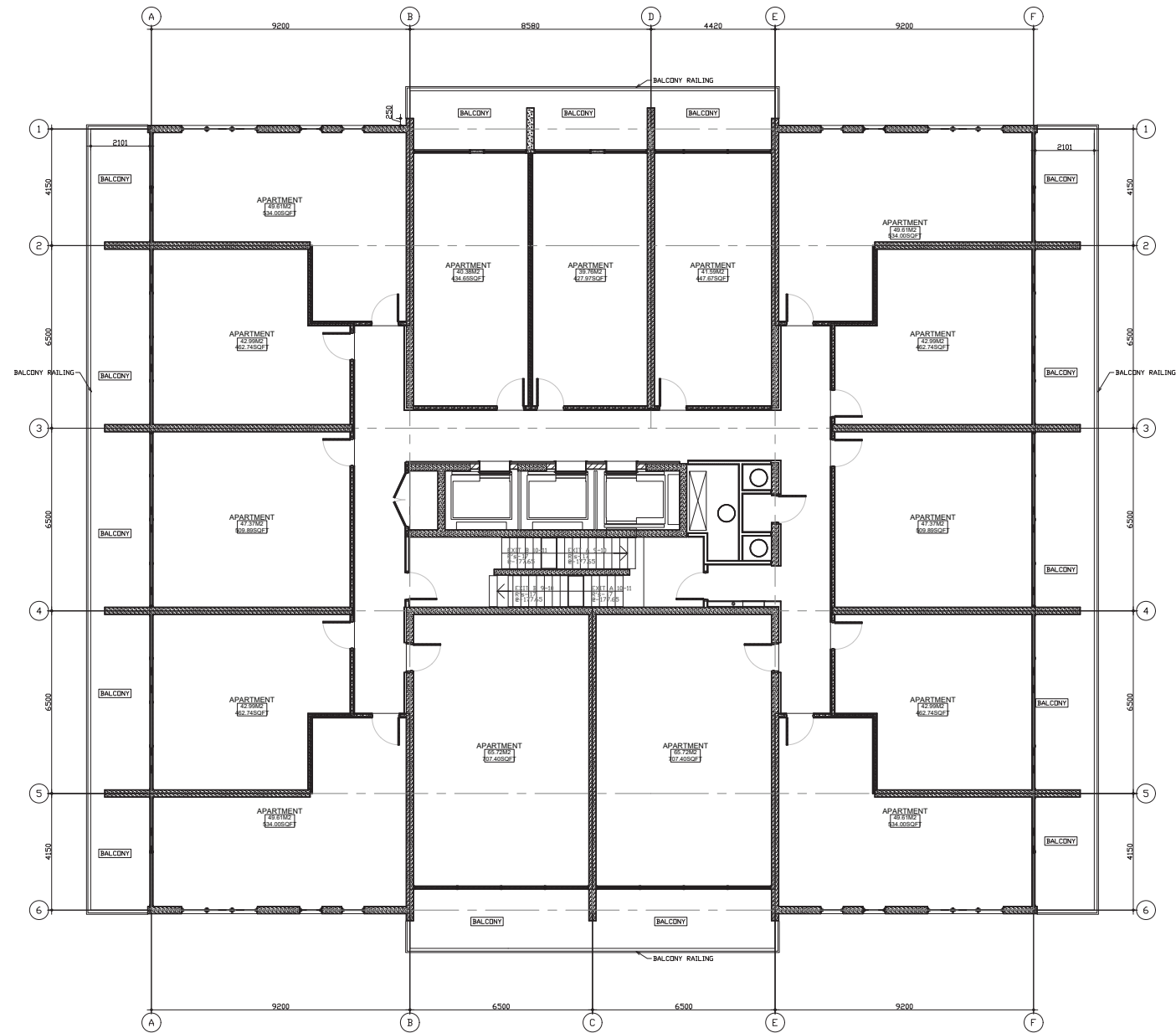


Source: SRM Architects (Dec 14, 2022)

Floor Plans - Levels 6-14
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package

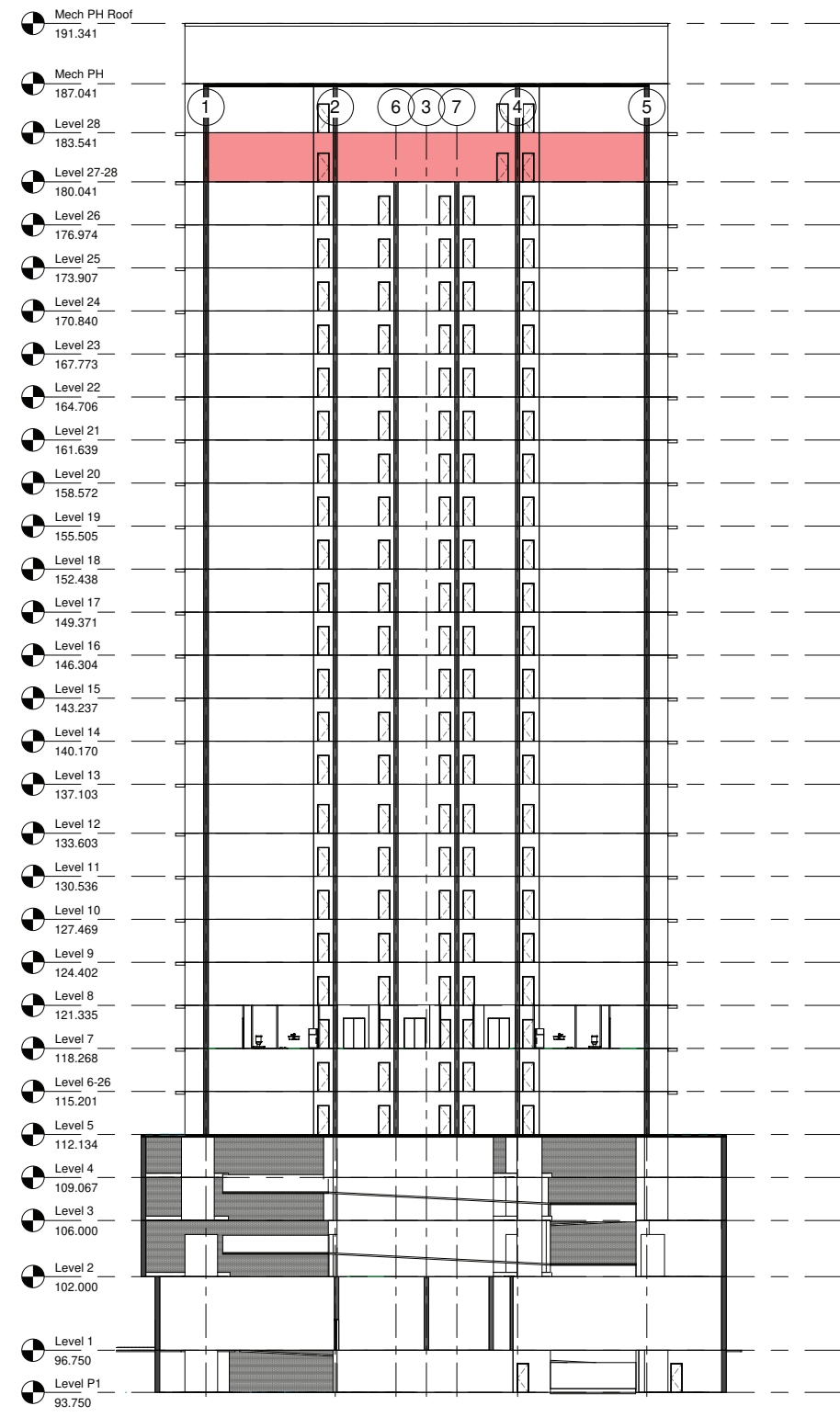
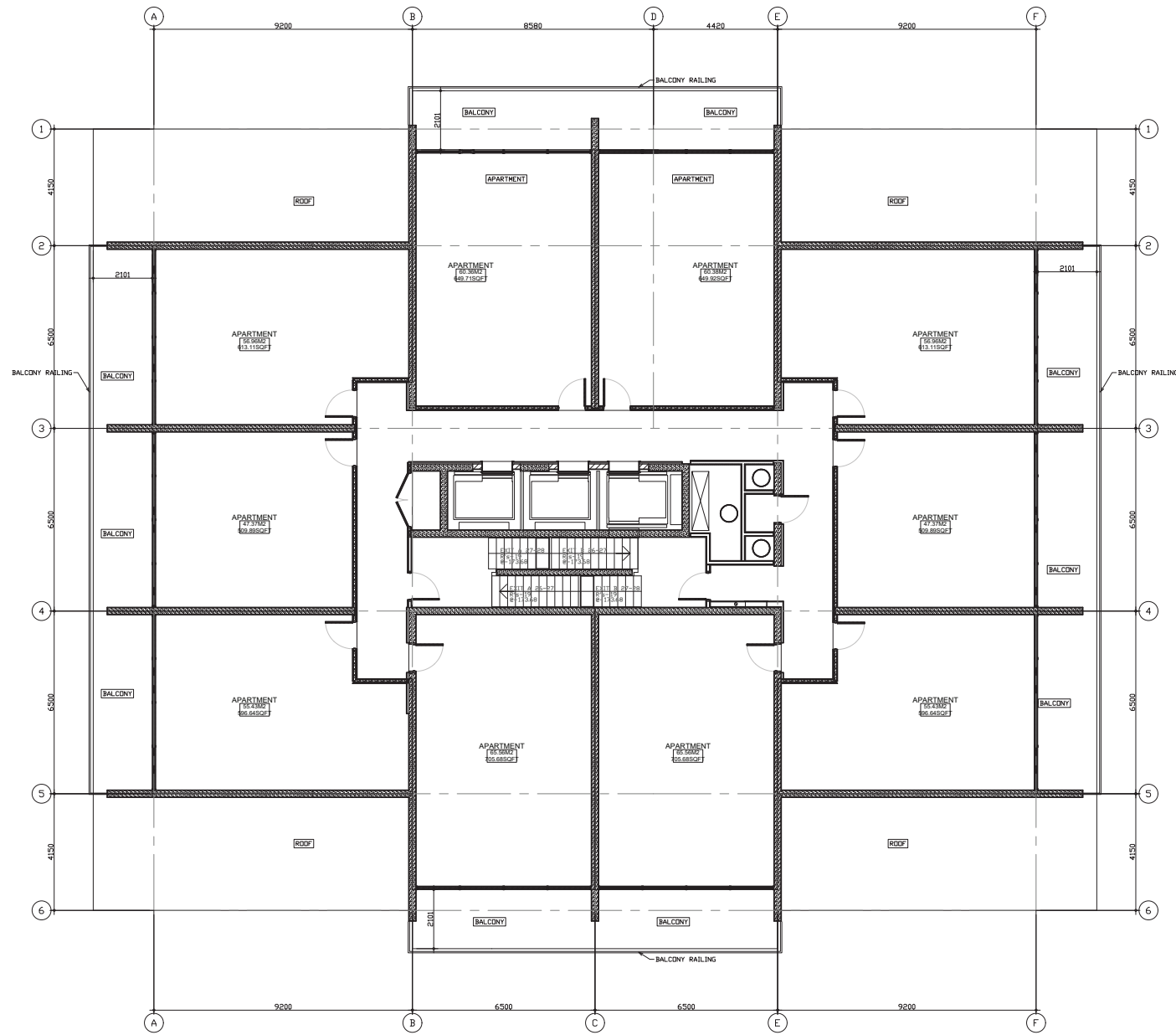




Source: SRM Architects (Dec 14, 2022)

Floor Plans - Levels 16-26
58 York Boulevard, Hamilton

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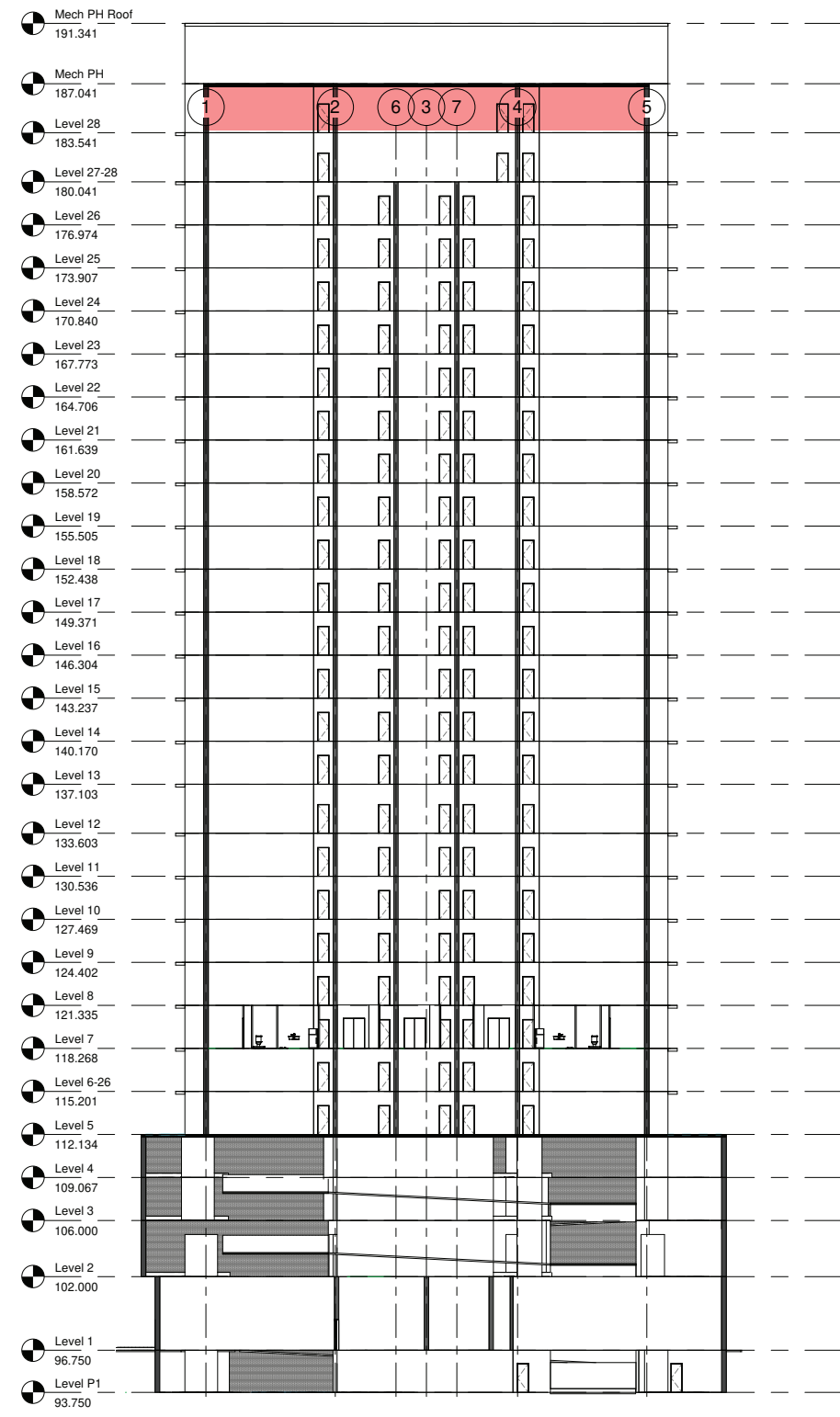
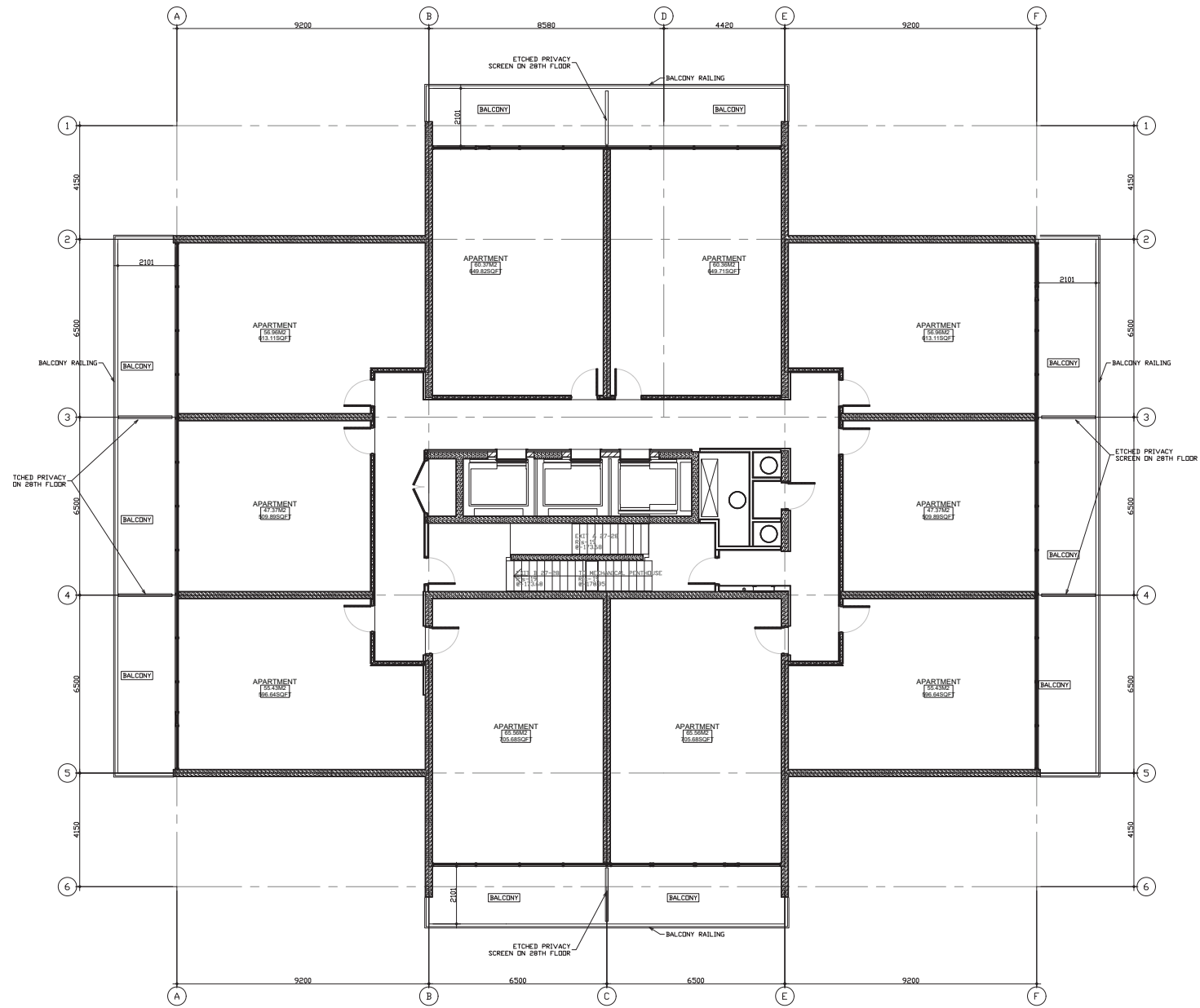


Source: SRM Architects (Dec 14, 2022)

Floor Plan - Level 27
58 York Boulevard, Hamilton

D2021
February 8, 2024
Design Review Panel Submission Package





Source: SRM Architects (Dec 14, 2022)

Floor Plan - Level 28
58 York Boulevard, Hamilton

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