

FINAL REPORT

Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment

Airport Employment Growth Transportation Master Plan Update City of Hamilton, Ontario

Submitted to:

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Executive Summary

The Executive Summary highlights key points from the report only; for complete information and findings, as well as the limitations, the reader should examine the complete report.

In November 2021, the City of Hamilton retained Golder Associates Ltd., a member of WSP (Golder) to conduct a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (Cultural Heritage Report) for the Airport Employment Growth District (AEGD) Transportation Master Plan (TMP) Update, fulfilling Phases 1 and 2 of the Municipal Class Environmental Assessment (EA) process for collector and arterial roads (the 'Project').

The *Urban Hamilton Official Plan* (UHOP) indicates that a significant proportion of growth across the City of Hamilton identifies the AEGD as an important employment area. The AEGD is a planned development area of 551 net developable hectares of employment land per the Secondary Plan. The entirety of this area has been designed to provide for a major business park development that effectively integrates with and complements the existing John C. Munro Hamilton International Airport. The Secondary Plan area is bounded by Garner Road East and Twenty Road West to the north; Upper James Street to the east; White Church Road West to the south; and Fiddler's Green Road to the west (the 'Project study area'). For the purposes of this report, the Project study area includes a 50 m buffer, capturing adjacent properties in accordance with the definition of 'adjacent' in the UHOP.

The AEGD TMP (2011) has been previously completed as an element of the AEGD Secondary Plan and incorporated into the UHOP, with policies and mapping. The Appeal to the Secondary Plan resulted in removal of "white belt" areas from the urban boundary an AEGD TMP Implementation Update (2016).

The development pattern within the AEGD lands has been changing from the time of Secondary Plan completion, and a review of road classifications and road right-of-way resulted in the City of Hamilton's review of the AEGD TMP. The approximately 20 proposed changes to the collector and arterial road network, including removals and realignments of roadways, have triggered a requirement under the EA process for a TMP amendment and public engagement to fulfill Phases 1 and 2 for Schedule B and C EA project requirements within the TMP. The AEGD Secondary Plan was completed without detailed input related to the presence of Built Heritage Resources (BHR) and Cultural Heritage Landscapes (CHL). Due to comments received from the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the City was notified that BHRs and CHLs require better representation in the AEGD TMP to inform proposed roadway link changes and to fulfill the EA process requirements. As such, this Cultural Heritage Report identifies existing and potential BHRs and CHLs within and adjacent to the Project study area. Further, the report includes a review of the background history of the study area, the results of a property visit undertaken to confirm existing conditions, a preliminary impact assessment to identify negative impacts to BHRs and CHLs resulting from the alternative roadway changes in the network, preliminary mitigation recommendations, and recommendations as to whether Cultural Heritage Evaluation Reports (CHER) or Cultural Heritage Impact Assessments (CHIA) may be required for all or parts of the study area to be impacted.

The cultural heritage identification and evaluation documented in this Cultural Heritage Report follows the process set out in the *Draft Existing Conditions and Preliminary Impact Assessment Report Guidelines* provided by the MHSTCI (2019). In addition, best practice in heritage identification and assessment has been used, as outlined in the *Standards and Guidelines for the Conservation of Provincial Heritage Properties* (MHSTCI, 2010), *Identification and Evaluation Process* (2014), and the *Ontario Heritage Toolkit* (2006a).

This Cultural Heritage Report was prepared by Joel Konrad, PhD, CAHP, Cultural Heritage Lead, Ontario, Lindsay Benjamin, MAES, RPP, MCIP, CAHP, Cultural Heritage Specialist, Stephen Jarrett, MA, Cultural Heritage Specialist, Alisha Mohamed, MA, Cultural Heritage Specialist and Chelsey Tyers, BES, MCIP, RPP, Cultural Heritage Specialist. The report was carried out for the entire AEGD TMP study area. A property visit was conducted on December 6, 2021, which confirmed a total of 96 BHRs and 73 CHLs in the study area (Table 2, Appendix B). The impacts of Alternative 1(Do nothing) and Alternative 2 (Amended road network) were assessed for potential impacts to the identified BHRs and CHLs (Table 3, Appendix C). Alternative 1 (Do nothing) will have the least impact on the identified cultural heritage resources. If Alternative 2 (Amended road network) is selected, it is recommended that the alignment of the new roads and expansion of existing roads seek to avoid direct and indirect impacts to BHRs and CHLs where feasible. The preferred alignments should require as little property acquisition as possible.

Recommendations to avoid or reduce adverse impacts to the BHRs and CHLs identified in the Project study area are summarized below. Note that these recommendations are based on Golder's current understanding of the Project and may need to be revisited if there are any changes to the Project.

- When determining the preferred alternatives for the network in the AEGD TMP Update, consideration should be given to alignments that directly and indirectly impact as few BHRs and CHLs as is feasible. Alignments should be selected that require as little property acquisition as possible.
- 2) Staging and construction activities should be appropriately located and/or planned to avoid impacts to the identified BHRs and CHLs.

- 3) Once preferred alternatives or detailed designs of the proposed road network are available for the study area, this report should be updated with a confirmation of impacts of the undertaking on the BHRs and CHLs identified within and/or adjacent to the study area to recommend appropriate mitigation measures. Mitigation measures may include, but are not limited to, completing a CHER, CHIA or Documentation Report, or employing suitable measures such as landscaping, buffering or other forms of mitigation, where appropriate. In this regard, provincial and municipal guidelines should be consulted for advice and further heritage assessment work should be undertaken as necessary and directed by City of Hamilton Staff.
- 4) Should construction activities occur within 40 m of any of the structures located on the identified BHRs or CHLs, the impacts of vibrations may need to be determined through an engineering assessment to ensure that there are no negative impacts to these resources. Any resulting mitigation measures should be implemented prior to construction, as needed.
- 5) Road work and associated construction may cause a temporary impact to the BHRs or CHLs in the study area. Each impacted landscape should be returned to pre-construction conditions (i.e., in-kind fence replacement), and where construction is anticipated to result in grading impacts and tree removal, post-construction landscaping with native tree species should be employed to mitigate visual impacts, resulting in minimal irreversible negative impacts to the BHRs and CHLs.
- 6) Although land acquisition and landscape disturbance beyond the municipal right-of-way is being assumed adjacent to collector and arterial road network corridors proposed for widening and new road alignments, the precise extent of property impacts is not known at this time. As such, CHERs and/or CHIAs have been recommended for all BHRs and CHLs with the potential to be directly, negatively impacted. CHERs are recommended to evaluate each property or landscape's potential CHVI. If a property or landscape is found to possess CHVI, a CHIA is recommended for completion prior to or during the detail design phase and before construction to determine appropriate alternatives or mitigation measures. It is recommended that City of Hamilton Heritage Planning Staff be consulted when recommending the preparation of CHERs and CHIAs.
- 7) Should future work require expansion of the Project study area or if the alternative roadway alignments are changed, a qualified heritage consultant should be contacted to confirm the impacts of the proposed work on known or potential BHRs and CHLs.

Study Limitations

Golder Associates Ltd. has prepared this report in a manner consistent with the guidelines developed by the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) and Canada's Historic Places *Standards and Guidelines for the Conservation of Historic Places* subject to the time limits and physical constraints applicable to this report. No other warranty, expressed or implied, is made.

This report has been prepared for the specific site, design objective, developments and purpose described to Golder Associates Ltd. by the City of Hamilton (the Client). The factual data, interpretations and recommendations pertain to a specific project as described in this report and are not applicable to any other project or site location.

The information, recommendations and opinions expressed in this report are for the sole benefit of the Client. No other party may use or rely on this report or any portion thereof without Golder Associates Ltd.'s express written consent. If the report was prepared to be included for a specific permit application process, then upon the reasonable request of the Client, Golder Associates Ltd. may authorize in writing the use of this report by the regulatory agency as an Approved User for the specific and identified purpose of the applicable permit review process. Any other use of this report by others is prohibited and is without responsibility to Golder Associates Ltd. The report, all plans, data, drawings and other documents as well as electronic media prepared by Golder Associates Ltd. are considered its professional work product and shall remain the copyright property of Golder Associates Ltd., who authorizes only the Client and Approved Users to make copies of the report, but only in such quantities as are reasonably necessary for the use of the report by those parties. The Client and Approved Users may not give, lend, sell, or otherwise make available the report or any portion thereof to any other party without the express written permission of Golder Associates Ltd. The Client acknowledges the electronic media is susceptible to unauthorized modification, deterioration and incompatibility and therefore the Client cannot rely upon the electronic media versions of Golder Associates Ltd.'s report or other work products.

Unless otherwise stated, the suggestions, recommendations and opinions given in this report are intended only for the guidance of the Client in the design of the specific project.

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1.0 INTRODUCTION

1.1 Study Purpose and Objectives

In November 2021, the City of Hamilton retained Golder Associates Ltd., a member of WSP (Golder) to conduct a Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (Cultural Heritage Report) for the Airport Employment Growth District (AEGD) Transportation Master Plan (TMP) Update, fulfilling Phases 1 and 2 of the Municipal Class Environmental Assessment (EA) process for collector and arterial roads (the 'Project'). The study will identify and evaluate collector and arterial road network changes, including removals and realignments of roadways, proposed through the Project.

This Cultural Heritage Report was undertaken to identify municipally, provincially, and federally recognized heritage properties and potential heritage properties within and adjacent to the Project study area to evaluate the impacts that the Project may have on built heritage resources (BHR) and cultural heritage landscapes (CHL).

A Cultural Heritage Report is required for the EA process to: identify existing and potential BHRs and CHLs; review the background history of the Project area; complete a site visit to confirm existing conditions; provide a preliminary impact assessment to conserve BHRs and CHLs; identify mitigation and/or monitoring for potential impacts; and determine whether additional heritage reporting is required.

To meet these objectives, the report will:

- Introduce the study including the purpose and methodology used to undertake the work.
- Review background studies to complete a summary history of the study area using local histories, historical mapping and aerial photographs. This work will trace the evolution of the study area and aid in the identification of existing and potential BHRs and CHLs.
- Obtain information regarding the City of Hamilton's heritage recognitions included on the Municipal Heritage Register (registered non-designated, and/or designated heritage properties) and the identification of City inventoried properties (heritage properties, CHLs, cemeteries and burials, places of worship, and places of education) within the study area.
- Confirm the presence of previously recognized BHRs and CHLs. This process will aid in the identification of resources that may be impacted by the undertaking. This task will include a review of municipal, provincial, and federal heritage registers and inventories, as well as cultural heritage assessments previously completed in the study area.

This work will be conducted in accordance with the *Ontario Heritage Act* (OHA) (2005), the *Provincial Policy Statement* (2020), the *Environmental Assessment Act* (1990), and the *Urban Hamilton Official Plan* (UHOP) (2013).

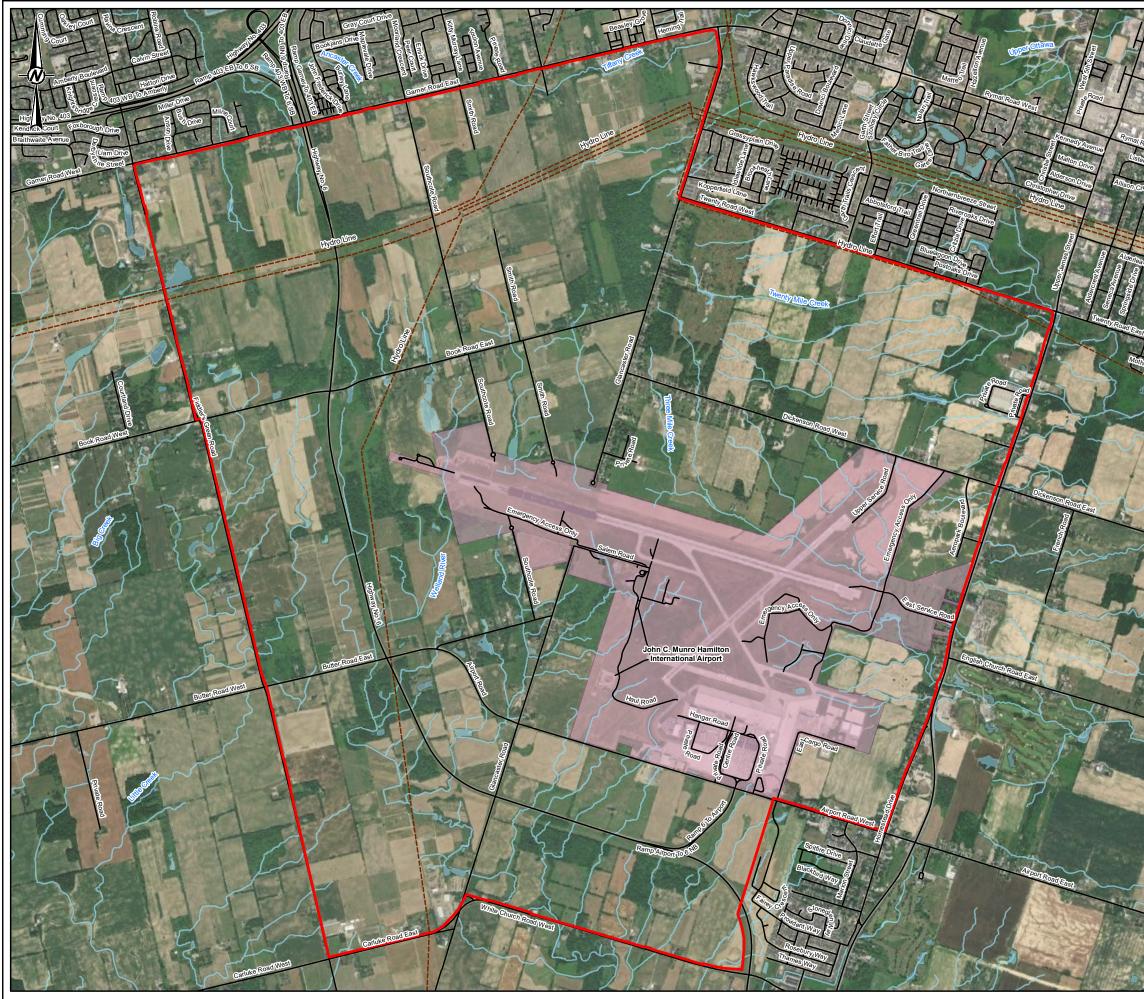
1.2 Project Description and Study Area

The *Urban Hamilton Official Plan* (UHOP) indicates that a significant proportion of growth across the City of Hamilton identifies the AEGD as an important employment area. The AEGD is a planned development area of 551 net developable hectares of employment land per the Secondary Plan. The entirety of this area has been designed to provide for a major business park development that effectively integrates with and complements the existing John C. Munro Hamilton International Airport. The AEGD Secondary Plan was completed without detailed input related to the presence of BHRs and CHLs.

The AEGD TMP (2011) has been previously completed as an element of the AEGD Secondary Plan and incorporated into the UHOP, with policies and mapping. The Appeal to the Secondary Plan resulted in the removal of "white belt" areas from the urban boundary and an AEGD TMP Implementation Update (2016).

The development pattern within the AEGD lands has been changing from the time of Secondary Plan completion, and a review of road classifications and road right-of-way resulted in the City of Hamilton's review of the AEGD TMP. The approximately 20 proposed changes to the collector and arterial road network, including removals and realignments of roadways, have triggered a requirement under the EA process for a TMP amendment and public engagement to fulfill Phases 1 and 2 for Schedule B and C EA project requirements within the TMP. Due to comments received from the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI), the City was notified that BHRs and CHLs require better representation in the AEGD TMP to inform proposed roadway link changes and to fulfill the EA process requirements.

The study area boundary is consistent with the AEGD Secondary Plan area and is bounded by Garner Road East and Twenty Road West to the north; Upper James Street to the east; White Church Road West to the south; and Fiddler's Green Road to the west (the 'Project study area'). For the purposes of this report, the Project study area includes a 50 m buffer, capturing adjacent properties (Figure 1).





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2.0 LEGISLATIVE FRAMEWORK

2.1 United Nations Declaration on the Rights of Indigenous Peoples

On June 21st, 2021, the Canadian federal government enacted *United Nations Declaration on the Rights of Indigenous Peoples Act* and confirmed that the *United Nations Declaration on the Rights of Indigenous Peoples* (Declaration - 2007) "must be implemented in Canada." As a result, Indigenous peoples in Canada are recognized as having unique rights, including those that pertain to the conservation of Indigenous heritage. As per Articles 11 and 31 of the Declaration:

- 11. 1) Indigenous peoples have the right to practice and revitalize their cultural traditions and customs. This includes the right to maintain, protect and develop the past, present and future manifestations of their cultures, such as archaeological and historical sites, artefacts, designs, ceremonies, technologies and visual and performing arts and literature.
- 31. 1) Indigenous peoples have the right to maintain, control, protect and develop their cultural heritage, traditional knowledge and traditional cultural expressions, as well as the manifestations of their sciences, technologies and cultures, including human and genetic resources, seeds, medicines, knowledge of the properties of fauna and flora, oral traditions, literatures, designs, sports and traditional games and visual and performing arts. They also have the right to maintain, control, protect and develop their intellectual property over such cultural heritage, traditional knowledge, and traditional cultural expressions.

2) In conjunction with Indigenous peoples, States shall take effective measures to recognize and protect the exercise of these rights.

These rights to historical sites, ceremonies, cultural traditions, etc. (collectively understood as Indigenous heritage) are pertinent to the Environmental Assessment process through Articles 25 and 26 of the Declaration, which state that:

- 25. Indigenous peoples have the right to maintain and strengthen their distinctive spiritual relationship with their traditionally owned or otherwise occupied and used lands, territories, waters and coastal seas and other resources and to uphold their responsibilities to future generations in this regard.
- 26. 1) Indigenous peoples have the right to the lands, territories and resources which they have traditionally owned, occupied or otherwise used or acquired.

2) Indigenous peoples have the right to own, use, develop and control the lands, territories and resources that they possess by reason of traditional ownership or other traditional occupation or use, as well as those which they have otherwise acquired.

3) States shall give legal recognition and protection to these lands, territories and resources. Such recognition shall be conducted with due respect to the customs, traditions, and land tenure systems of the Indigenous peoples concerned.

2.2 Provincial Policy Context

2.2.1 Planning Act and Provincial Policy Statement

The Ontario Planning Act (1990) and associated Provincial Policy Statement (PPS) (2020) mandate heritage conservation in land use planning. Under the Planning Act, conservation of "features of significant architectural, cultural, historical, archaeological or scientific interest" are a "matter of provincial interest" and integrates this at the provincial and municipal levels through the PPS 2020. Issued under Section 3 of the Planning Act, PPS 2020 recognizes that cultural heritage and archaeological resources "provide important environmental, economic, and social benefits", and that "encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes" supports long-term economic prosperity (PPS 2020:6,22).

The importance of identifying and evaluating BHRs and CHLs is recognized in two policies of PPS 2020:

- Section 2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved
- Section 2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved

Terms defined in Section 6.0 of the PPS 2020 and relevant to this report include:

- Adjacent lands: for the purposes of policy 2.6.3, those lands contiguous to a protected heritage property or as otherwise defined in the municipal official plan.
- Built heritage resource (BHR): means a building, structure, monument, installation or any manufactured or constructed part or remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. Built heritage resources are located on property that may be designated under Parts IV or V of the Ontario Heritage Act, or that may be included on local, provincial, federal and/or international registers.
- Conserved: means the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision-maker. Mitigative measures and/or alternative development approaches can be included in these plans and assessments.

- Cultural heritage landscape (CHL): means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the Ontario Heritage Act; or have been included in on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms.
- Development: means the creation of a new lot, a change in land use, or the construction of buildings and structures requiring approval under the *Planning Act*.
- Heritage attributes: the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built, constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (i.e., significant views or vistas to or from a protected heritage property).
- Protected heritage property: property designated under Parts IV, V or VI of the Ontario Heritage Act; property subject to a heritage conservation easement under Parts II or IV of the Ontario Heritage Act; property identified by the Province and prescribed public bodies as provincial heritage property under the Standards and Guidelines for Conservation of Provincial Heritage Properties; property protected under federal legislation, and UNESCO World Heritage Sites.
- Significant: means, in regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the Ontario Heritage Act.

The definition for significant includes a caveat that "while some significant resources may already be identified and inventoried by official sources, the significance of others can only be determined after evaluation." The criteria for significance established by the Province as well as the need for evaluation is outlined in the following section. Municipalities implement PPS 2020 through an official plan, which may outline further heritage policies (see Section 2.4.1).

2.2.2 Environmental Assessment Act

The purpose of the *Environmental Assessment Act* (1990) is "the betterment of the people of the whole or any part of Ontario by providing for the protection, conservation and wise management, in Ontario, of the environment" (*Environmental Assessment Act* 2009, Part I-Section 2). The Act defines the environment broadly to include the built and cultural environment and outlines a planning and decision-making process to ensure that potential environmental effects are considered before a project begins. This legislation applies to provincial ministries and agencies, municipalities and other public bodies.

2.2.3 Ontario Heritage Act

The Ontario Heritage Act (OHA) (2005) gives municipalities and the provincial government powers to preserve the heritage of Ontario, with a primary focus on protecting heritage properties and archaeological sites. The OHA grants the authority to municipalities and to the province to identify and designate properties of heritage significance, provide standards and guidelines for the preservation of heritage properties and enhance protection of heritage conservation districts, marine heritage sites and archaeological resources.

Designation ensures the conservation of important places and can take the form of individual designations (Part IV of the OHA) or as part of a larger group of properties, known as a Heritage Conservation District (Part V of the OHA). An evaluation using the criteria outlined in Ontario Regulation (O. Reg) 9/06 is used to determine whether a property possesses cultural heritage value or interest (CHVI) and may be worthy of designation under the OHA. Designation offers protection for properties under Sections 33, 34 and 42 of the OHA, prohibiting the owner of a designated property from altering, demolishing or removing a building or structure on the property unless the owner applies to the council of the municipality and receives written consent to proceed with the alteration, demolition or removal.

In addition to designated properties, the OHA allows municipalities to list properties that are considered to have CHVI on their Municipal Heritage Register. Under Part IV, Section 27 of the OHA, municipalities must maintain a Register of properties situated in the municipality that are of CHVI. Section 27 (1.1) states that the register shall be kept by the clerk and that it must list all designated properties (Part IV and V). Under Section 27 (1.2), the Register may include property that has not been designated, but that council believes to be of CHVI. Listed properties (or registered non-designated properties in the case of the City of Hamilton's terminology), although recognized as having CHVI, are not protected under the OHA to the same extent as designated properties, but are acknowledged under Section 2 of the PPS 2020 under the *Planning Act*. An owner of a listed heritage property must provide the municipality with 60 days' notice of their intention to demolish a building or structure on the property.

The OHA also allows for the designation of provincial heritage properties (PHP). Part III.1 of the OHA enables the preparation of standards and guidelines that set out the criteria and process for identifying the CHVI of PHPs (Part II of the OHA) and provincial heritage properties of provincial significance (PHPPS) (O. Reg. 10/06 of the OHA) and to set standards for their protection, maintenance, use, and disposal.

2.2.3.1 Ontario Regulation 9/06

The criteria for determining CHVI is defined in O. Reg. 9/06. This regulation was created to ensure a consistent approach to the designation of heritage properties under the OHA. All designations under the OHA made after 2006 must meet the criteria outlined in the regulation.

A property may be designated under Section 29 of the OHA if it meets one or more of the following criteria for determining whether it is of cultural heritage value or interest:

- 1) The property has design value or physical value because it:
 - i) Is a rare, unique, representative or early example of a style, type, expression, material or construction method;
 - ii) Displays a high degree of craftsmanship or artistic merit; or
 - iii) Demonstrates a high degree of technical or scientific achievement.
- 2) The property has historic value or associative value because it:
 - i) Has direct associations with a theme, event, belief, person, activity, organization, or institution that is significant to a community;
 - ii) Yields, or has the potential to yield information that contributes to an understanding of a community or culture; or
 - iii) Demonstrates or reflects the work or ideas of an architect, artist, builder, designer, or theorist who is significant to a community.
- 3) The property has contextual value because it:
 - i) Is important in defining, maintaining or supporting the character of an area;
 - ii) Is physically, functionally, visually or historically linked to its surroundings; or
 - iii) Is a landmark.

2.3 Greater Golden Horseshoe Heritage Policies

The *Provincial Growth Plan for the Greater Golden Horseshoe* (2016) recognizes the importance of cultural heritage resources. In general, the *Growth Plan* strives to conserve and promote cultural heritage resources in a way that supports the social, economic, and cultural well-being of all communities, including First Nations and Métis communities. Section 4.2.7 of the Growth Plan states that:

- Cultural heritage resources will be conserved in accordance with the policies in the PPS, to foster a sense of place and benefit communities, particularly in strategic growth areas.
- Municipalities will work with stakeholders, as well as First Nations and Métis communities, to develop and implement official plan policies and strategies for the identification, wise use and management of cultural heritage resources.
- Municipalities are encouraged to prepare and consider archaeological management plans and municipal cultural plans in their decision-making.

2.4 Municipal Heritage Policies

2.4.1 Urban Hamilton Official Plan

The City's heritage policies are outlined in the *Urban Hamilton Official Plan* (UHOP), Volume 1, adopted by Council in 2009 and put into effect in 2013. The Plan defines cultural heritage resources as including tangible features, structures, sites and/or landscapes that, either individually or as part of a whole, are of historical, architectural, archaeological and/or scenic value that may also represent intangible heritage, such as customs, ways-of-life, values and activities (City of Hamilton, 2021:23).

The following sections of the UHOP (Section 3.4) highlight the policies for cultural heritage resources:

- B.3.4.1.3 Ensure that all new development, site alterations, building alterations, and additions are contextually appropriate and maintain the integrity of all on-site or adjacent cultural heritage resources.
- B.3.4.2.1(g) Ensure the conservation and protection of cultural heritage resources in planning and development matters subject to the Planning Act either through appropriate planning and design measures or as conditions of development approvals.
- B.3.4.2.1(h) Conserve the character of areas of cultural heritage significance, including designated heritage conservation districts and cultural heritage landscapes, by encouraging those land uses, development and site alteration activities that protect, maintain and enhance these areas within the City.
- B.3.4.2.6 The City recognizes there may be cultural heritage properties that are not yet identified or included in the Register of Property of Cultural Heritage Value or Interest or designated under the Ontario Heritage Act, but still may be of cultural heritage interest. These may be properties that have yet to be surveyed, or otherwise identified, or their significance and cultural heritage value has not been comprehensively evaluated but are still worthy of conservation.
- B.3.4.2.7 The City shall ensure these non-designated and non-registered cultural heritage properties are identified, evaluated, and appropriately conserved through various legislated planning and assessment processes, including the Planning Act, R.S.O., 1990 c.P.13, the Environmental Assessment Act and the Cemeteries Act.

The UHOP also includes policies for the AEGD in Section 2.3. Section 2.3.1 reads:

The City shall undertake a municipally initiated comprehensive review and secondary plan for an Airport Employment Growth District, identified as Special Policy Area C in the Rural Hamilton Official Plan, generally bounded by the existing urban boundary adjacent to Upper James Street to the east, White Church and Fiddler's Green Roads on the south, Garner Road on the west and Glancaster Road, and Twenty Road West on the north [Mod 5(b)] (City of Hamilton, 2021:3). Section 2.3.2 states that "Upon completion of the secondary plan, including the phasing of development for the future Airport Employment Growth District, the City shall initiate an Official Plan amendment: a) to include specific lands within the urban boundary; b) to add new parent and secondary plan policies and mapping for the lands identified in Policy B.2.2.1 a) B.2.3.1 [Mod 5(c)] (City of Hamilton, 2021:3).

2.4.2 Airport Employment Growth District Secondary Plan

Management of cultural heritage resources is sometimes addressed under Secondary Plans or other special policies. The study area comprises the AEGD Secondary Plan, which has the following vision statement specifically referencing cultural heritage:

"The Hamilton Airport Employment Growth District is vibrant and visually appealing and the natural and cultural heritage resources in the area have been preserved and used to establish a distinct character for the area. It is a working community that attracts a range of airport related and other businesses providing both conventional and knowledge-based services. The environmental footprint of the district has been managed through a range of sustainable design techniques and the character of the surrounding land uses have been protected through appropriate land use transitions and transportation planning" (City of Hamilton, 2021:2).

Section 8.2.12 of the AEGD Secondary Plan identifies the principles as it relates to cultural heritage:

- The development shall preserve and celebrate important cultural sites and features. The intent is to:
 - Dialogue with First Nations to determine where important cultural features are located within the employment district;
 - Conduct archaeological assessments or other appropriate studies prior to commencing development;
 - Protect, reflect and display significant cultural sites;
 - Consider using cultural and natural heritage landscape features such as the Greenbelt as a key component of the brand for the district (City of Hamilton, 2021:7).

3.0 METHODOLOGY

As mentioned above, the study area for this Cultural Heritage Report constitutes the AEGD Secondary Plan area as well as all adjacent property parcels located within a 50 m buffer. The scope of this study was defined by guidance set out in the *Draft Existing Conditions and Preliminary Impact Assessment Report Guidelines provided by the MHSTCI* (2019), as well as best practice in heritage identification and assessment outlined in the *Standards and Guidelines for the Conservation of Provincial Heritage Properties* (MHSTCI, 2010), *Identification and Evaluation Process* (2014), MHSTCI *Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes: A Checklist for the Non-Specialist* (2016; the MSTCI Checklist) and the *Ontario Heritage Tool Kit: Heritage Resources in the Land Use Planning Process* (MHSTCI, 2006). The MHSTCI Checklist provides a screening tool to identify all known or recognized BHRs and CHLs in the study area, as well as commemorative plaques, cemeteries, Canadian Heritage River watersheds, properties with buildings or structures 40 or more years old, or potential CHLs.

With this scope, Golder completed the following tasks:

- Researched archival and published sources relevant to the historical and geographic context of the study area;
- Reviewed federal, provincial, and municipal heritage registers, inventories and databases to identify known BHRs and CHLs in the study area, including:
 - Canadian Register of Historic Places;
 - Parks Canada Directory of Federal Heritage Designations and Directory of Heritage Railway Stations;
 - Canadian Heritage Rivers System list of designated heritage river systems;
 - Ontario Heritage Trust (OHT) Places of Worship Inventory, Plaque Database, web mapping tool showing OHT Buildings and Easements, and OHA Register;
 - Ontario Historical County Maps Project nineteenth century map collection (University of Toronto);
 - The Canadian County Atlas Digital Project nineteenth century map collection (McGill University);
 - Historical Topographic Map Digitization Project twentieth century map collection (Ontario Council of University Libraries);
 - Air Photos of Southern Ontario twentieth century aerial imagery collection (University of Toronto Map and Data Library); and
 - Google Street View©.



- Engaged with Heritage Planning Staff at the City of Hamilton and obtained the following information containing heritage resource data for the AEGD study area (exported into mapping shapefiles and Excel lists by the City for ease of reference):
 - List of inventoried, registered non-designated, and designated heritage properties;
 - List of CHLs;
 - List of properties included on the City's Inventory of Cemeteries and Burials (provides research and history on each property);
 - List of properties included on the City's Inventory of Places of Worship (provides research and history on each property);
 - List of properties included on the City's Inventory of Places of Education;
 - List of cultural heritage assessment reports prepared within the boundaries of the AEGD study area. Seven reports were provided by the City.
 - Eleven archaeological assessment reports completed within the study area were provided.
- Conducted a field investigation from the public right-of-way (ROW) to record all known and potential BHRs and CHLs located within and adjacent to the study area, and to understand the wider built heritage and cultural landscape context;
- Completed screening-level evaluations of properties with buildings or structures 40 or more years old and evaluated their potential CHVI using the criteria prescribed in O. Reg. 9/06. This report does not provide a comprehensive evaluation of a property according to O. Reg. 9/06 and does not satisfy the requirement for a Cultural Heritage Evaluation Report (CHER);
- Assessed the risk of impact to properties of known and potential CHVI; and,
- Where necessary, recommended mitigation and conservation measures using MHSTCI and other guidance.

Field investigations were conducted by Lindsay Benjamin, MAES, RPP, MCIP, CAHP, Cultural Heritage Specialist on December 6, 2021. This included taking digital photographs of streetscapes and properties in the study area from the public ROW.

Descriptions of architectural styles and elements used in this Cultural Heritage Report employ terms provided in Blumenson (1990), Ricketts et al. (2004), Hubka (2013), and the Canadian Inventory of Historic Buildings (Parks Canada, 1980). Landscape analysis and landform and vegetation description relies on terms and concepts presented in the Historic Scotland Historic Landuse Assessment (1999) and Australian Soil and Land Survey Field Handbook, Third Edition (2017).

The approach and terms for impact assessment and mitigation measures follows best practices outlined in the MHSTCI's *Standards and Guidelines for the Conservation of Provincial Heritage Properties* and *Ontario Heritage Tool Kit: Heritage Resources in the Land Use Planning Process*, supplemented with other recognized international guidance such as the *Standards and Guidelines for the Conservation of Historic Places in Canada* (Canada's Historic Places, 2010).

For the purposes of this study, any property previously identified by a municipality, municipal staff, provincial or federal agencies, or cultural heritage assessment reporting as containing, or having the potential to contain, CHVI will be determined to be a BHR or CHL, and if applicable, will be discussed in Section 5.4.

3.1 Record of Engagement

Table 1 summarizes the results of engagement conducted for this Cultural Heritage Report.

Contact	Dates of Communication	Description of Information Received
Stacey Kursikowski, Cultural Heritage Planner City of Hamilton Stacey.Kursikowski@ hamilton.ca	Meeting: November 23, 2021	Review of development applications in study area and circulation of relevant studies. Clarity on inventories of BHRs and CHLs provided by the City. Request that revisions to the draft Cultural Heritage Report be made in track changes.
Kevin DeMille, Natural Heritage Coordinator Ontario Heritage Trust Kevin.DeMille@heritagetrust.on.ca	Email Sent: December 2, 2021 Email Received: December 2, 2021	Confirmation that no conservation easements or Trust-owned properties are located within the study area.
Karla Barboza, Heritage Planner MHSTCI karla.barboza@ontario.ca	Email Sent: December 2, 2021 Email Received: December 3, 2021	Confirmation that no properties within the study area have been designated by the Minister and that the MHSTCI is not aware of any PHPs within the study area.

Table 1: Results of Engagement

4.0 HISTORICAL CONTEXT

4.1 Historical Context Overview

The study area is located on Lots 43 to 54, Concession 4 to 6 in the former Geographic Township of Ancaster, and Lots 1 to 5, Concession 2 to 5 in the former Geographic Township of Glanford, County of Wentworth, now the City of Hamilton, Ontario. The following provides a generalized cultural history of the Indigenous people within the study area's surroundings.

The sections below present a sequence of Indigenous land-use for the region from the earliest known human occupation following deglaciation, as well as more recent Euro-Canadian land-use history in the region. These periods are the Pre-Contact period and the Post-Contact (Historic) period.

4.2 Pre-Contact Period

The pre-contact period in Ontario has been reconstructed, primarily, from the archaeological record and interpretations made by archaeologists through an examination of material culture and site settlement patterns. Technological and temporal divisions of the pre-contact period have been defined by archaeologists based on changes to natural, cultural, and political environments that are observable in the archaeological record. It is pertinent to state that although these divisions provide a generalized framework for understanding the broader events of the pre-contact period, they are not an accurate reflection of the fluidity and intricacies of cultural practices that spanned thousands of years. The following presents a sequence of Indigenous land-use from the earliest human occupation following deglaciation to the more recent past based on the following periods as defined by archaeologists as the:

- Paleo Period;
- Archaic Period;
- Woodland Period; and
- Post-Contact Period.

4.2.1 Paleo Period

Paleo period populations were the first to occupy what is now southern Ontario, moving into the region following the retreat of the Laurentide Ice Sheet approximately 11,000 years before present (BP). The first Paleo period populations to occupy southern Ontario are referred to by archaeologists as Early Paleo (Ellis and Deller, 1990).

Early Paleo period groups are identified by their distinctive projectile point types, exhibiting long grooves, or "flutes," that likely functioned as a hafting mechanism (method of attaching the point to a wooden stick). These Early Paleo group tool types include Gainey (c.10,900 BP), Barnes (c.10,700), and Crowfield (c.10,500) (Ellis and Deller, 1990). By approximately 10,400 BP, Paleo projectile points transitioned to various unfluted varieties such as Holcombe

(c.10,300 BP), Hi Lo (c.10,100 BP), and Unstemmed and Stemmed Lanceolate (c.10,400 to 9,500 BP). These types were used by Late Paleo period groups (Ellis and Deller, 1990). Both Early and Late Paleo period populations were highly mobile, participating in the hunting of large game animals. Paleo period sites often functioned as small campsites where stone tool production and maintenance occurred (Ellis and Deller, 1990).

4.2.2 Archaic Period

By approximately 8,000 BP, climatic warming supported the growth of deciduous forests in southern Ontario. These forests introduced new flora and faunal resources, which resulted in subsistence shifts and a number of cultural adaptations. This change is reflected in the archaeological record by new tool-kits that are reflective of a shift in subsistence strategies and has been categorized as the Archaic period.

The Archaic period in southern Ontario is sub-divided into the Early Archaic (c.10,000 to 8,000 BP), Middle Archaic (c.8,000 to 4,500 BP), and the Late Archaic (c.4,500 to 2,800 BP) periods. Generally, in North America, the Archaic period represents a transition from big game hunting to broader, more generalized subsistence strategies based on local resource availability. This period is characterized by the following traits:

- An increase in stone tool variation and reliance on local stone sources;
- The emergence of notched and stemmed projectile point types;
- A reduction in extensively flaked tools;
- The use of native copper;
- The use of bone tools for hooks, gorges, and harpoons;
- An increase in extensive trade networks; and,
- The production of ground stone tools and an increase in larger, less portable tools.

The Archaic period is also marked by population growth with archaeological evidence suggesting that, by the end of the Middle Archaic period (c.4,500 BP), populations had steadily increased in size (Ellis, et al., 1990).

Over the course of the Archaic period, populations began to rely on more localized hunting and gathering territories and were shifting to more seasonal encampments. From the spring into the fall, settlements were focused in lakeshore/riverine locations where a variety of different resources could be exploited. Settlement in the late fall and winter months moved to interior sites where the focus shifted to deer hunting and the foraging of wild plants (Ellis et al., 1990:114). The steady increase in population size and the adoption of a more localized seasonal subsistence strategy led to the transition into the Woodland period.

4.2.3 Early and Middle Woodland Periods

The beginning of the Woodland period is defined by the emergence of ceramic technology. Similar to the Archaic period, the Woodland period is separated into three timeframes: the Early Woodland (c.2,800 to 2,000 BP), the Middle Woodland (c.2,000 to 1,200 BP), and the Late Woodland (c.1,200 to 350 BP) (Spence et al., 1990; Fox, 1990).

The Early Woodland period is represented in southern Ontario by two cultural complexes: the Meadowood Complex (c.2,900 to 2,500 BP), and the Middlesex Complex (c.2,500 to 2,000 BP). During this period, the life ways of Early Woodland populations differed little from that of the Late Archaic with hunting and gathering representing the primary subsistence strategies. The pottery of this period is thick walled, friable, and, as such, likely did not have a long use life. These early ceramics are typically undecorated but exhibit cord impressions, which are likely the result of the techniques used during manufacture rather than decoration (Spence et al., 1990).

The Middle Woodland period has been differentiated from the Early Woodland period by changes in lithic tool forms (i.e., projectile points, expedient tools), and the increased decorative elaboration of ceramic vessels (Spence et al., 1990). Additionally, archaeological evidence suggests the rudimentary use of maize (corn) horticulture by the end of the Middle Woodland Period (Warrick, 2000).

In southern Ontario, the Middle Woodland has been divided into three different complexes based on regional cultural traditions: the Point Peninsula Complex, the Couture Complex, and the Saugeen Complex. These groups are differentiated by sets of characteristics that are unique to regions within the province, specifically regarding ceramic decorations.

The Point Peninsula Complex extends from south-central and eastern Ontario into southern Quebec. The northernmost borders of the complex can be found along the Mattawa and French Rivers. Ceramics are coil constructed with conical bases, outflaring rims, and flat, rounded, or pointed lips. The interior surfaces of vessels are often channelled with a comb-like implement, creating horizontal striations throughout. The exterior is smoothed, or brushed, and decoration generally includes pseudo-scallop stamps or dentate impressions. Occasionally, ceramics will have been treated with a red ochre wash (Spence et al, 1990).

The Saugeen Complex is found generally in south-central Ontario and along the eastern shores of Lake Huron. The Saugeen Complex ceramics are similar in style to Point Peninsula Complex; however, the vessels tended to be cruder than their Point Peninsula counterparts. They were characterized by coil construction with thick walls, wide necks, and poorly defined shoulders. Usually, the majority of the vessel was decorated with pseudo-scallop stamps or dentate impressions, with the latter occurring more frequently at later dates (Spence et al., 1990).

4.2.4 Late Woodland Period

There is much debate as to whether a transitional phase between the Middle and Late Woodland Periods is present in Ontario, but it is generally agreed that the Late Woodland period of occupation begins around 1,100 BP. The Late Woodland period in southern Ontario can be divided into three cultural sub-phases: The early, middle, and late Woodland periods. The early Late Woodland is characterized by the Glen Meyer and Pickering cultures and the middle Late Woodland is characterized by the Uren and Middleport cultures. These groups are ancestral to the Iroquoian-speaking Neutral-Erie (Neutral), the Huron-Wendat (Huron), and Petun Nations that inhabited southern Ontario during the late Late Woodland period (Smith, 1990: 285).

The Pickering and Glen Meyer cultures co-existed within southern Ontario during the early Late Woodland period (c.1250-700 BP). Pickering territory is understood to encompass the area north of Lake Ontario to Georgian Bay and Lake Nipissing (Williamson, 1990). Glen Meyer is centred around Oxford and Norfolk counties, but also includes the southeastern Huron basin and the western extent is demarcated by the Ekfrid Clay Plain southwest of London, Ontario (Noble, 1975). Villages of either tradition were generally smaller in size (~1 ha) and composed of smaller oval structures, which were later replaced by larger structures in the Late Woodland period. Archaeological evidence suggested a mixed economy where hunting and gathering played an important role, but small-scale horticulture was present, indicating a gradual shift from hunting-gathering to a horticultural economy (Williamson, 1990).

The first half of the middle Late Woodland period is represented by the Uren culture (700-650 BP) and the second half by the Middleport (650-600 BP). Uren and Middleport sites of the middle Late Woodland share a similar distribution pattern across much of southwestern and south-central Ontario. (Dodd et al., 1990). Significant changes in material culture and settlement-subsistence patterns are noted during this short time. Iroquois Linear, Ontario Horizontal, and Ontario Oblique pottery types are the most well-represented ceramic assemblages of the middle Late Woodland period (Dodd et al., 1990). At Middleport sites, material culture changes included an increase in the manufacture and use of clay pipes as well as bone tools and adornments (Dodd et al., 1990; Ferris & Spence, 1995).

The appearance of evidence of small year-round villages, secondary ossuary burials, and what are thought to be semi-subterranean sweat lodges suggest a marked increase in sedentism in southern Ontario during the Uren and Middleport cultures (Ferris & Spence, 1995). The increasing permanency of settlements resulted in the development of small-scale cultivation and a subsequent increased reliance on staple crops such as maize, beans, and squash (Dodd et al., 1990; Warrick, 2000; Ferris & Spence, 1995).

Archaeological evidence from the middle Late Woodland sites also documents increases in population size, community organization and village fissioning, and the expansion of trade

networks. The development of trade networks with northern Algonquian peoples has also been inferred from findings at Middleport sites along the northern parts of southwestern and south-central Ontario. These changes resulted in the more organized and complex social structures observed in the late Late Woodland period.

During the late Late Woodland period, village size significantly increased as did the complexity of community and political systems. Villages were often fortified with palisade walls and ranged in size from a few longhouses to over 100 longhouses observed in large villages. Larger longhouses oriented differently than others in the village have been associated with primary familial groups and it has been suggested that longhouses that were located outside of palisade walls may have been for visiting groups for the purposes of trade or social gatherings (Ramsden, 1990). More recent research has indicated that smaller, temporary camp or cabin sites were often used seasonally for the tending of agricultural fields or as fishing camps (Ramsden, 1990). By this time, large-scale agriculture had taken hold, making year-round villages even more practical as a result of the ability to store large crop yields over winter.

Early contact with European settlers at the end of the Late Woodland period resulted in extensive changes to the traditional lifestyles of most populations inhabiting Ontario including settlement size, population distribution, and material culture. The introduction of Europeanborne diseases significantly increased mortality rates, resulting in a drastic drop in population size (Warrick, 2000).

4.3 Post-Contact Period

4.3.1 Pre-Confederation Treaties

The study area, located in the City of Hamilton, is situated within land negotiated under Treaty 3 (the Between the Lakes Purchase) between the Mississaugas and the Crown (Indigenous and Northern Affairs Canada, 2010). On December 7, 1782, an agreement was signed between Mississauga and officials representing the Crown, which confirmed and amended the taking of approximately three million acres of land for settlement by Loyalists arriving during and after the American Revolutionary War (MCFN, 2017). A portion of this land (550 000 acres) was granted to the Six Nations for their loyalty to the Crown during the war (MCFN, 2017).

4.3.2 County of Wentworth

Following the Toronto Purchase of 1787, today's southern Ontario was included within the old Province of Quebec and divided into four political districts: Lunenburg, Mechlenburg, Nassau, and Hesse. These became part of the Province of Upper Canada in 1791, and renamed the Eastern, Midland, Home and Western Districts, respectively. The study area was located within the former Nassau District, then later the Home District, which originally included all lands between an arbitrary line on the west running north from Long Point on Lake Erie to Georgian Bay, and a line on the east running from Presqu'ile Point on Lake Ontario to the Ottawa River. Each district was further subdivided into counties and townships. In 1816, Wentworth County was created within the Gore District (established in 1792) from the southwest portions of York County in the Home District, and the west portion of the Niagara District (Page and Smith, 1875). The townships originally included in Wentworth County were Ancaster, Barton, Beverly, Binbrook, Caistor, Flamborough East and West, Glanford, Onondaga, Saltfleet, and Seneca. The County was named in honour of Sir John Wentworth, Lieutenant Governor of Nova Scotia from 1792-1808. The earliest settlers to the County were United Empire Loyalists. The first saw and grist mill was built in the town of present-day Ancaster in 1791 (Mika and Mika, 1983:624). The first paper mill was established in 1836 at Crooks Hollow, within an industrial complex of mills in West Flamborough (Mika and Mika, 1983:625). The first public works endeavour in the County was the Burlington Bay Canal, which was completed in 1832 and "was said to have 'made' the City of Hamilton" (Page and Smith, 1875:5).

The County of Wentworth was a site of importance during the War of 1812. The battle of Stoney Creek was a turning point in the War that prevented Upper Canada from American takeover (Mika and Mika, 1983:625). In the early hours of June 5, 1838, 700 British soldiers staged a surprise attack on 3,000 American forces and were successful. The British soldiers had come from Burlington Heights, a strategic defense position for British and Canadian forces.

Between 1850 and 1854, Wentworth and Halton Counties were joined for government purposes into the United Counties of Wentworth and Halton; however, this change was short-lived as the Counties were official separated by 1854. In 1973, Wentworth County was renamed the Regional Municipality of Hamilton-Wentworth and in 2001 it was amalgamated with six constituent municipalities into the City of Hamilton.

4.3.3 Township of Ancaster

Ancaster Township was one of many townships that Lieutenant Governor John Graves Simcoe named after places in England, Ancaster specifically being named after a Lincolnshire Parish. The land for Ancaster Township was acquired by the British from the Mississaugas in 1784 as a part of the Between the Lakes Treaty that was originally signed in 1784 and again in 1792 due to uncertainties with the description of the land in the original land surrender (Government of Ontario, 2022).

The original settlers in the area consisted of United Empire Loyalists (UEL), most of which had fought for the British in the American Revolutionary War (Woodhouse, 1973). Settlers arrived as early as 1789, but as the area had not yet been surveyed, they occupied the land informally (Woodhouse, 1973). In 1793, the first township survey was completed and the first legal settlers began to occupy the area two years later. The Township of Ancaster became part of Wentworth County in 1816. The southern triangle of the Township was known as the Scotch Block for the number of settlers who had arrived from Scotland (Green, 2004).

Located beside the natural break in the Niagara Escarpment, and beside a significant creek flowing over it, the police village of Ancaster became the location of a large number of mills. By the 1830s, Ancaster was a popular destination as the commercial centre for the area, however this popularity was short-lived. In 1832, the Desjardin Canal in Dundas, Ancaster's neighbour and rival, opened and allowed a bypass around Ancaster (Welch & Payne, 2015). In addition, the town was also bypassed by the railway, which went to Hamilton (Welch & Payne, 2015).

4.3.4 Township of Glanford

The Township of Glanford, named for the parish of Glanford-Brigg in Lincolnshire, England, was first surveyed in 1794 by Augustus Jones using the Single Front Survey System that was commonly used between 1783 and 1818 (AECOM, 2021:12). The system was intended to produce a square pattern of five 200-acre lots bounded on all four sides by road allowances. However, due to imprecise surveying and unusual lot dimensions, most of the lots in the Township of Glanford resulted in 188-acre lots. The resulting survey created the modern farm landscape and road pattern that is visible throughout much of the Township today (Dean & Mathews, 1969).

Most of the land grants originally given out following the Crown Survey were owned by absentee owners, however settlers began to arrive shortly after the survey. Due to the barrier presented by the Niagara Escarpment, settlement was slower than the neighbouring townships to the north, in Saltfleet and Barton. After being deforested by pioneer settlers, the Township of Glanford was suitable for grain cultivation and mixed agriculture. By 1841, the population in the township had grown to over 1,000 people (Glanford Historical Society, 1985). By the mid-1950s, the township population had slowly increased to over 2,000 inhabitants.

The new Township of Glanbrook was formed through the amalgamation of the Townships of Binbrook and Glanford in 1974, the same year in which Wentworth County was replaced by the Regional Municipality of Hamilton-Wentworth. The Township of Glanbrook was absorbed into the expanding City of Hamilton in 2001 (Glanbrook Heritage Society, 2012).

4.3.5 City of Hamilton

The first settler within what is now known as the City of Hamilton was Mr. Robert Land, who settled on 300 acres of land in the area in 1778 (Page and Smith, 1875). However, the neighbouring towns of Ancaster, Stoney Creek and Barton were preferable for agriculture and settlement, so Hamilton remained relatively sparsely populated until the late eighteenth century (Page and Smith, 1875). Early settlers did not establish themselves in the city until after 1812, and included Mr. Charlton of Charlton Vinegar works, Peter Ferguson, Nathaniel Hughson, Captain Durand, Daniel Kirkendall and Edward Jackson (Page and Smith, 1875:XV).

The City of Hamilton was named after George Hamilton, who in 1815 purchased a house and 257 acres in a small village that was known as the Head of the Lake (Weaver, 2019). Hamilton sold parcels of his estate to Loyalists and newcomers and delineated roadways, and in doing

so laid out the town. Hamilton was designated as the administrative seat of the newly created Gore District, and was named after its founder (Weaver, 2019).

The City of Hamilton developed as the centre of the County of Wentworth, with a number of industrious companies being established and incorporated throughout the nineteenth century. The building of the Great Western Railway increased the availability of produce and trade goods and sparked the development of the city throughout the nineteenth century (Page and Smith, 1875). The Hamilton Water Works Company was incorporated in April 1836, resulting in the construction of a large reservoir, and by 1847, the Burlington Dry Dock and Ship-Building Company was established (Page and Smith, 1875). The City of Hamilton was also an educational centre, with school construction and attendance increasing rapidly from 1812. By 1875, the City boasted the best schools and education system in the province (Page and Smith, 1875). The first Board of Police was established in 1833, and by 1875 the Barton Street penitentiary was established.

Hamilton's history of industry and expansion was turbulent. Two housing crises, a cholera epidemic, debt accrued by the City Council, and rapid expansion and industry resulted in times of growth and prosperity being punctuated by periods of poverty and disease (Page and Smith, 1875). However, Hamilton established itself at the Head of the Lake as a locus for trade, industry and community development.

Although the railway boom collapsed in 1857, stove and farm-implement foundries had been attracted to Hamilton, and in the following decades the town established other industrial businesses that flourished into the mid-twentieth century (Weaver, 2019). Clothing and sewing machine manufacturing developed during the American Civil War and by the 1890s the Hamilton Blast Furnace Company was producing pig iron. In the early 1900s through to 1913, national railway construction and American branch plants serving prairie markets began a factory and residential construction boom (Weaver, 2019). During both World Wars, industries in Hamilton pivoted to produce war material and continued post-war to produce appliances, automobiles and houses. However, with the closing of textile mills and knit-wear plants in the 1950s and 1960s, Hamilton became more dependent on steel and related industries (Weaver, 2019). The result of increasing global competition by the end of the twentieth century saw three of the region's largest employers, Otis Elevator, Firestone and International Harvester close their local operations, and two of the major steel plants reduce their workforce by almost half (Weaver, 2019). Despite these economic setbacks, traditional manufacturing continues to support the local economy in part while the service, research and technology sectors have continued to grow (Weaver, 2019).

In the early 1970s, the creation of the Regional Municipality of Hamilton-Wentworth began the process of amalgamating all of the communities that once comprised the Head of the Lake, a process that was completed in 2001 when the Regional Municipality and its six constituent municipalities were amalgamated into the City of Hamilton (Henley, 2020).

4.3.6 John C. Munro Hamilton International Airport

The John C. Munro Hamilton International Airport (the Airport) was constructed in October 1940 as a wartime air force training station. It was originally designed as a multi-purpose military field and was used for flight training, air navigation, telegraphy, and air gunnery. After World War II, the Airport transitioned from a military establishment into a public facility (JCMHIA, n.d.). In 1972, the Canadian Warplane Heritage Museum was founded on the east side of the airport lands as a non-profit organization whose mandate includes preserving a complete collection of aircraft flown by Canadians and the Canadian military from the beginning of World War II to present (CWHM, 2022).

In the early 1980s, local politician John C. Munro, a Hamilton Member of Parliament and cabinet minister, secured a \$55 million federal investment to expand and develop the airport's facilities resulting in the airport being renamed in his honour in 1998 (CAPA, 2022). The expansion was complete by 1986 with the new facility able to accommodate larger aircraft and increased passenger and cargo traffic (JCMHIA, n.d.).

On February 15, 1993, a fire destroyed Hangar 3 and one of the Canadian Warplane Heritage Museum hangars. As a result of the fire and significance of the site, Regional Council earmarked \$12 million to develop a new museum. The new Warplane Museum opened to the public on March 2, 1996 (JCMHIA, n.d.)

In the period since its first expansion in 1981, the airport has grown to become one of Canada's largest intermodal courier / cargo gateways, a commercial passenger airport for the greater Hamilton area and a strong economic driver for the city and region (JCMHIA, n.d.). Today, the airport consists of two runways, a single terminal, various outbuildings, and encompasses 484 hectares.

4.3.7 Historical Mapping Review

A review of historical mapping and aerial photography was undertaken to understand the changing landscape and built environment within the Project study area. To determine the presence of historical features, nineteenth century historical county maps and twentieth century topographic maps and aerial photographs were reviewed. While these maps and photographs were not the only visual sources consulted for the purposes of this study, they were determined to provide the best overview of land development in the study area. It should also be noted that the absence of structures or other features shown on the historical maps does not preclude their presence on these properties. Illustrating all homesteads on the historical atlas maps would have been beyond the intended scope of the atlas and, often, homes were only illustrated for those landowners who purchased a subscription.

The earliest cartographic resource consulted was the 1859 Map of the County of Wentworth by Surtees (Figure 2, Appendix A). This map shows the original lot and concession roads in the study area much the same as they are today. Very few rivers or streams are indicated in the

study area. The settlement of Mount Hope is noted at the intersection of present day Homestead Drive and Airport Road West, with smaller lots illustrated on the northwest corner. Although the settlement of North Glanford is not named on the map, smaller lots had been subdivided on the northwest corner of the intersection of present day Upper James Street and Dickenson Road. Proprietor information is provided on the 1859 map, however the footprint of structures are not.

The 1875 Illustrated Historical Atlas of the County of Wentworth, Saltfleet Township by Page & Smith shows the Township of Ancaster to the west and Glanford to the east, divided by present day Glancaster Road (Figure 3, Appendix A). This map provides the names of occupants and shows the increased subdivision of the original lot configurations. Structures are indicated on this map, illustrating that many of the lots had been settled with farmsteads. Orchards are also shown, most of which are located in close proximity to the structures, likely farm houses. Post offices are shown and all are located in developing settlement areas such as North Glanford and Renforth. Renforth was a thriving community with two hotels, a store, lumber mill, cheese factory, church and brickyard (Green, 2004). Inns are also illustrated with one at Renforth, and another at the southwest corner of the former alignment of English Church Road, and present day Upper James Street. The settlement of Mount Hope had continued to expand by this point north and south of present day Airport Road West, likely with a number of small lots that have been shaded on the map. A collection of buildings is noted at what will become the settlement of Southcote at the intersection of Southcote Road and Book Road East. Three Mile Creek is shown entering the centre of the east side of the study area, and meandering northwest, and Twenty Mile Creek is seen entering the extreme northeast corner of the study area.

Twentieth century development of the study area is recorded in the National Defence's collection of topographic maps. The 1907 topographic map depicts the road network in the area, which continued to closely resemble present day alignments with the exception of the addition of Smith Road (Figure 4, Appendix A). Predominantly frame dwellings are noted, with some masonry structures. Further development of settlement areas is illustrated. A post office continued to be noted in Renforth and North Glanford, with new post offices being depicted in Glanford (Mount Hope) and Southcote. The settlement of Glanford had also expanded to include a Town Hall, two blacksmith shops, a hotel and a school. A cemetery is noted at North Glanford and another school is shown on the east side of present day Upper James Street, south of Twenty Road. Southcote had also continued to develop with a blacksmith and a cemetery located west of the settlement. Another cemetery is noted on the south side of present day Salem Road, which is now contained with the lands occupied by the Airport. Three Mile Creek, in addition to some smaller creeks, continue to be illustrated in the study area.

The 1938 topographic map (Figure 5, Appendix A) depicts little change from 1907 aside from the introduction of additional residences, particularly along present day Airport Road West and the north portion of Upper James Street (Highway 6). The introduction of the Mount Hope Aerodrome (later the John C. Munro Hamilton International Airport) can be seen in the 1954 aerial image (Figure 6, Appendix A) fronting present day Airport Road West, located in the Township of Glanford, in the southeast portion of the study area. The road network has changed little aside from the removal of the west leg of English Church Road to accommodate the Mount Hope Aerodrome. The rural, agricultural and residential land uses surrounding the airport are still visible, as are the settlement areas of Southcote, Renforth, Mount Hope, and North Glanford. Three Mile Creek is shown entering the east side of the study area and terminating at the airport lands.

The 1996 topographic map (Figure 7, Appendix A) demonstrates the increased development of the study area, however agricultural and residential land uses are still dominant. Most notably, the Hamilton Airport has been expanded to the west, interrupting Glancaster Road as well as Smith and Southcote Road, as well as to the east towards Highway 6. A hydro corridor has been added traveling east-west in the north portion of the Township of Ancaster. Residential development has increased and tree farms and nurseries are indicated in the southwest portion of the study area. The settlement areas of Southcote, Renforth, Mount Hope and North Glanford are still illustrated, and the settlement of Garners Corners is illustrated on the north side of the intersection of Garner Road East and Southcote Road, adjacent to the study area boundary. This settlement was not noted on the other mapping reviewed in this section.



5.0 EXISTING CONDITIONS

5.1 Property Visits

A property visit was conducted via publicly accessible lands on December 6, 2021 by Lindsay Benjamin, Cultural Heritage Specialist, to record the existing conditions of the Project study area and all adjacent properties. The field review was preceded by a review of available historical and current aerial photographs and maps. These photographs and maps, as well as the inventory information provided by the City of Hamilton related to BHRs and CHLs, were reviewed for any potential heritage resources that may be extant in the study area. The existing conditions of the study area are described below. Eighty-six (86) BHRs and 73 CHLs were identified and are presented in **Table 2** in Appendix B. Mapping of these BHRs and CHLs is presented in Figure 8 and 9 in Appendix D.

5.2 Description of Existing Conditions

The study area is composed of primarily residential and agricultural land uses, with the exception of the Airport. Residential land uses are concentrated along the north and east boundaries of the study area. The road network in the study area is largely unchanged since the mid-nineteenth century with the exception of road interruptions caused by the introduction of the Airport.

Described beginning roughly in the northwest and moving to the southeast, the study area consists of Fiddlers Green Road, a portion of Carluke Road East, Butter Road East, Book Road East, Highway 6, Southcote Road, Smith Road, Garner Road East, Glancaster Road, Airport Road, White Church Road West, Twenty Road West, Dickenson Road West, Upper James Street, Talbot Lane, Homestead Drive, and all adjacent properties (Figure 1).

5.2.1 Fiddlers Green Road

Fiddlers Green Road is a two-lane paved arterial road that travels roughly north-south, marking the west boundary of the study area. The road is constructed to a rural cross-section with narrow gravel shoulders. Fiddlers Green Road in the north portion of the study area is lined with large lot residential and commercial land uses (Image 1). Some of the residences in this area date to the nineteenth century. From Book Road south through the remainder of the study area, Fiddlers Green Road is dominated by agricultural farmsteads, many dating to the nineteenth century, and fields under active cultivation (Image 2). The topography of the road is generally flat with some gently rolling hills.



Image 1: Fiddlers Green Road looking south from Garner Road East



Image 2: Fiddlers Green Road looking north from Carluke Road East

5.2.2 Carluke Road East

Carluke Road East is a two-lane paved arterial road that travels roughly west-east, marking a small portion of the south boundary of the study area (Image 3). The road is constructed to a rural cross-section with narrow gravel shoulders. Carluke Road East is lined with agricultural farmsteads, many dating to the nineteenth century, and fields under active cultivation. The topography of the road is generally flat.



Image 3: Carluke Road looking east from Fiddlers Green Road

5.2.3 Butter Road East

Butter Road East is a two-lane paved arterial/collector road that travels roughly west-east across the west half of the study area. The original alignment of the road has been interrupted slightly as a result of the introduction of Airport Road. Butter Road East curves slightly south before transitioning to Airport Road. Highway 6 has also been introduced, passing beneath the built-up Butter Road Underpass.

The road is constructed to a rural cross-section with narrow gravel shoulders. Butter Road East, west of Airport Road, is lined with agricultural farmsteads, many dating to the nineteenth century, and fields under active cultivation (Image 4). This portion of the road is quite flat. East of Airport Road, the street is distinguished by large lot rural residential properties, many with dense tree cover and some with ponds. The topography of this portion of the road is characterized by a series of rolling hills under cover of the tree canopy, providing picturesque views (Image 5).



Image 4: Butter Road East looking east from Fiddlers Green Road

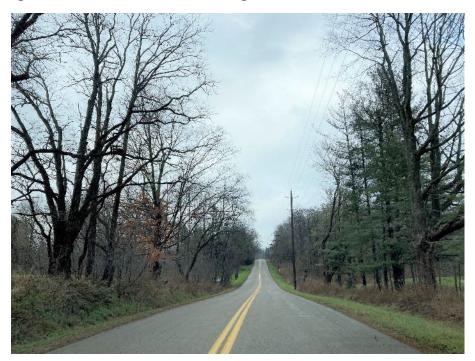


Image 5: Butter Road East looking east from Airport Road

5.2.4 Book Road East

Book Road East is a two-lane paved collector road that travels roughly west-east across the west half of the study area (Image 6). Highway 6 has been introduced, intersecting Book Road East at grade through a signalized intersection. Aside from this improved intersection, the road is constructed to a rural cross-section with narrow gravel shoulders. Book Road East is lined with agricultural farmsteads, many dating to the nineteenth century, as well as fields under active cultivation and rural residential properties. The nineteenth century settlement of Southcote is located at the intersection of Southcote Road and Book Road East and a concentration of smaller lot residences, as well as one institutional land use, characterize this portion of the road. Book Road East is quite flat aside from a rise and fall in elevation in proximity to the east side of Highway 6 (Image 7).



Image 6: Book Road East looking east from Fiddlers Green Road



Image 7: Book Road East looking west to Highway 6

5.2.5 Highway 6

Highway 6 is a two-lane, paved, provincial highway that was introduced to the study area landscape in the twenty-first century (Images 8 and 9). The highway travels north-south through the west side of the study area, before curving to the east south of Butter Road East and exiting the study area east on White Church Road West. Highway 6 is constructed to a rural cross-section with wide gravel shoulders. As a provincial highway, there are no active entrances to properties along its alignment. Intersections with Highway 6 have been signalized or underpasses have been constructed. Highway 6 is bordered by agricultural fields, wood lots and a hydro corridor.



Image 8: Highway 6 looking north from Butter Road East Underpass



Image 9: Highway 6 looking south from Butter Road East Underpass

5.2.6 Southcote Road

Southcote Road is a two-lane paved collector road that travels roughly north-south across the west portion of the study area. The road is constructed to a rural cross-section with narrow gravel shoulders. The original alignment of the road has been interrupted through the introduction of the Airport Road, which now bisects a portion of the road between Book Road East and Butter Road East.

Southcote Road is lined with agricultural farmsteads, many dating to the nineteenth century, fields under active cultivation, and rural residential properties. The nineteenth century settlement of Southcote is located at the intersection of Southcote Road and Book Road East and a concentration of smaller lot residences, as well as one institutional land use, characterize this portion of the road (Image 12).

Southcote Road between Butter Road East and the Airport lands is particularly scenic due to the rolling topography and dense tree and vegetation cover closely lining the road (Image 10). The north portion of Southcote Road is relatively flat (Image 11).



Image 10: Southcote Road looking north from Butter Road



Image 11: Southcote Road looking north from Book Road East



Image 12: Southcote Road looking south from Book Road East to collection of houses in Southcote settlement area

5.2.7 Smith Road

Smith Road is a two-lane paved local road that travels roughly north-south across the west portion of the study area (Images 13 and 14). The road is constructed to a rural cross-section with narrow gravel shoulders. The original alignment of the road has been interrupted through the introduction of the Airport, which now bisects the road between Book Road East and Butter Road East.

Smith Road is lined with agricultural farmsteads, many dating to the nineteenth century, fields under active cultivation, and rural residential properties. The topography of the road is relatively flat.



Image 13: Smith Road looking north from Book Road East



Image 14: Smith Road looking south from Book Road East

5.2.8 Garner Road East

Garner Road East is a two-lane paved arterial road that travels roughly west-east, marking approximately 4.45 km of the north boundary of the study area (Images 15 to 17). West of Highway 6, the road is constructed to a rural cross-section with narrow gravel shoulders. East of Highway 6, curbs are introduced. Some small stretches of Garner Road include sidewalks.

Garner Road East is primarily lined with residential, agricultural, commercial and institutional land uses. Many of the residences that line the road date to the nineteenth century, some of which are remnant farmsteads. Actively cultivated agricultural fields characterize portions of the south side of Garner Road. A concentration of nineteenth century resources, such as residences, churches and cemeteries are clustered on Garner Road East between Fiddlers Green Road and Highway 6 (Image 15). This section of the road also contains portions with tree cover. More contemporary residential developments are also observed east of Highway 6, on the north side of Garner Road East.

The topography of the road is generally flat with some gently rolling hills and one notable rise in elevation in proximity to Highway 6.



Image 15: Garner Road East looking east from Fiddlers Green Road



Image 16: Garner Road East looking east from Smith Road



Image 17: Garner Road East looking west from Glancaster Road

5.2.9 Glancaster Road

Glancaster Road is a two-lane paved arterial/collector road that travels roughly north-south through the centre of the study area and marks a small portion of the east study area boundary (Images 18 to 20). The road is constructed to a rural cross-section with narrow gravel shoulders. The original alignment of the road has been interrupted through the introduction of the Airport, which now bisects the road between Dickenson Road West and Butter Road East.

The west side of the north portion of Glancaster Road is lined with contemporary residential subdivisions and the east side is characterized by agricultural fields. This portion of the road also contains institutional land uses on both sides (a church and school). Between Twenty Road and the Airport lands, the east side of Glancaster Road is lined with small lot, late twentieth century residences, while the west side contains agricultural farmsteads and large lot residences set in dense wood lots. South of the Airport lands, Glancaster Road is primarily lined with agricultural farmsteads, many dating to the nineteenth century, and fields under active cultivation.

In the south portion of the study area, the road has been built up and passes over Highway 6 via an underpass. The topography of the road contains some gently rolling portions but is generally flat.

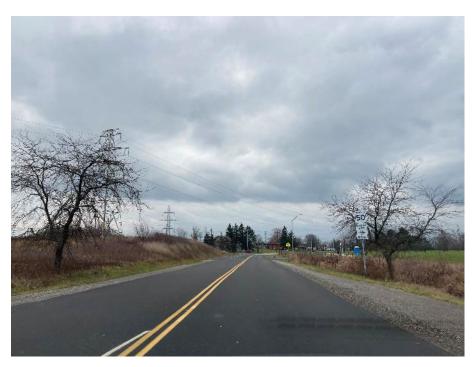


Image 18: Glancaster Road looking south from Garner Road East



Image 19: Glancaster Road looking north from Butter Road East, note airport lands to east



Image 20: Glancaster Road looking north from White Church Road West

5.2.10 Airport Road West

Airport Road West is a two-lane paved arterial/collector road that travels roughly west-east through the study area (Images 21 and 22). The west extension of Airport Road beyond Glancaster Road, connecting to the Butter Road East alignment, was constructed sometime between 1954 and 1996. The road is constructed to a rural cross-section with narrow gravel shoulders. Sidewalks are included on a portion of the south side of Airport Road, west of Homestead Drive.

Airport Road West is lined with a mix of land uses. Most notable, and its namesake, is the Airport, which abuts the north side of Airport Road between Glancaster Road and East Cargo Road. The Airport lands also contain the Canadian Warplane Heritage Museum, KF Aerospace, and the recently constructed Mohawk College Centre for Aviation Technology.

The west and south side of Airport Road West are characterized by agricultural land uses, many of which have been recently cleared for future development. The west side of Airport Road is lined with a mid-twentieth century residential development. The intersection of Airport Road and Homestead Drive is the crossroads of the nineteenth century settlement of Mount Hope.

The topography of Airport Road is flat.



Image 21: Airport Road looking west toward airport lands from East Cargo Road



Image 22: Airport Road looking east toward Homestead Drive

5.2.11 White Church Road West

White Church Road West is a two-lane paved collector road that travels roughly west-east, marking a portion of the south boundary of the study area (Images 23 and 24). The road is constructed to a rural cross-section with narrow gravel shoulders. White Church Road West is lined with nineteenth century agricultural farmsteads, fencerows, and fields under active cultivation. The topography of the road is rolling and lined with mature trees, providing picturesque views of the surrounding agricultural landscape (Image 23).

White Church Road West intersects with Highway 6 and is carried over the highway via an elevated underpass.



Image 23: White Church Road West looking east from Fiddlers Green Road, note the rolling topography



Image 24: White Church Road West looking east from Glancaster

5.2.12 Twenty Road West

Twenty Road West is a two-lane paved collector road that travels roughly west-east, marking a portion of the north boundary of the study area (Image 25). The road is constructed to a rural cross-section with narrow gravel shoulders. The south side of Twenty Road West is characterized by large lot nineteenth century agricultural farmsteads with significant setbacks from the road. A majority of these farmsteads are now vacant and proposed for redevelopment (Image 26). The north side of Twenty Road West is lined with late twentieth century and contemporary residences, tree lots, and a residential subdivision west of Upper James Street. The topography of the road rises slightly from east to west.





Image 25: Twenty Road West looking west from Upper James Street



Image 26: Example of nineteenth century farmsteads with long gravel drives that line south side of Twenty Road West

5.2.13 Dickenson Road West

Dickenson Road West is a two-lane paved arterial road that travels roughly west-east through the study area (Images 27 and 28). The road is constructed to a rural cross-section with narrow gravel shoulders.

Dickenson Road West is characterized by nineteenth century agricultural farmsteads, late twentieth century and contemporary residences, and some commercial land uses. The Airport lands span to the south side of the road, west of the newly constructed Aeropark Boulevard. A large Amazon distribution warehouse is located to the east of the Airport lands, at the southwest corner of Dickenson Road West and Upper James Street. A cemetery is located near the northwest corner of this intersection, indicative of the nineteenth century settlement of North Glanford. The topography of the road is relatively flat.



Image 27: Dickenson Road West looking east from Glancaster Road



Image 28: Dickenson Road West looking west from Upper James Street

5.2.14 Upper James Street

Upper James Street is a four-lane paved arterial road that travels roughly north-south, marking the east boundary of the study area (Images 29 and 30). The road is constructed to a rural cross-section with narrow gravel shoulders and does not include sidewalks. Upper James Street contains a mix of land uses. Portions of the street between Twenty Road and Dickenson Road are lined with collections of early twentieth century and post-war residences. Large commercial and industrial land uses are interspersed between the residences in these areas, as are nineteenth century farmsteads and an estate. This portion of Upper James street appears to be in transition with many cleared former agricultural properties and vacant residences.

The intersection of Upper James Street and Dickenson Road represents the nineteenth century settlement of North Glanford (Image 30). Little of this former settlement remains visible on Upper James Street aside from two nineteenth century residences on the west side of the street. South of Dickenson Road, Upper James Street is characterized by large lot industrial and commercial land uses such as an Amazon distribution warehouse. Portions of agricultural fields line the street as do clusters of late nineteenth century rural residential lots.

South of English Church Road East on the east side of Upper James Street, a golf course and agricultural commercial land use are present, along with a nineteenth century church and cemetery. In the south end of Upper James Street, the road alignment curves east to bypass Homestead Drive and the settlement of Mount Hope. The topography of the road is generally flat.



Image 29: Upper James Street looking south from Twenty Road West



Image 30: Upper James Street looking south from Dickenson Road

5.2.15 Talbot Lane

Talbot Lane is a narrow, single lane, tar and chip local road constructed to a rural cross-section that extends approximately 200 m west from Upper James Street before terminating (Image 31). The lane is lined with a small number of large and small lot residences dating to the nineteenth and twentieth century. The land to the west of Talbot Lane is characterized by actively cultivated agricultural fields.



Image 31: Talbot Lane looking west from Upper James Street

5.2.16 Homestead Drive

Homestead Drive is a two-lane paved collector road that travels roughly north-south, marking a portion of the east boundary of the study area (Images 33 and 34). The road is constructed to a rural cross-section with narrow gravel shoulders, allowing little room for widening. A sidewalk is located on a portion of the east side of the road. An entrance sign and feature, containing an airplane, are located at the north boundary of the settlement, paying homage to the aeronautical history of the area (Image 32).

Homestead Drive is located in the nineteenth century settlement of Mount Hope and is lined with a number of nineteenth century structures including residences, churches, shops, recreational and commercial buildings. Most of the commercial buildings are clustered near the intersection of Homestead Drive and Airport Road. Contemporary twentieth century residences are interspersed among the older structures. Actively cultivated agricultural fields flank the properties fronting Homestead Drive. Mature trees are growing in close proximity to the road throughout the settlement area. The topography of the road is generally flat with some gently rolling hills.



Image 32: Entrance feature to settlement of Mount Hope, Homestead Drive looking south from Upper James Street



Image 33: Homestead Drive looking north to Upper James Street



Image 34: Homestead Drive looking south toward Airport Road

5.3 Previous Cultural Heritage Assessments

A number of cultural heritage assessments have been completed for portions of the study area, identifying and evaluating BHRs or CHLs with known or potential CHVI. A summary of identified EA and planning applications completed in the study area and on file with the City of Hamilton are summarized below (it should be noted that this list is not exhaustive and other cultural heritage reporting may exist for the study area):

- AEGD Windshield Survey. City of Hamilton, July 2021.
- Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment, Glancaster Road Municipal Class EA Phase 3 and 4, City of Hamilton (Draft). AECOM, September 2021.
- Cultural Heritage Impact Assessment, Twenty Road West Plan of Subdivision in Glanbrook, City of Hamilton, Ontario. Golder, September 2020.
- Documentation and Salvage Report, 9236 & 9322 Dickenson Road West, Glanbrook, *City of Hamilton*. Megan Hobson, March 2020.
- Cultural Heritage Screening Report, Twenty Road West Plan of Subdivision, Glanbrook, City of Hamilton, Ontario. Golder, December 2019.
- Documentation and Salvage Report, 167 Book Road East [Book House], Ancaster. Megan Hobson, November 2018.

- Cultural Heritage Resource Assessment: Built Heritage Resources and Cultural Heritage Landscapes, Existing Conditions – Impact Assessment, Ancaster Elevated Water Reservoir Municipal Class Environmental Assessment, City of Hamilton, Ontario. ASI, September 2016 (Revised November 2016 and January 2018).
- Preliminary Cultural Heritage Resource Assessment: Built Heritage Resources and Cultural Heritage Landscapes, Existing Conditions, Dickenson Road, Upper James Street to Glancaster Road Municipal Class Environmental Assessment, City of Hamilton, Ontario. ASI, August 2019.
- Cultural Heritage Impact Assessment, Upper West Side Draft Plan of Industrial Subdivision in Glanbrook, City of Hamilton, Ontario. Golder, June 2018.
- Heritage Impact Assessment Report, 140 Garner Road East, City of Hamilton. MHBC, March 2018.
- Cultural Heritage Impact Assessment, Lampman House, 1021 Garner Road East, Ancaster. Detritus Consulting Inc., November 2014.
- Cultural Heritage Assessment Report, Hamilton Rapid Transit A-Line. ASI, 2012.
- Documentation and Salvage Report, 345 Glancaster Road, Clearance of Draft Plan Condition #3, 25T-201002. GSP Group, November 2011.
- Cultural Heritage Assessment, 9255 Airport Road West, Hamilton, Ontario, Proposed Mountain Gate Subdivision. Chapple Heritage Services, August 2007.

5.4 Identified Built Heritage Resources and Cultural Heritage Landscapes

As described in Section 3.0, known and potential BHRs and CHLs were identified based on a review of the City's Municipal Heritage Register and Inventories, notably the AEGD Windshield Survey (2021), previously completed cultural heritage assessments in the study area, the MHSTCI checklist, and historical research and field investigations. Properties with a construction date 40 or more years old were field documented, and then evaluated as having potential CHVI based on whether they met one or more of the criteria prescribed in O. Reg. 9/06. Available mid-nineteenth to late-twentieth century maps and photographs, as well as current satellite imagery, were consulted to assist with determining the age of buildings or structures within the study area.

The properties located within and adjacent to the Project study area were assessed for this Cultural Heritage Report and 86 BHRs and 73 CHLs have been identified. These BHRs and CHLs are described and numbered (roughly beginning in the northwest of the study area and moving to the southeast) in detail in the inventory presented in **Table 2** in Appendix B and are mapped in Figure 8 and 9 in Appendix D.

6.0 PRELIMINARY IMPACT ASSESSMENT

6.1 Development Description

The AEGD is a planned development area of 551 net developable hectares of employment land per the Secondary Plan. The entirety of this area has been designed to provide for a major business park development that effectively integrates with and complements the existing John C. Munro Hamilton International Airport. The AEGD TMP (2011) has been previously completed as an element of the AEGD Secondary Plan and incorporated into the UHOP, with policies and mapping.

The development pattern within the AEGD lands has been changing from the time of Secondary Plan completion, and a review of road classifications and road right-of-way resulted in the City of Hamilton's review of the AEGD TMP (2011). As part of the City of Hamilton's review of the AEGD TMP, the City is exploring a 'do nothing' approach (Alternative 1) and an amended network (Alternative 2). Alternative 2 includes approximately 20 proposed changes to the collector and arterial road network, including removals and realignments of roadways, have triggered a requirement under the EA process for a TMP amendment to fulfill Phases 1 and 2 for Schedule B and C EA project requirements within the TMP. See Figure 8 in Appendix D for an illustration of the Project's proposed collector and arterial road network that are being considered as part of Alternative 2.

Land acquisition and landscape disturbance beyond the municipal ROW is being assumed adjacent to collector and arterial road network corridors proposed for widening, as well as new road alignments. The precise extent of property impacts is not known at this time. Due to the Appeal to the Secondary Plan that resulted in removal of "white belt" areas from the urban boundary, road network changes and expansion is only being considered for the study area east of Highway 6 and north of Airport Road. The proposed road network examined for the remainder of the study area is for future consideration only and beyond the time horizon of the existing UHOP.

The Project is also considering other future infrastructure, including Employment Supportive Centres and potential roundabouts. Employment Supportive Centres are intended to serve as small scale focal points serving a limited range of amenity uses for AEGD employees. Where practical, development within the Employment Supportive Centres will be integrated with a transit facility. Proposed locations for these centres have been illustrated on Figure 8 in Appendix D with a 100 m buffer provided to anticipate a variety of land uses (i.e., small plaza, bus stop, public washroom, etc.). Similarly, intersections under consideration for the introduction of roundabouts are also illustrated on Figure 8 in Appendix D and may be subject to change.

Temporary working spaces and laydown areas may be required adjacent to the road allowances and other proposed infrastructure to facilitate the movement and storage of equipment necessary for construction. The exact locations of these areas, if required, are not yet confirmed. Furthermore, potential construction activities such as excavation, if required, will include the use of excavators, dump trucks and heavy traffic and may require vegetation removal.

6.2 Assessment Methodology

When determining the impact a development or site alteration may have on known or identified BHRs and CHLs, the MHSTCI *Heritage Resources in the Land Use Planning Process* advises that the following "negative impacts" be considered:

- Destruction of any, or part of any, significant heritage attributes, or features¹;
- Alteration that is not sympathetic or is incompatible, with the historic fabric and appearance²;
- Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden³;
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship⁴;
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features⁵;
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces⁶; and,
- Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect a cultural heritage resource⁷.

⁶ A direct impact in the MHSCTI *Info Bulletin* 3.

⁷ In the MHSTCI *Heritage Resources in the Land Use Planning Process*, this refers only to archaeological resources, but in the MHSCTI *Info Bulletin 3*, this is an example of a direct impact to "provincial heritage property, including archaeological resources".



¹ This is used as an example of a direct impact in the MHSCTI *Info Bulletin 3*.

² A direct impact in the MHSCTI Info Bulletin 3.

³ An indirect impact in the MHSCTI Info Bulletin 3.

⁴ An indirect impact in the MHSCTI Info Bulletin 3.

⁵ An example of a direct and indirect impact in the MHSCTI Info Bulletin 3. It is a direct impact when significant views or vistas within, from or of built and natural features are obstructed, and an indirect impact when "a significant view of or from the property from a key vantage point is obstructed."

Other potential impacts to heritage attributes may also be considered, such as encroachment or construction vibration (Plate 1). Although the effect of traffic and construction vibration is not fully understood, vibrations may be detectible in buildings with setbacks of less than 40 metres from a curbside (Crispino & D'Apuzzo, 2001; Ellis, 1987; Wiss, 1981). Historic structures, particularly masonry buildings, are susceptible to damage from vibration caused by pavement breakers, plate compactors, utility excavations, and increased heavy vehicle travel in the immediate vicinity. Like any structure, they are also threatened by collisions with heavy machinery or subsidence from utility line failures (Randl, 2001:3-6).

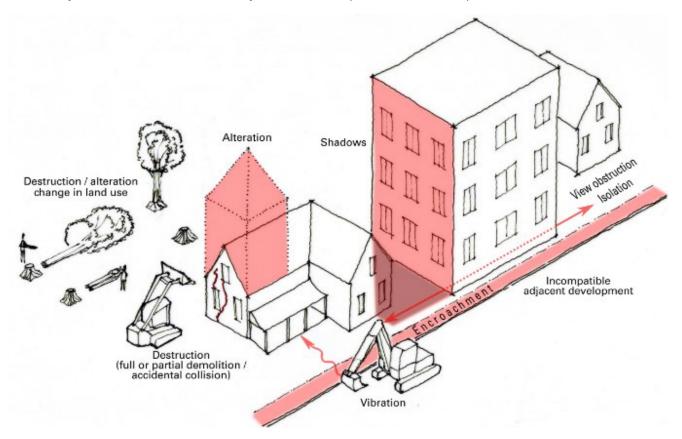


Plate 1: Examples of negative impacts to built heritage resources and cultural heritage landscapes

Although the MHSTCI *Heritage Resources in the Land Use Planning Process* identifies types of impact, it does not advise on how to describe its nature or extent. For this, the MHSTCI *Guideline for Preparing the Cultural Heritage Resource Component of Environmental Assessments* (1990:8) provides criteria of:

- Magnitude (amount of physical alteration or destruction that can be expected);
- Severity (the irreversibility or reversibility of an impact);
- Duration (the length of time an adverse impact persists);
- Frequency (the number of times an impact can be expected);
- Range (the spatial distribution, widespread or site specific, of an adverse impact); and,
- Diversity (the number of different kinds of activities to affect a heritage resource).

Since advice to describe magnitude is not included in the MHSTCI Guideline or any other Canadian guidance, the ranking provided in the *ICOMOS Guidance on Heritage Impact Assessments for Cultural World Heritage Properties* (ICOMOS, 2011) is adapted here. While developed specifically for World Heritage Sites, it is based on a general methodology for measuring the nature and extent of impact to cultural resources in urban and rural contexts developed for the *UK Highways Agency Design Manual for Roads and Bridges [DMRB]: Volume 11, HA 208/07* (2007: A6/11) (Bond & Worthing, 2016:166-167) and aligns with approaches developed by other national agencies such as the Irish Environmental Protection Agency (reproduced in Kalman & Létourneau, 2020:390) and New Zealand Transport Agency (2015).

The ICOMOS impact assessment ranking is:

- Major
 - Change to key historic building elements, such that the resource is totally altered.
 Comprehensive changes to the setting.
- Moderate
 - Change to many key historic building elements, such that the resource is significantly modified.
 - Changes to the setting of an historic building, such that it is significantly modified.
- Minor
 - Change to key historic building elements, such that the asset is slightly different.
 - Change to the setting of an historic building, such that it is noticeably changed.

- Negligible
 - Slight changes to historic building elements or setting that hardly affect it.
- No impact
 - No change to fabric or setting.

The analysis of impacts includes a summary of each impacted property's CHVI, and terms provided under conservation or mitigation recommendations are defined as follows:

- Avoid: A recommendation to avoid means to move project components to locations a distance from the identified BHR or CHL. In all cases avoidance is the preferred approach, although it is recognized that other factors may preclude selecting this option.
- Monitor for construction vibration: Although direct impacts to BHRs and CHLs may be avoided, indirect impacts from construction vibration may still present a risk. If a vibration risk is identified, the following measures are usually recommended:
 - Site control and communication: The property and specifically the footprint of the building should be clearly marked on project mapping and communicated to all project personnel for avoidance during design, construction, and subsequent operation.
 - Create a physical buffer: Temporary fencing should be erected at the nearest property line or lines to ensure that all excavation, installation, and associated vehicle traffic during construction will not accidentally impact the property.
 - Monitor for vibration impact: Continuous ground vibration monitoring should be carried out near the foundations of the building using a digital seismograph capable of measuring and recording ground vibration intensities in digital format in each of three orthogonal directions. The instrument should also be equipped with a wireless cellular modem for remote access and transmission of data. The installed instrument should be programmed to record continuously, providing peak ground vibration levels at a specified time interval (i.e., five minutes) as well as waveform signatures of any ground vibrations exceeding a threshold level that would be determined during monitoring. The instrument should also be programmed to provide a warning should the peak ground vibration level exceed the guideline limits specified (such as 8.0 mm/s). In the event of either a threshold trigger or exceedance warning, data would be retrieved remotely and forwarded to designated recipients.

Conduct a Cultural Heritage Evaluation Report or Cultural Heritage Impact Assessment

- If a potential BHR or CHL cannot be avoided and will be directly impacted by the project, a Cultural Heritage Evaluation Report (CHER) is recommended to determine if the potential resource meets the criteria for CHVI as prescribed in O. Reg. 9/06. If the CHER determines the property has CHVI, a Cultural Heritage Impact Assessment (CHIA) may be required to determine the impact of the proposed detailed design on the property's heritage attributes.
- For recognized BHRs and CHLs, and those evaluated to have CHVI, a CHIA is
 recommended to determine the impact of the proposed detailed design on the
 property's heritage attributes and recommend mitigation and conservation measures to
 reduce or avoid adverse effects. The CHIA should follow guidance provided by the
 MHSTCI and the municipal terms of reference and official plan policies.

6.3 Summary of the Preliminary Impact Assessment

The impacts of Alternative 1 (Do nothing) and Alternative 2 (Amended road network) proposed through the AEGD TMP Update on the potential and known BHRs and CHLs in the study area are assessed in **Table 3** in Appendix C. In summary, Alternative 1 (Do nothing) will have the least impact on the identified cultural heritage resources. If Alternative 2 (Amended road network) is selected, it is recommended that the alignment of the new roads and expansion of existing roads seek to avoid direct and indirect impacts to BHRs and CHLs where feasible. The preferred alignment should require as little property acquisition as possible.

7.0 CONCLUSIONS

The results of the background historical research and review of the secondary source materials, including historic mapping, revealed that the study area consists of lands that have been shaped by early agricultural settlement dating from the nineteenth and early twentieth centuries, as well as the post-war development and evolution of the John C. Munro Hamilton International Airport.

The following provides a summary of the assessment results:

- A total of 96 BHRs and 73 CHLs were identified within and adjacent to the Project study area.
- Of these, five (5) are designated under Part IV of the OHA, seven (7) are registered nondesignated properties included on the City of Hamilton's Municipal Heritage Register, 138 have been inventoried by the City of Hamilton, four (4) have been identified in previous cultural heritage assessments (there was overlap between inventoried resources and those previously identified), and 16 were identified during field review.
- Most of the identified CHLs and BHRs reflect the nineteenth century agricultural development of the study area, as well as residential development typical of the post-war construction boom.
- A large portion of the study area is composed of the John C. Munro Hamilton International Airport.
- Eight (8) cemeteries are located in the study area. All have been inventoried by the City of Hamilton aside from the Ancaster Pet Cemetery (CHL 17).
- Nineteenth century settlement areas and associated BHRs and CHLs remain in the study area and include Mount Hope, North Glanford, Southcote and Garners Corners.

The impacts of Alternative 1(Do nothing) and Alternative 2 (Amended road network) were explored in **Section 6** and **Table 3** in Appendix C. Alternative 1 (Do nothing) will have the least impact on the identified cultural heritage resources. If Alternative 2 (Amended road network) is selected, it is recommended that the alignment of the new roads and expansion of existing roads seek to avoid direct and indirect impacts to BHRs and CHLs where feasible. The preferred alignments should require as little property acquisition as possible.

8.0 RECOMMENDATIONS

Recommendations to avoid or reduce adverse impacts to the BHRs and CHLs identified in the Project study area are summarized below. Note that these recommendations are based on Golder's current understanding of the Project and may need to be revisited if there are any changes to the Project.

- When determining the preferred alternative for the network in the AEGD TMP Update, consideration should be given to alignments that directly and indirectly impact as few BHRs and CHLs as is feasible. Alignments should be selected that require as little property acquisition as possible.
- 2) Staging and construction activities should be appropriately located and/or planned to avoid impacts to the identified BHRs and CHLs.
- 3) Once preferred alternatives or detailed designs of the proposed road network are available for the study area, this report should be updated with a confirmation of impacts of the undertaking on the BHRs and CHLs identified within and/or adjacent to the study area to recommend appropriate mitigation measures. Mitigation measures may include, but are not limited to, completing a CHER, CHIA or Documentation Report, or employing suitable measures such as landscaping, buffering or other forms of mitigation, where appropriate. In this regard, provincial and municipal guidelines should be consulted for advice and further heritage assessment work should be undertaken as necessary and directed by City of Hamilton Staff.
- 4) Should construction activities occur within 40 m of any of the structures located on the identified BHRs or CHLs, the impacts of vibrations may need to be determined through an engineering assessment to ensure that there are no negative impacts to these resources. Any resulting mitigation measures should be implemented prior to construction, as needed.
- 5) Road work and associated construction may cause a temporary impact to the BHRs or CHLs in the study area. Each impacted landscape should be returned to pre-construction conditions (i.e., in-kind fence replacement), and where construction is anticipated to result in grading impacts and tree removal, post-construction landscaping with native tree species should be employed to mitigate visual impacts, resulting in minimal irreversible negative impacts to the BHRs and CHLs.

- 6) Although land acquisition and landscape disturbance beyond the municipal ROW is being assumed adjacent to collector and arterial road network corridors proposed for widening and new road alignments, the precise extent of property impacts is not known at this time. As such, CHERs and/or CHIAs have been recommended for all BHRs and CHLs with the potential to be directly, negatively impacted. CHERs are recommended to evaluate each property or landscape's potential CHVI. If a property or landscape is found to possess CHVI, a CHIA is recommended for completion prior to or during the detail design phase and before construction to determine appropriate alternatives or mitigation measures. It is recommended that City of Hamilton Heritage Planning Staff be consulted when recommending the preparation of CHERs and CHIAs.
- 7) Should future work require expansion of the Project study area or if the alternative roadway alignments are changed, a qualified heritage consultant should be contacted to confirm the impacts of the proposed work on known or potential BHRs and CHLs.



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Signature Page

We trust that this report meets your current needs. If you have any questions, or if we may be of further assistance, please contact the undersigned.

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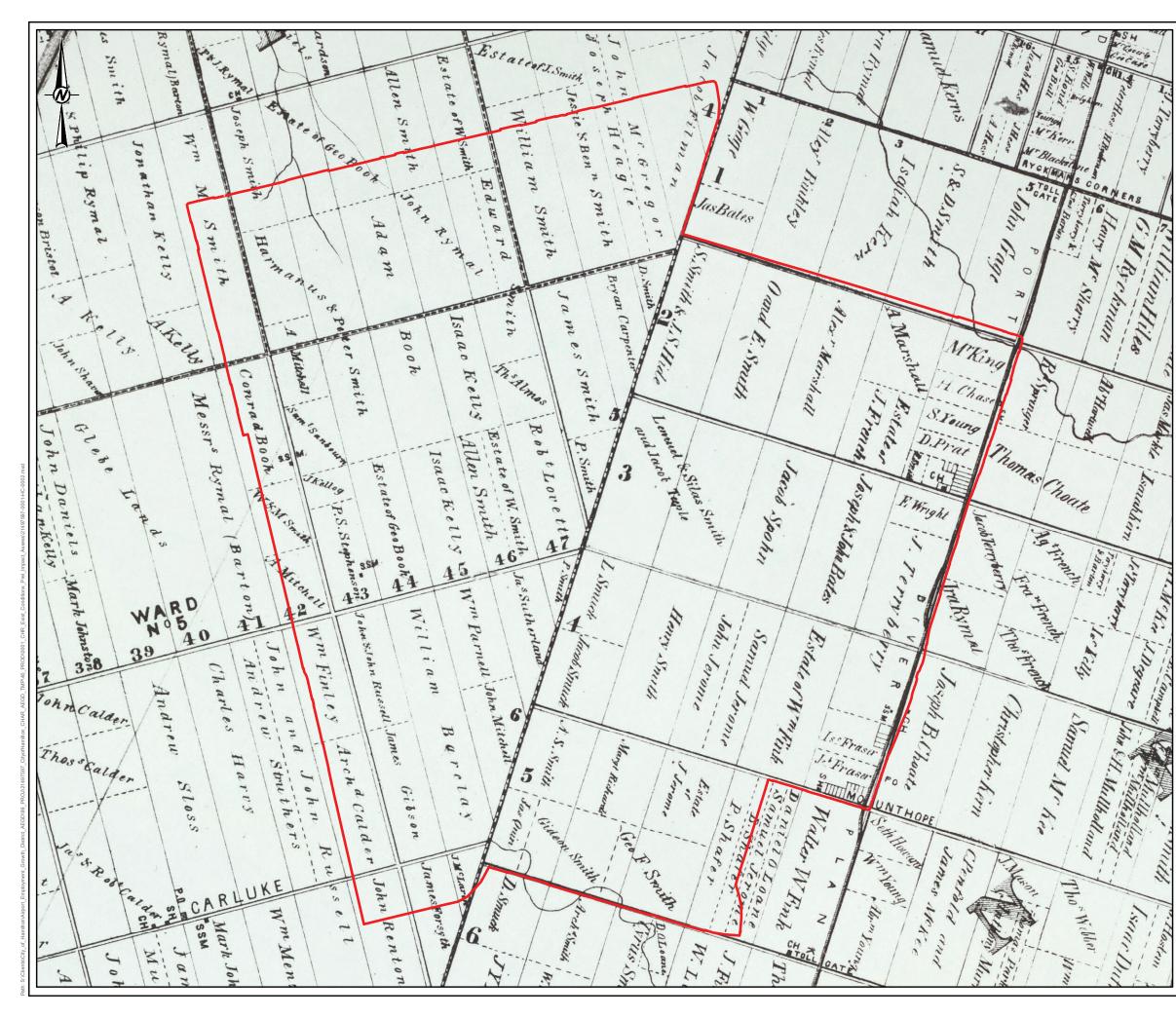
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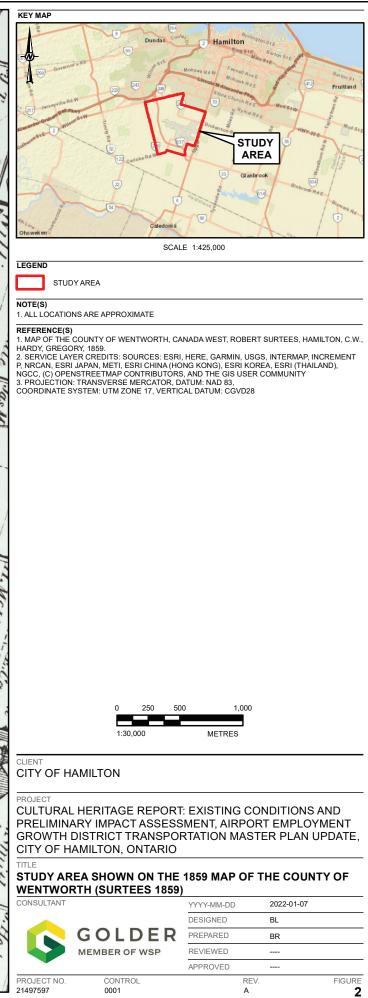


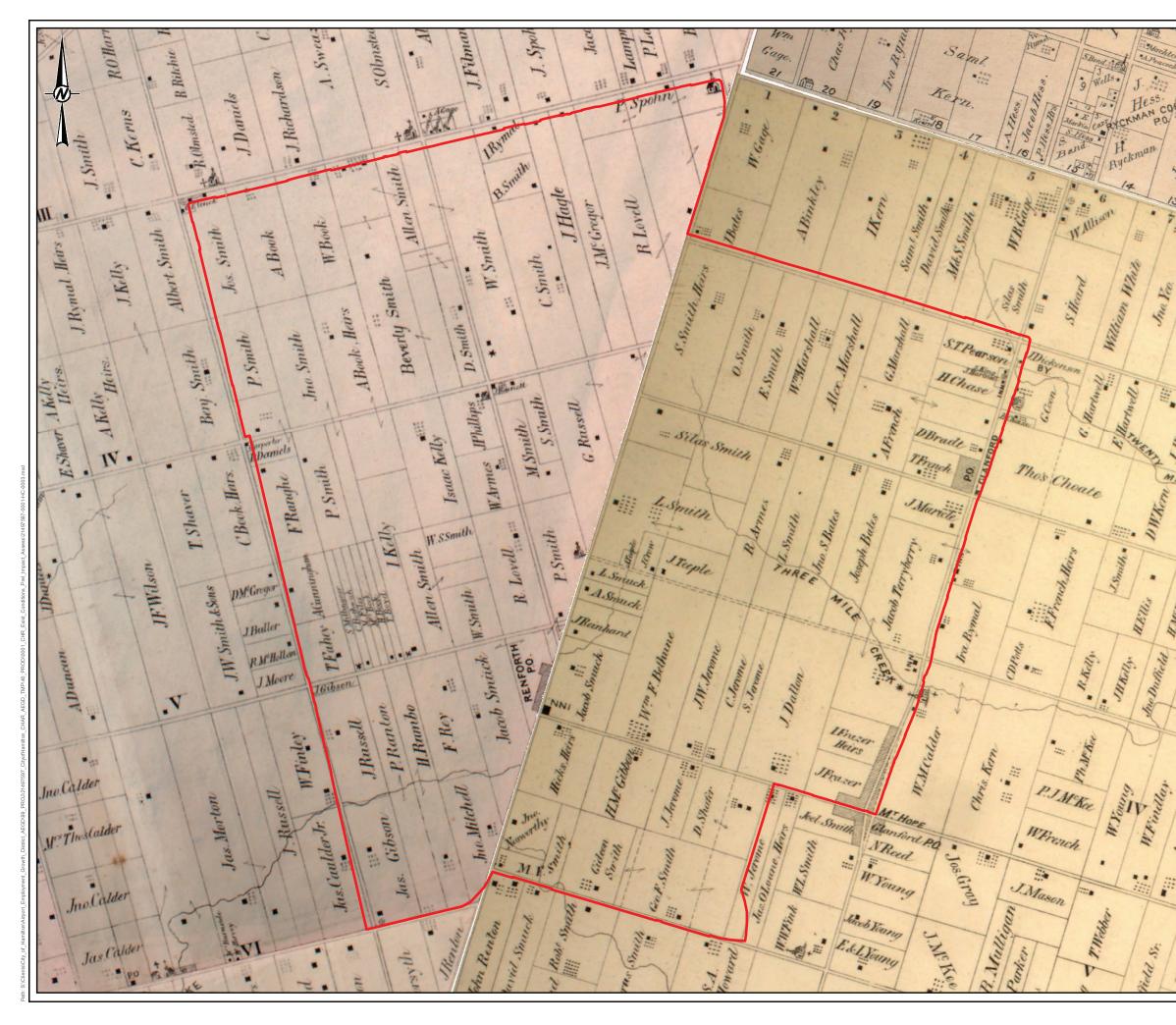
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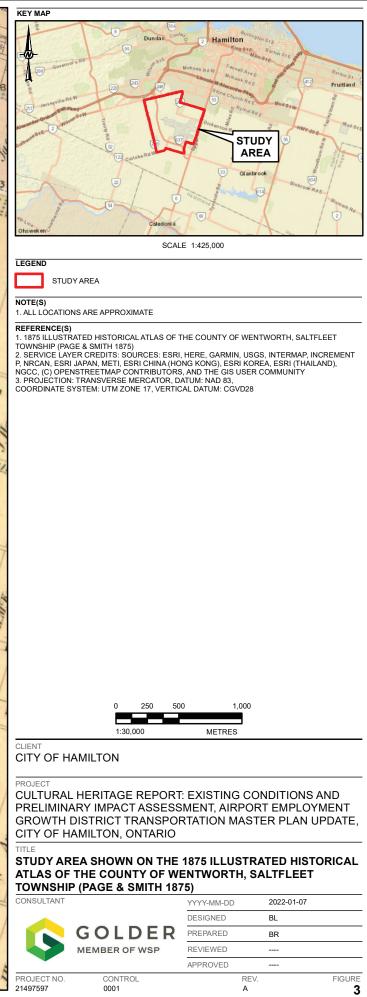
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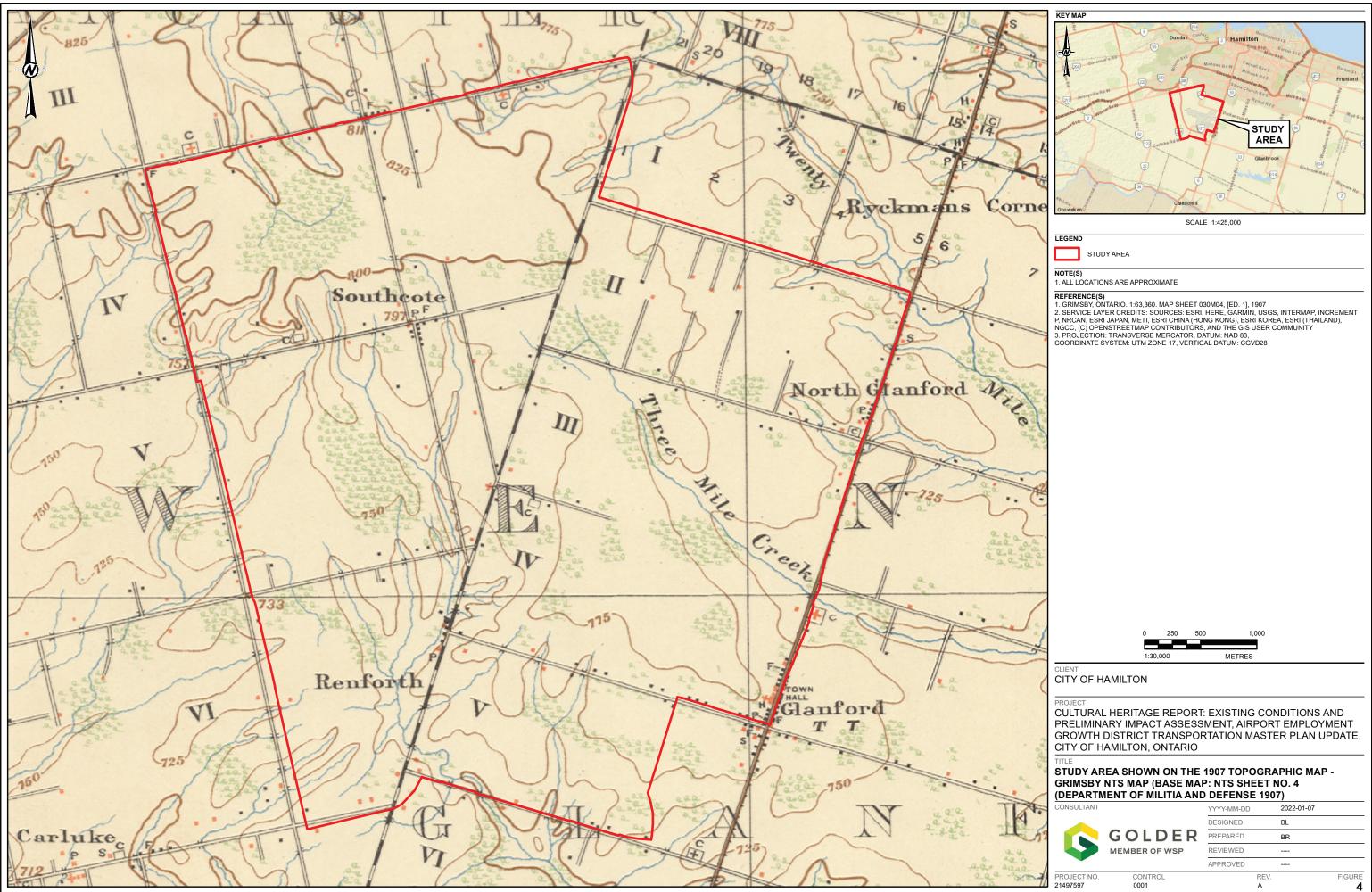


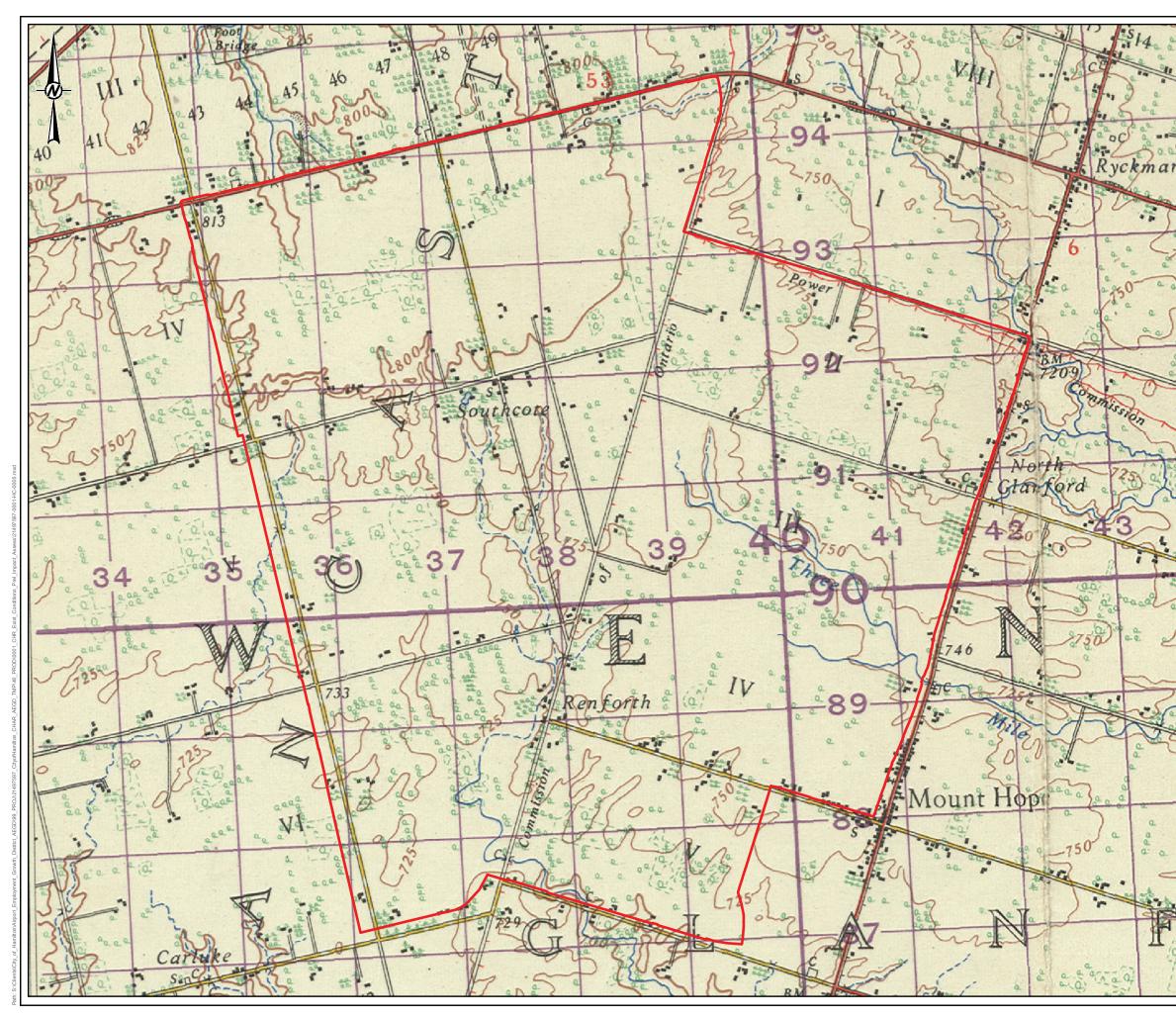


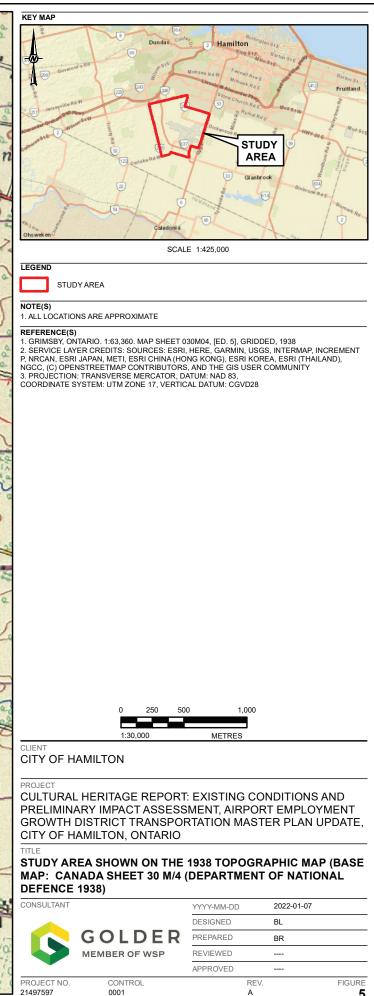




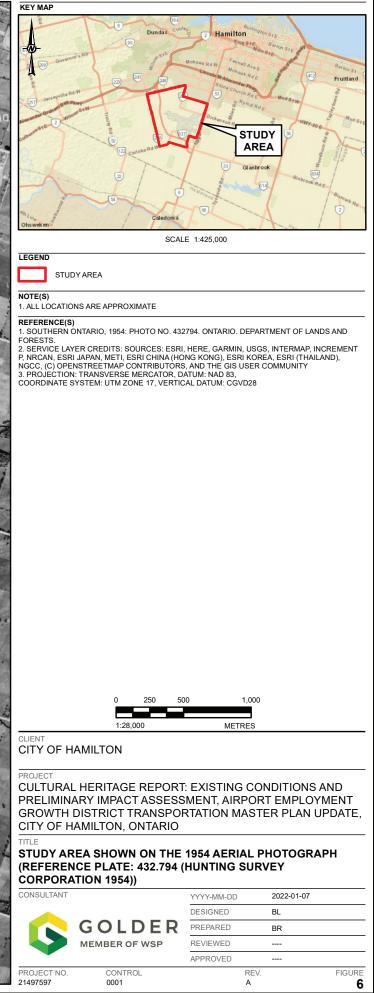




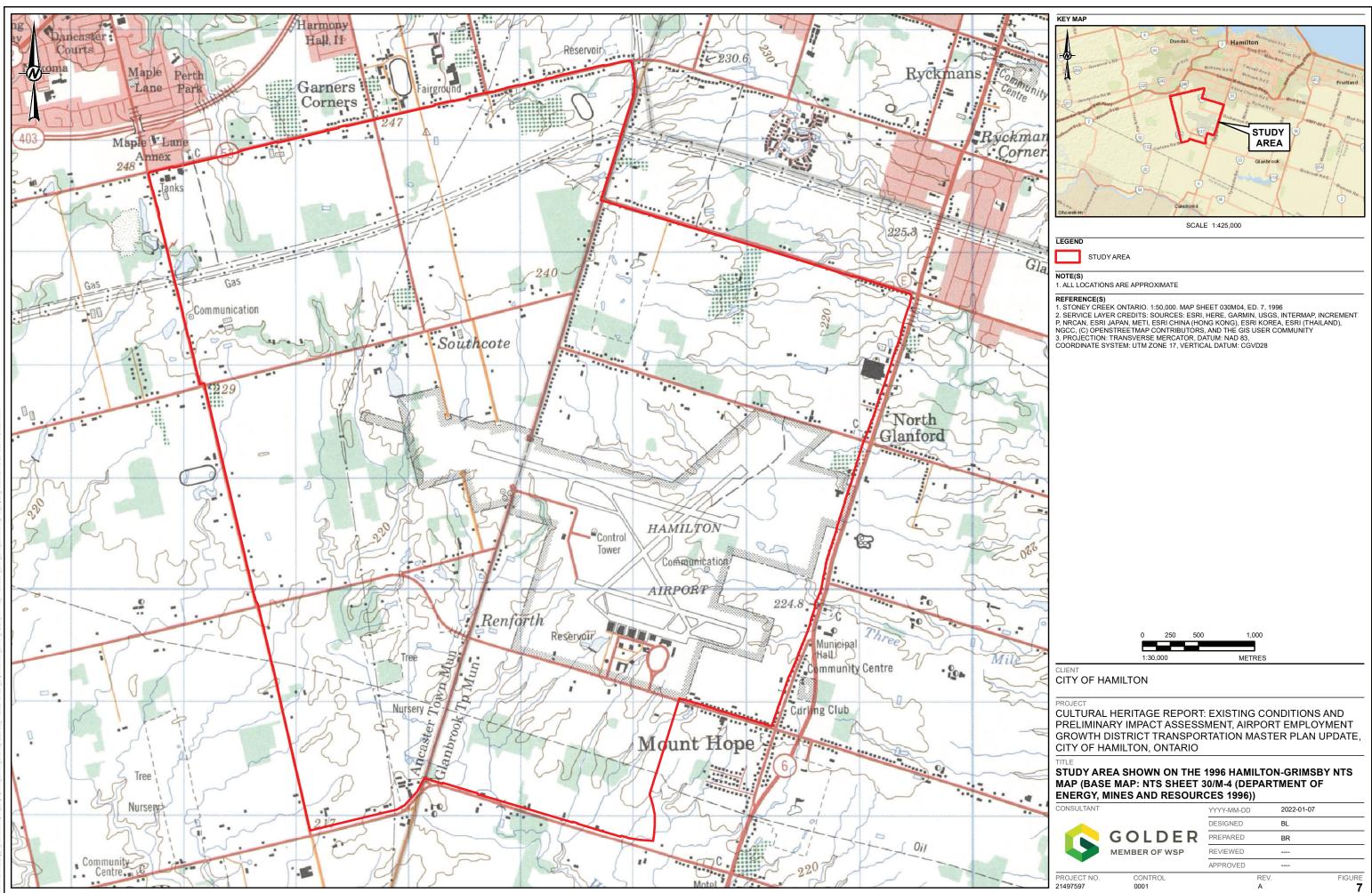








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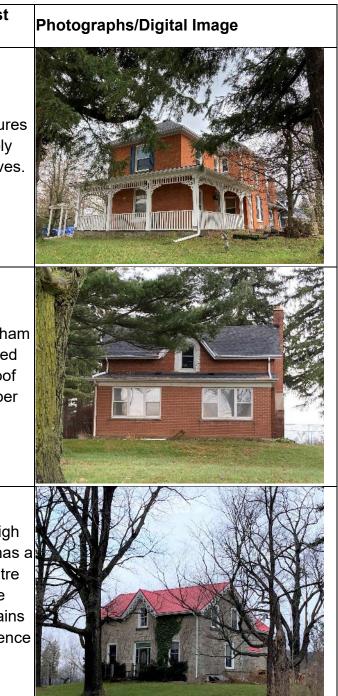
Identified Built Heritage Resources and Cultural Heritage Landscape



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
BHR 1	Residence	558 Fiddlers Green Road, Ancaster; Smith / Rowley Hous	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Heritage Research Report (Carolyn Hawley, 1980)	Constructed in 1840, the original owner was Stephen Smith. The residence is a one-and-a-half storey frame Ontario Gothic Revival house with brick exterior and a gable roof. The front facade features a three-bay arrangement with central projecting frontispiece with a gable roof and decorative bargeboard, a central door with sidelights and transom, and decorative buff brick quoining and window surrounds. It includes a rear extension. The house is surrounded by grassy open areas, mature vegetation, gardens, circulation routes and a split rail fence.	
BHR 2	Residence	588 Fiddlers Green Road, Ancaster	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report (n.d.)	Constructed in 1850, the residence is historically associated with Albert Smith. The one-and-a-half storey frame house has a cross-gable roof, gable dormer, vinyl siding and stone foundations. The projecting bay features a bay window on the first storey. The house has been altered through the addition of an external brick chimney, replacement of windows with modern glass and fake muntin, and replacement of the original porch. The property retains a concrete block garage or outbuilding with a shed roof, grassy open spaces with a variety of vegetation, circulation routes, and cultivated fields.	
BHR 3	Residence	1032 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1920, the vernacular one-storey residence is wood frame with a front gable roof clad in board and batten. The structure has been heavily modified. Located on a corner lot, the façade is oriented toward Book Road West.	

Table 2: Identified Built Heritage Resources and Cultural Heritage Landscapes with Known or Potential Cultural Heritage Value or Interest within the Study Area

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 4	Residence	1039 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1895, the residence is historically associated with the Carpenter family. The two-storey vernacular orange brick residence feature a wrap-around covered porch with decorative posts and spindles (possibly modern additions), a tall brick chimney, and a hip roof with projecting eaves The residence is surrounded by mature trees. A series of significant additions project from the south elevation.
BHR 5	Residence	1297 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1870, the property is historically associated with Gunningha (1875 Illustrated Historical Atlas of the County of Wentworth). The modified vernacular Ontario Cottage is one-and-a-half storeys with a side gable roof and projecting centre gable on the façade. The property contains a numbe of mature trees.
BHR 6	Residence	1694 Fiddlers Green Road, Ancaster	Inventoried, LACAC Heritage Report (1983)	Constructed in 1869, the vernacular Gothic Revival Cottage displays a high degree of craftsmanship. The one-and-a-half storey cut-stone structure has stone foundation, side gable roof clad in metal with a projecting front centre gable, a symmetrical front facade with a central entrance below the gable with a transom and sidelights and flanking windows. The residence contain a one-and-a-half storey rear wing with decorative bargeboard. The residen is set among a stand of mature trees.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
			Constructed in 1869, the property is associated with W. Ranton (1875 Illustrated Historical Atlas of the County of Wentworth).	
BHR 7	Commercial	1770 Fiddlers Green Road, Ancaster; Oakrun Farm Bakery	Inventoried; City of Hamilton AEGD Windshield Survey	The vernacular Ontario Cottage is a one-and-a-half storey wood frame residence with a side gable roof clad in metal with a projecting centre gable peak.
			(2021)	A large industrial bakery complex is located on the property, however the original Ontario Cottage remains to the northwest and is visible from Carlul Road. The remainder of the property is composed of agricultural fields.
BHR 8	Residence	166 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1875, the residence on the property is a vernacular expression of the Edwardian architectural style. The two-and-a-half storey structure features a bell-cast hip roof, front dormer, red brick cladding, and covered front porch with a pedimented gable supported by lonic columns. I is possible that an early-twentieth century substantial addition has been made to an existing late-nineteenth century residence. The residence is se on elevation higher than the road and is surrounded by mature vegetation.
				It's reported that the Fiddlers Green Blacksmith shop, constructed in the 1880s, was moved from the hotel site at the southeast corner of Garner Road and Fiddlers Green Road to this property. It is now a large board and batten storage shed at the rear of the property (not visible from the road) (Green, 2004).
BHR 9	Residence	343 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	The construction date of this vernacular residence is reported to be 1870. The structure appears to have been heavily modified. Built to a rectangular plan, the residence is one-storey and features a gable roof as well as a small, gable roofed addition projecting from the east elevation. Original woo windows appear to remain, however the exterior and roof cladding does no appear original. The residence is set on a forested lot with a narrow setbac from Book Road East.



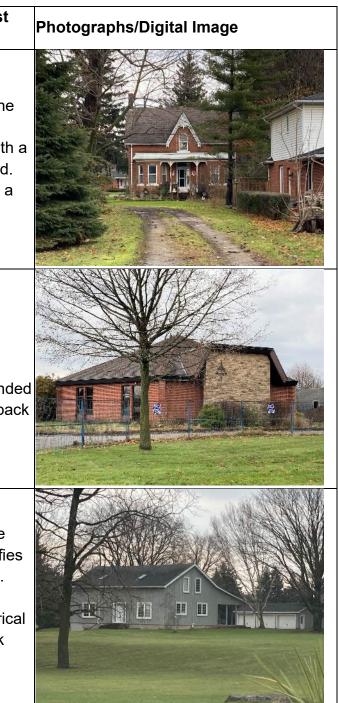
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 10	Institutional / Place of Worship	522 Book Road East, Ancaster; Christadelphian Heritage College	Inventoried (Places of Education)	This property has been the site of an elementary school since 1843. The original brick S.S. No. 6 Southcote Public School was located on donated land at the corner of Book Road East and Southcote Road. With the original school house removed, another S.S. No. 6 Southcote Public School was constructed in 1960, this time a one-storey Brutalist style structure. The school was designed by architect Charles Moeller (McIntosh and Moeller) and built by Fleetwood Construction. The current school, Christadelphian Heritage College, was established on the property in 2002 and is located in one-storey contemporary brick building with a hip roof and gable peaks.
BHR 11	Residence	558 Book Road East, Ancaster	Inventoried	Constructed in 1867, the early vernacular residence resembles a post-war cottage revival style and appears to have been modified from the original historic construction (noted by the stone foundation on the west side of the building). The residence is located in the Southcote settlement area. The one-and-a-half storey wood frame residence features a side gable roo with shallow verges, a broken-course stone foundation, flat-headed window openings (modern replacements), horizontal vinyl siding, and a raised open porch stoop.
BHR 12	Residence	1071 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1875, the one-storey, wood frame vernacular residence on the property is built to a rectangular plan and features a symmetrical façad side gable roof, and horizontal siding. A one-storey addition projects from t rear elevation. The residence is located in the Southcote settlement area.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 13	Residence	1085 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1900, the modified Ontario Cottage on the property is one- and-a-half storeys. It is of wood frame construction with a side gable roof with a steeply-pitched central gable over the entrance. A one-storey addition has been added to the south elevation. The property contains two detache accessory structures, a garage and a shed. The residence is located in the Southcote settlement area.
BHR 14	Residence	911 Smith Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1850 in the Georgian style, the property is an early and rare example of the architectural style in Ancaster. The two-and-a-half storey re brick residence features a side gable roof and a symmetrical three-bay facade with a central entrance and covered porch. A number of matures ar located on the property.
BHR 15	Residence	4 Garner Road East, Ancaster; Clink House	(Carolyn Hawley, 1980); City	Constructed in 1850 for the original owner George Clink and his family, the property is located on the prominent corner of Garner Road East and Fiddlers Green Road with a narrow setback, most notably from Garner Roa East. The style of the residence on the property is representative of a Gothic Revival Ontario Cottage. The one-and-a-half storey red brick residence displays a high degree of craftsmanship through its bargeboard detailing, projecting front gable, and three-bay facade with a central entrance, transc and sidelights.



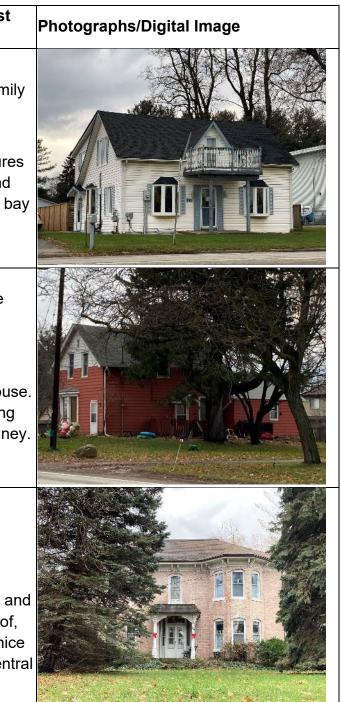
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 16	Residence	45 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1880, the residence on the property is representative of the Gothic Revival Ontario Cottage style of architecture. The one-and-a-half storey brick structure features a side gable roof, central front entrance with projecting gable above, a covered front porch, and decorative bargeboard. The residence has a deep setback from Garner Road East, accessed by a dirt drive, and is buffered by a 1950s subdivision on both sides.
BHR 17	Place of Worship	70 Garner Road East, Ancaster; Ancaster Christian Reformed Church	Inventoried, Volume 7A; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018)	Constructed in 1976, the property has historical associations with the Christian Reformed Church. The one-storey red brick structure has been built to a hexagonal plan, reflective of the Modern architectural style. A decorative limestone wall is centred on the façade and beneath the extend roofline. An addition projects from the south elevation. The church is setba from Garner Road East on a grass lot with a large parking lot to the rear.
BHR 18	Residence	114 Garner Road East, Ancaster; Book House	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report Volume 6 (1985); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1850, the property is associated with the Book family. The vernacular residence is an early structure in Ancaster. MPAC data identifie a c.1850 construction date, but previous research suggests c.1805-1810. The one-and-a-half storey wood frame residence is clad in siding and features a side gable roof with a rear sloped roof addition and a symmetric façade. The property contains a tree-lined drive and the house is setback from Garner Road East on a large, grassed lot.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 19	Residence	243 Garner Road East, Ancaster (former address 243 Highway 53); Richardson-Hyslop- Frebold House	Designated under Part IV of the OHA (By-Law No. 85-90); 140 Garner Road East, Hamilton HIA (MHBC, 2018); LACAC Report Volume 5 (n.d.)	Reasons for Designation (1985): This building known as 243 Highway 53 East in the Town of Ancaster has historical and architectural value and interest. This home is one of the best remaining Gothic-Revival style houses in Ancaster and a monument to the early Scottish masons, with the pointed arch windows and front bay extended slightly forward and the picturesque façade completed with the recessed front door, side lights, transom and flanking 6/6 windows. The da of construction is between 1858-1867, erected by John Richardson who liv here until 1883 and then his daughter Jennet and husband David A. Hyslog until 1925 who operated a canning factory, in this area. Heritage Attributes: - all exposed stone and stone facades of walls and foundations; - cornices, soffits, fascia; - all exterior windows including sash, frames, sills; - all exterior wooden architectural fenestrations such as the side porch, roo structure and the side doors with jambs and sills; - the front door and back door including side lights and fanlight and all the related woodwork and sill both of inner and outer doors; - the finial above central gable; and - the exterior chimneys.
BHR 20	Residence	378 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1920, the residence on the property is representative of a vernacular Farm House. The red brick two-and-a-half storey structure features a stone foundation, side gable roof with a projecting front dormer, tall side brick chimney, decorative brickwork, segmental and round-headed window openings with brick voussoirs and stone lug sills, and a pedimented covered front porch supported by columns.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 21	Residence	581 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1900, the property may be associated with the Garner familias indicated on the 1875 Wentworth County Atlas, Ancaster Township. The property's style is representative of the vernacular Ontario Cottage architectural style. The one-and-a-half storey wood frame structure feature a side gable roof, central front entrance with a projecting gable above and flanking windows. The façade has been modified with the introduction of bawindows. The residence has a narrow setback from Garner Road East.
BHR 22	Residence	701 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1900, the property may be historically associated with the Filman family as the 1875 Wentworth County Atlas, Ancaster Township illustrates the J. Filman farm house in the approximate location of this building. The residence on the property is representative of a vernacular Farm Hous The one-and-a-half storey wood frame structure is clad in aluminum siding and features a gable roof, a projecting side gable peak, and a brick chimne The residence has a narrow setback from Garner Road East.
BHR 23	Residence	716 Garner Road East, Ancaster; Rymal House	Inventoried; LACAC Report Volume 5 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1879, the property is believed to be associated with the Rymal family. A Rymal farm house is shown in this location in the 1875 Wentworth County Atlas for Ancaster Township. The property's expression is rare and representative of the Italianate architectural style. The property displays a high degree of craftsmanship at artistic merit, displayed through the two-storey massing, truncated hip roof flanking three-sided projecting bays with hip roofs, ornate bracketed cornic with decorative fascia, segmentally-arched window openings, and the cent covered front entrance with decorative wooden posts and bargeboard.



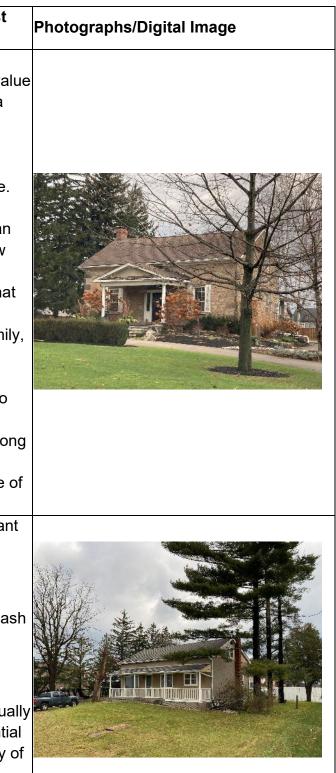
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 24	Residence	792 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	MPAC data suggests that the residence on the property was constructed ir 1945, however the design of the home suggests it dates to the turn of the twentieth century. The one-and-a-half storey residence features a tall hip roof with a projecting front gable window, a tall brick side chimney, and a central entrance with flanking windows. The property contains a number of mature trees.
BHR 25	Residence	819 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1900, the residence on the property is a unique vernacular structure. The two-storey house features red brick cladding on the first storey, a gambrel roof, covered front porch, and a window in the façade of the gambrel roof. It appears that a rear addition has been added to the nor elevation. The property contains a number of mature trees.
BHR 26	Residence	910 Garner Road East, Ancaster	Inventoried; LACAC Report Volume 5 (n.d.)	The property contains a two-storey vernacular Georgian style residence constructed in 1823, and represents an early structure in Ancaster. The residence is clad in vinyl siding and the side gable roof is clad in steel. The asymmetrical façade includes an evenly spaced fenestration and an entrance with a pedimented covered porch. The residence is setback from Garner Road East on a heavily treed lot.



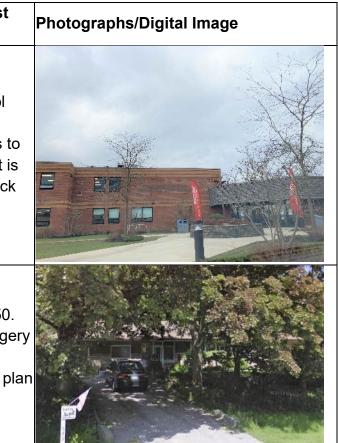
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 27	Commercial	944 Garner Road East, Ancaster; Bennett's Apple & Cider	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1927, the property, an early twentieth century farmstead, is associated with Bennett's Apple and Cider, established c.1911. The residence on the property is a two-and-a-half storey vernacular Georgian structure. The building is of red brick construction and features a bell-cast hip roof with flanking chimneys, a covered front porch, and segmentally-arched window openings with brick voussoirs. The rear of the property contains various detached agricultural outbuildings as well as orchards.
BHR 28	Residential	992 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1953, the residence on the property is representative of a vernacular Period Revival cottage. The one-and-a-half storey wood frame structure features a symmetrical façade with a centrally placed entrance w a gable roof, two flanking window bays, flanking gabled dormers, a central chimney, and a side wing.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 29	Residence	1021 Garner Road East, Ancaster; The Lampman Stone House		Statement of Cultural Heritage Value or Interest (2020): The property at 1021 Garner Road East, Ancaster has cultural heritage val as a stone dwelling built between 1854-1858 by John Lampman and as a representative example of Neo-Classical architecture with Georgian and Classical Revival elements. The Lampman House includes decorative quoins, voussoirs, sidelights, and a transom window. The modest, symmetrical plan was typical of Loyalist architecture in Ontario at the time. The Lampman family were German-American immigrants who settled in British Canada following the American Revolutionary War. John Lampman and his family were formational members of the establishment of the New Connection Methodist sect in Canada, a Protestant denomination which seceded from the Wesleyan Methodist Church. Some sources indicate that the sect's first meeting in Canada was held in the Lampman House. The property is significant in its historical associations with the Lampman Family one of the region's earliest settler families and United Empire Loyalists. Contextually, the property was once part of a much larger parcel of land granted to Matthias Lampman in 1792-93. The Lampman House was also once located adjacent to a frame house built by Peter Lampman in 1896 (since demolished) at 1061 Garner Road East. The property is located alor Garner Road East, formerly known as "Methodist Row" and is nearby a number of historic churches forming part of this unique cultural landscape or religious settlement.
		1157 Garner Road East, Ancaster	EA CHR (AECOM, 2021); Previously Identified by the Ancaster Village Heritage	Constructed in 1863, the property is associated with a potentially significan theme (pre-Confederation development) and may be associated with a significant family, the Olmsteads.
BHR 30	Residence			As a vernacular Farm House, the residence's style, type or expression is early. The one-and-a-half storey wood frame structure is clad in pebble das and features a side gable roof and covered full-length front porch, and modified window openings. The residence is built on slightly raised topography, flanked by mature trees.
			Committee	The property supports the former historic character of the area and is visua and historically linked to its surroundings. Further research has the potentia to yield information that contributes to an understanding of the community of Ancaster.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 31	Institutional	92 Glancaster Road, Ancaster; Hamilton District Christian High School	Inventoried (Places of Education)	The Hamilton District Christian High School is a private secondary school located on a large, landscaped lot that includes a soccer field, baseball diamond, volleyball courts, and parking lots. The school building appears to be a contemporary structure that dates to the early twenty-first century. It is two-storeys and built to an L-shaped plan. The structure is clad in red brick with yellow brick bands and features a flat roof.
BHR 32	Residence	549 Glancaster Road, Glanbrook	Previously Identified; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	The property contains a one-and-a-half storey brick residence built c.1950. Determination of an estimated construction date was aided by aerial image from 1959 that indicates the presence of the structure. The residence is screened by mature vegetation but it appears to be built to a rectangular pl and features a side gable roof with a centre gable peak.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 33	Farmscape	1640 Glancaster Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1900, the residence on the property is representative of a vernacular Gothic Revival Farm House. The one-and-a-half storey red brick residence displays a high degree of craftsmanship exhibited through its cross-gable roof, projecting front gable with ground-floor bay window, covered porch and steeply-pitched projecting gabled dormer with decorativ bargeboard. A large barn and concrete silo are located to the south of the residence. The structures on the property are located on raised topography.
				The residence on the property is believed to have been constructed betwee 1820 and 1840 by builder Gideon Smith.
BHR 34	Residence	9630 White Church Road West, Glanbrook	Inventoried; LACAC Report (1985); City of Hamilton AEGD Windshield Survey (2021)	The residence is an early and rare example of Loyalist architecture in Glanbrook. The one-and-a-half storey red brick structure features a stone and concrete foundation, a side gable roof with matching brick chimneys, and a three-bay façade with a central entrance with a transom and sideligh beneath a covered wraparound porch with a pedimented gable peak. A two bay garage is located to the rear of the residence and both structures are located on raised topography. A wood fence has recently been constructed in front of the residence.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 35	Residence	9890 Airport Road West, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	The residence on the property was constructed in 1919 for original owner Cornelius Smith by a builder named Rispin. The two-storey red brick residence is a vernacular Edwardian structure that features a stone foundation, hip roof with overhanging eaves, and a projecting bay on the façade with a gable roof. The original covered wraparound porch has been enclosed with red brick. The residence is built on a rise in topography and low dry stone wall is located in front of the house along Airport Road. A number of mature trees line the south property boundary.
BHR 36	Residence	9174 Airport Road, Glanbrook	Inventoried; LACAC Report (1984)	The residence on the former nineteenth farmscape property was construct in 1872 while under the ownership of William Sanders Sr. The residence w demolished in 2000, however a barn appears to remain at the rear of the lo among dense tree cover. The presence of the barn could not be confirmed when viewed from Airport Road. The property appears to have been subdivided from a large agricultural lot.
BHR 37	Residence	9142 Airport Road, Glanbrook	Inventoried; LACAC (1985)	The City's inventory notes that the residence was constructed in 1878, however the structure appears to be a post-war bungalow, likely constructe in the 1950s. The property contributes to evolved-dynamic streetscape of post-war bungalows along Airport Road West (CHL 50).





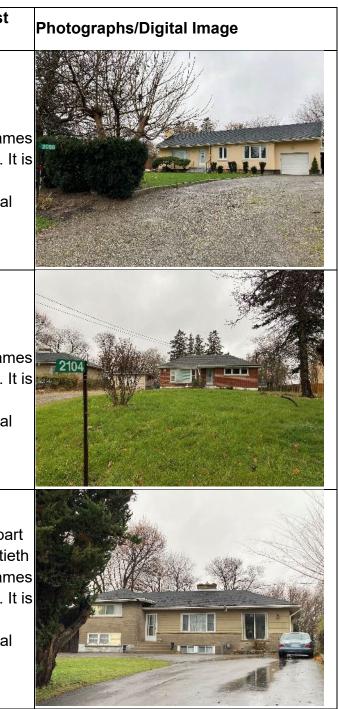
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 38	Residence	9132 Airport Road, Glanbrook	Inventoried	The residence on the property appears to be a late twentieth century vernacular structure built to a roughly rectangular plan with a varied roof lin The structure is clad in red brick and vinyl siding and features a covered porch. A number of additions have been constructed at the rear of the residence and a large workshop, built to a rectangular plan with a side gab roof, is located at the rear of the property, accessed by a concrete driveway The property appears to have been subdivided from a large agricultural lot.
BHR 39	Residence	9046 Airport Road, Glanbrook	Identified in the field	The vernacular residence on the property reflects elements of the Ontario Cottage style and likely dates to the turn of the twentieth century. A review topographic mapping from 1907 illustrates a frame residence in approximately the location of this property. The one-and-a-half storey building features a side gable roof with a symmetrical façade and a centre gable peak above the entrance. The building is clad in vinyl siding. A two-c garage with a gable roof is located at the rear of the residence.
BHR 40	Residence	9032 Airport Road, Glanbrook	Identified in the field	The property contains a vernacular residence that likely dates to the turn of the twentieth century. A review of topographic mapping from 1907 illustrate a frame residence in approximately the location of this property. The one- and-a-half storey building features a rusticated concrete block foundation, a side gable roof, a red brick chimney, a symmetrical façade and upper store gable windows that cut through the roofline. The building is clad in vinyl siding and appears to be modified.



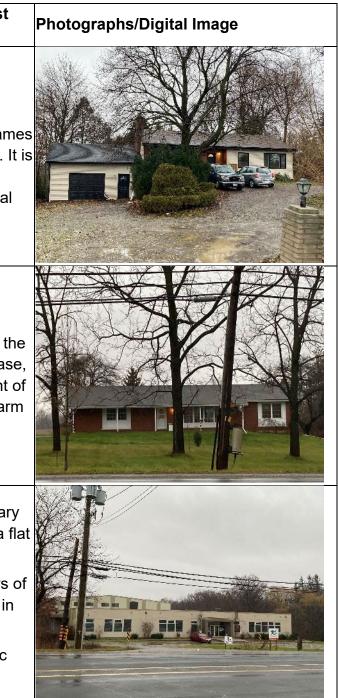
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 41	Residence	9188 Dickenson Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	The property contains a vernacular house built in 1871. A review of topographic mapping from 1907 illustrates a frame residence in approximately the location of this property. The structure is two-storeys with a gable roof and covered porch. The façade of the house appears to be an addition. Located on the north side of Dickenson Road West, an early transportation route, the residence is set close to the road and reflects the early twentieth-century settlement along Dickenson Road.
BHR 42	Residence	9070 Dickenson Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	MPAC data suggests that the residence on the property was constructed in 1873, however LACAC research dates the structure to 1850. It was built by the Dickenson Brothers and is believed to be associated with the Dickenson family, a prominent family in Glanford's history and namesake for the road. The residence is representative of the Gothic Revival Ontario Cottage style and features a projecting front gable with a lancet window, and a three-bay facade with a central covered entrance. The property helps define the historic character of the area as it was built with a narrow setback from Dickenson Road.
BHR 43	Residence	9036 Dickenson Road West, Glanbrook	Previously Identified; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	The residence on the property is a one-storey, wood frame, vernacular structure constructed between 1929 and 1938. It features a side gable root a cut stone foundation, a stone chimney, and an entrance offset from centr flanked by contemporary windows with false shutters. The residence has a narrow setback from Dickenson Road West.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 44	Residence	2098 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1959, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 45	Residence	2104 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1956, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 46	Residence	2112 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1956, the one-storey wood frame split-level residence is par of a streetscape of small-to-medium-scale residences from the mid-twentie century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. It a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.



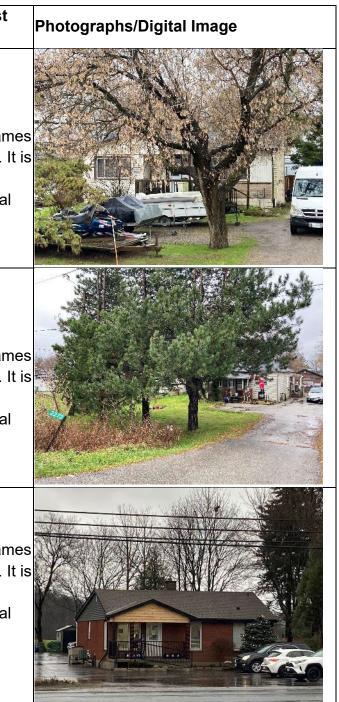
BHR (CHL #	I vno ot Pronortv	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 4	7 Residence	2118 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1956, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. It a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 4	8 Residence	2129 Upper James Street, Glanbrook	Identified in the field	Constructed c.1950, the one-storey wood frame ranch style residence contributes to the streetscape of small-to-medium-scale residences from th mid-twentieth century along this section of Upper James Street. In this case the residence was likely constructed in the post-war boom. It is a remnant of the development of the area as it transitioned from rural properties and farr complexes to a peri-urban landscape with commercial enterprises.
BHR 4	9 Place of Worship	2149 Upper James Street, Glanbrook; Church on the Rock	Inventoried	Constructed in 1959, the Church on the Rock is a one-storey contemporary vernacular brick structure clad in stucco with a concrete foundation and a f and gable roof. The property defines, maintains and supports the City of Hamilton's views protecting and recognizing the important role that places of worship play in
				community life. The subject property is the location of the former Pioneer Memorial public school, which has been demolished.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
		2235 Upper James Street, Glanbrook	Registered Non-Designated; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Although the residence on the property is recorded in the City's Inventory to have been constructed in 1842, MPAC data lists the date as 1873. Glanbrook Historical Society research indicates that Asa Choate purchased the property in the 1860s and thereafter built the residence (2007b).
BHR 50	Residence			The one-storey brick Regency style building features a hipped roof and belvedere with a single storey summer kitchen addition at the rear with a gable roof. An external brick chimney is located on the north elevation and the main entrance on the façade includes sidelights, a transom, the origina door and porch pediment. A line of new brick at the top of the wall plates suggests that the roof of the summer kitchen has been raised, however the original structure appears little altered. The house sits on fieldstone footing on a slight rise of land amid cultivated fields. The property retains some mature deciduous trees to the north, south and east of the building.
BHR 51	Residence	2254 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1955, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises. The residence is boarded and vacant.
BHR 52	Residence	2264 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1953, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 53	Residence	2270 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1956, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. It a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 54	Residence	2272 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1951, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 55	Commercial	2273 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1953, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises. The building on the property now serves a commercial land use and is surrounded by a paved parking lot.



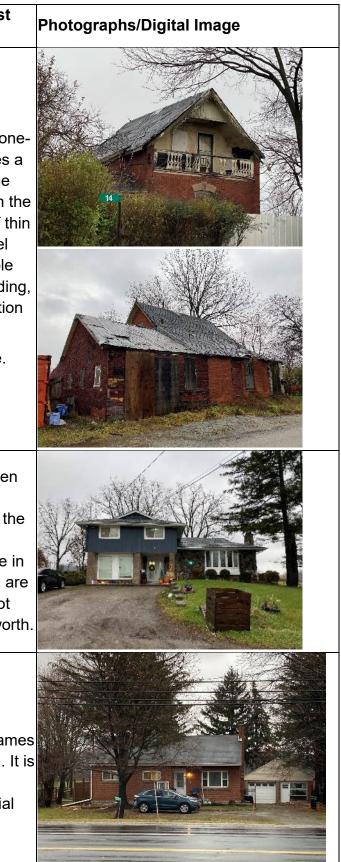
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 56	Residence	2274 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1958, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper James Street. In this case, the residence was constructed in the post-war boom. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 57	Residence	19 Talbot Lane, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1954 in the North Glanford Historic Settlement Area, the small one-storey wood frame vernacular cottage features a concrete block foundation, a low hip roof, a tripartite window with three-over-one hung wood windows, and a red brick chimney. Its construction date is consistent with the other post-war residences built in the surrounding area in the mid-twentieth century.



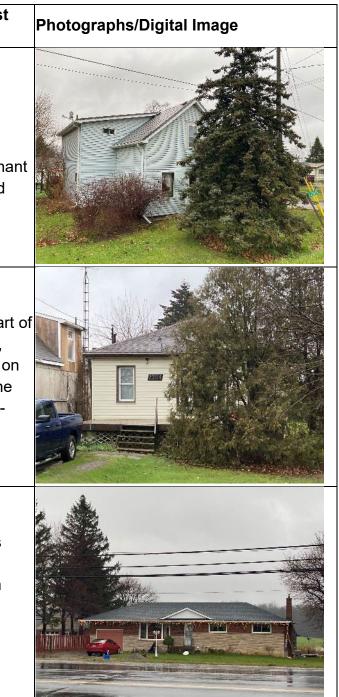


BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 58	Residence	24 Talbot Lane, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1896 in the North Glanford Historic Settlement Area, the on and-a-half storey red brick and wood frame vernacular residence features stone foundation and a gable roof with bargeboard in the gable peak. The upper storey features a recessed covered porch accessed from a door in the gable peak. The decorative balusters of the porch railing appear to be of the cut wood pieces. The lower storey window features an arched stone lintel with a keystone. A significant one-storey addition (woodshed) with a gable roof clad in insulbrick has been constructed on the south side of the buildin obscuring much of the structure. Both the original structure and the addition appear to be in a state of disrepair. The Lowden family are noted to have been early occupants of the house.
BHR 59	Residence	38 Talbot Lane, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	The residence on the property is noted in the City's Inventory to have been constructed in 1871, however it is unclear if the extant residence is the original structure, heavily modified, or a new building. The gable peak of th façade could be evidence of an older one-and-a-half storey structure beneath the slope roofed projecting addition and recladding of the façade i a stone veneer. Two stone or brick buildings in this approximate location an illustrated on the 1907 topographic map of the area, however they are not illustrated on the 1875 Illustrated Historical Atlas of the County of Wentwor
BHR 60	Residence	2277 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1954, the one-storey wood frame bungalow is part of a streetscape of small-to-medium-scale residences from the mid-twentieth century all built on similar sized lots with uniform setbacks from Upper Jam Street. In this case, the residence was constructed in the post-war boom. If a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.





BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 61	Residence	2280 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1931, the one-and-a-half storey wood frame vernacular residence is part of an early-to-mid-twentieth century streetscape along Upper James Street, south of Talbot Road, characterized by modest residential structures set on narrow lots with similar setbacks. It is a remna of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 62	Residence	2284 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1954, the one-storey wood frame vernacular cottage is part an early-to-mid-twentieth century streetscape along Upper James Street, south of Talbot Road, characterized by modest residential structures set or narrow lots with similar setbacks. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri- urban landscape with commercial enterprises.
BHR 63	Residence	2285 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1965, the one-storey wood frame ranch style residence is part of a streetscape of small-to-medium-scale residences from the mid- twentieth century all built on similar sized lots with uniform setbacks from Upper James Street. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.



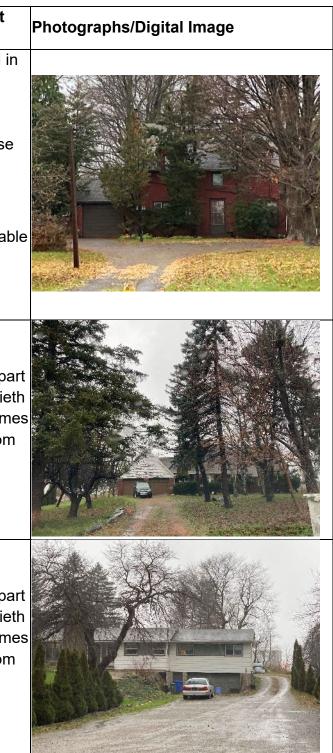
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 64	Residence	2288 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1926, the one-and-a-half storey wood frame vernacular residence is part of an early-to-mid-twentieth century streetscape along Upper James Street, south of Talbot Road, characterized by modest residential structures set on narrow lots with similar setbacks. It is a remna of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
		2292 Upper James Street, Glanbrook; Former North Glanford Post Office		The City's Inventory records the construction date of the building on the property as 1862, and LACAC lists a construction date of 1850. It is unclear if the one-storey extant brick residence is the original structure, heavily modified, or a new building. The elongated ends of the side gable roof do indicate the possible expansion of an existing structure. The foundation is currently concrete, however that may be a later modification.
BHR 65	Residence			The City's Inventory and the Glanbrook Heritage Society's history of North Glanford (2007b) record that the property is the former location of the Nort Glanford Post Office, which was located in the small grocery shop owned b John Dickenson at 2292 Highway 6 (present day Upper James Street) from roughly the mid-1860s to 1915.
				The residence is part of an early-to-mid-twentieth century streetscape alon Upper James Street, south of Talbot Road, characterized by modest residential structures set on narrow lots with similar setbacks.
BHR 66	Residence	2294 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019)	The one-storey wood frame vernacular residence on the property was constructed in 1907 and features a symmetrical façade and a notable top- storey addition that has enlarged the structure from one to two-storeys. Th side gable roof of both the addition and original portion of the building remain.
				The residence is part of an early-to-mid-twentieth century streetscape alon Upper James Street, south of Talbot Road, characterized by modest residential structures set on narrow lots with similar setbacks.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 67	Residence	2300 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); LACAC Report (1984); City of Hamilton AEGD Windshield Survey (2021)	The City's Inventory indicates the construction date of the single storey residence on the property as 1860. A review of the 1984 LACAC report indicates a different residence on the property, which is assumed to have been demolished. The extant bungalow was likely constructed in the 1950s consistent with the other similar residences that comprise the streetscape, characterized by mid-nineteenth century modest residential structures set of narrow lots with similar setbacks. The property now serves a commercial land use.
BHR 68	Residence	2310 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1940, the one-and-a-half storey wood frame vernacular cottage is part of an early-to-mid-twentieth century streetscape along Uppe James Street, south of Talbot Road, characterized by modest residential structures set on narrow lots with similar setbacks. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
				The City's Inventory notes the residence on the property was constructed in 1860, however MPAC data lists it as 1871. The property was owned by Joh Dickenson, a local member of parliament.
BHR 69	Residence	e 2332 Upper James Street, Glanbrook	West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012); City of Hamilton AEGD Windshield Survey (2021)	The one-and-a-half storey double brick Ontario Cottage was built in three stages and all of the bricks came from the local Dickenson brick yard, located at the southeast corner of Highway 6 (present day Upper James Street) and Twenty Road. The residence features a three-bay symmetrical façade with a side gable roof and a central gable peak above the entrance with sidelights and a transom.
				The property helps define the mid-nineteenth century rural / village streetscape character of the North Glanford Historic Settlement Area. The property is built close to Upper James Street and is visually and historically linked to its surroundings.



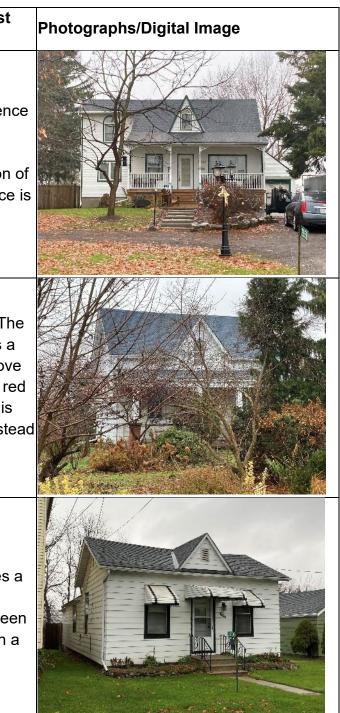
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 70	Residence	2340 Upper James Street, Glanbrook	Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The City's Inventory notes the residence on the property was constructed in 1950, however MPAC data lists it as 1871. Research indicates that the house was originally constructed as a barn to keep John Dickenson's horses. Dickenson was the owner of the neighbouring property at 2332 Upper James Street (BHR 69). In 1950, the barn was converted to a house by the first occupants, the Logan family. This likely explains the recorded construction date of 1950.
				The one-and-a-half storey brick vernacular structure features a stone foundation, side gable roof, and symmetrical three-bay façade with two gab windows.
				The property helps define the mid-nineteenth century rural / village streetscape character of the North Glanford Historic Settlement Area.
BHR 71	Residence	2492 Upper James Street, Glanbrook	Identified in the field	Constructed c.1960, the one-storey wood frame ranch style residence is pa of a streetscape of small-to-medium-scale residences from the mid-twentied century all built on similar sized lots with uniform setbacks from Upper Jame Street. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.
BHR 72	Residence	2502 Upper James Street, Glanbrook	Identified in the field	Constructed c.1960, the one-storey wood frame ranch style residence is pa of a streetscape of small-to-medium-scale residences from the mid-twentie century all built on similar sized lots with uniform setbacks from Upper Jame Street. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 73	Residence	2624 Upper James Street, Glanbrook	Identified in the field	The property contains a one-and-a-half storey red brick mid-nineteenth century vernacular residence. It features a unique façade with an eyebrow window in the centre of the hip roof, large bay windows flanking both sides the façade, stone quoins, and rusticated stone window sills. The residence is set upon a large, heavily treed lot lined with mature vegetation to the north, east and south. The residence maintains a setback similar to other twentieth century residences lining Upper James Street.
BHR 74	Residential	2724 Upper James Street, Glanbrook	Identified in the field	The property contains a twentieth century, two-storey, wood frame, multi- residential building with a concrete foundation, side gable roof and a gable peak offset from centre on the façade above the entrance. The structure is clad in vinyl siding and includes a recessed entrance covered with a pedimented gable. A one-storey addition projects from the south elevation and a large parking lot is located in front of the building.
BHR 75	Residence	2910 Upper James Street, Glanbrook	Identified in the field	The property contains a one-and-a-half storey early twentieth century vernacular cottage clad in vinyl siding. It features a side gable roof with a re- brick chimney and a symmetrical façade with two gabled dormer windows and a central entrance flanked by square window openings. The residence set on a rise in topography with a significant setback from Upper James Street. The lot is heavily treed.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 76	Residence	2958 Homestead Drive, Glanbrook	Identified in the field	The property contains a one-and-a-half and two-storey vernacular residence that appears to be a recreation of an Ontario Cottage, however the construction date and method of construction could not be confirmed. It features a centre gable peak above a covered porch. A two-storey portion the building built to a rectangular plan is located to the rear. The residence set on a treed lot setback from Homestead Drive.
BHR 77	Residence	2990 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	Constructed in 1877, the property was originally owned by P. Richards. Th one-and-a-half storey Ontario Cottage is clad in vinyl siding and features a side gable roof and a centre gable peak with a lancet window placed above the entrance. The front porch is covered and supported by columns with re brick bases. An addition is located at the rear of building. The residence is set on a heavily treed lot, obscuring the view to the property from Homeste Drive.
BHR 78	Residence	3000 Homestead Drive, Glanbrook	Inventoried	The residence on the property was constructed in 1867 and is an early example of an Ontario Cottage. The modest one-storey structure features side gable roof, and a symmetrical three-bay facade with a centre gable peak above the entrance. The residence is clad in vinyl siding and has bee enlarged through a rear, one-storey addition. The residence is located on a small, grassed lot with a narrow setback from Homestead Drive.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 79	Residence	3002 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	The property contains a wood frame two-and-a-half storey vernacular residence that reflects elements of the Queen Anne Revival style. It is recorded in the City's Inventory as being constructed in either the 1850s of 1920. The residence is constructed to an L-shaped plan with a cross gable roof and two projecting bays featuring pedimented gable peaks each with a rectangular window opening. The building is clad in vinyl siding and include a varied fenestration. A covered front porch addition has been added, as h a rear addition. The residence extends almost the full width of the lot and h a narrow setback from Homestead Drive.
BHR 80	Residence	3034 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	The property contains one-and-a-half storey brick Gothic Revival-inspired Ontario Farm House constructed in 1867. The residence features a stone foundation and a gable roof with projecting bays with decorative bargeboar in the peaks. The upper storey windows include decorative arched brick lintels and a bay window is located on the main floor. A covered porch has been added and features decorative cresting along the cornice line. The residence is set on one of the larger lots on Homestead Drive.
BHR 81	Place of Worship	3076 Homestead Drive, Glanbrook; Mount Hope United Church	Inventoried, Volume 7A; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Mount Hope United Church was built in 1876. The congregation, at that tim Methodist Episcopal, had been meeting in a wooden building in Mount Hop When the congregation built their own church, the contractors were Edwar Dickenson and Sons, John and Edward Jr., and the total cost was \$3,035. The bricks were made in the local Dickenson brickyard on the southeast corner of Highway 6 (present day Upper James Street) and Twenty Road. This church is the only one in the surrounding district without an attached cemetery. Gradually, the church became too small to accommodate those attending f Sunday evening services. In 1895, a Sunday School building was erected, also built by Dickenson and Sons. In June, 1959, construction started on the Christian Education Building. During construction, some 250 children were meeting each Sunday morning
				in the church sanctuary and the Mount Hope Lions Youth Centre. The offic opening of the new Christian Education building took place on Sunday, Ju 5, 1960 and the dedication ceremony was performed by the late Reverend Aubrey Jones, chairman of the Hamilton Presbytery.





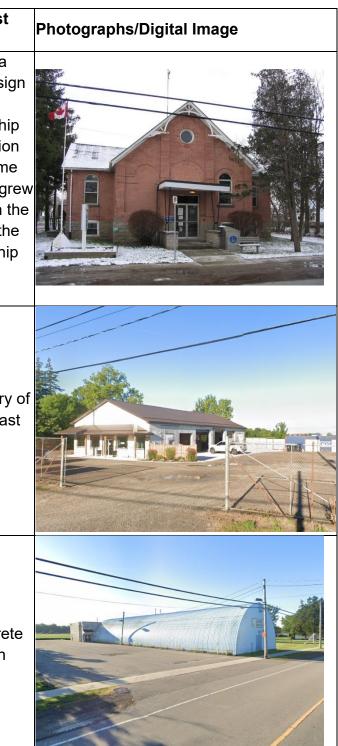
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 82	Commercial	3114 Homestead Drive, Glanbrook	Inventoried	The property appears to contain a two-bay workshop with a gambrel roof a a one-storey workshop with a gable roof, both of utilitarian construction potentially dating to the mid-twentieth century. There is no residence on thi property, however the circulation route visible in aerial photography indicate that it may be associated with the neighbouring property at 3116 Homestea Drive (BHR 83). A U-shaped driveway spans both properties.
				Both structures are set back significantly from the road on a lot with a tree- lined gravel driveway.
BHR 83	Residence	3116 Homestead Drive, Glanbrook	Inventoried	This property contains a one-storey wood frame vernacular cottage that likely dates to the mid-twentieth century. It features a hip roof and a concre foundation and the façade is symmetrical and composed of a central entrance flanked by rows of two-over-one windows. The dense tree cover and significant setback screens the residence when viewed from Homester Drive.
				The circulation route visible in aerial photography indicates that this proper may be associated with the neighbouring property at 3114 Homestead Driv (BHR 82). A U-shaped gravel driveway spans both properties.
BHR 84	Residence	3136 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	The two-storey frame vernacular residence on the property was constructe in 1899 and is reflective of the Georgian architectural style. It is clad in viny siding and features a stone foundation, a side gable roof and varied fenestration on the façade, which appears to be modified. Two red brick corbelled chimneys project from both ends of the gable roof, as well as the gable roof of the one-storey addition on the north elevation. The property is set on a treed lot with a narrow setback from Homestead Drive.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 85	Commercial	3154 Homestead Drive, Glanbrook	Identified in the field	The property contains a one-and-a-half storey vernacular commercial building located one lot north of the crossroads of Mount Hope. The structu likely dates to the turn of the twentieth century as a frame building is illustrated at this location on the 1907 topographic map of the area. The structure appears to have evolved over time with the façade composed of a round topped boomtown front clad in pebble dash on the upper portion and stucco on the lower portion. Behind the façade, the structure features a cro gable roof and the building extends through various additions to the rear of the lot. Most of the building is clad in sheet metal and the rear is clad in wo board and batten. Commercial store fronts are located at both the front and back of the building.
BHR 86	Commercial	3156 Homestead Drive, Glanbrook	Identified in the field	The property, located at the northwest corner of the crossroads of the settlement of Mount Hope, contains a one-and-a-half storey frame Ontario Cottage. The structure likely dates to the turn of the twentieth century as a frame building is illustrated at this location on the 1907 topographic map of the area. The residence features a concrete (or concrete clad) foundation, side gable roof and a roughly symmetrical façade with a central gable peak and an entrance flanked by two window openings. The residence is built to the lot line fronting Homestead Drive and with a narrow setback from Airpo Road.
BHR 87	Residence	2983 Homestead Drive	Inventoried	The property contains a one-and-a-half storey brick Ontario Cottage style residence built in 1867. The residence features a side gable roof and a symmetrical façade with a central gable peak and an entrance flanked by two window openings. Two vinyl clad extensions have been added to the rear of the structure, the first one-and-a-half storey and the second a single storey. The residence is built close to the lot line fronting Homestead Drive with four spaced out mature trees along Homestead Drive.

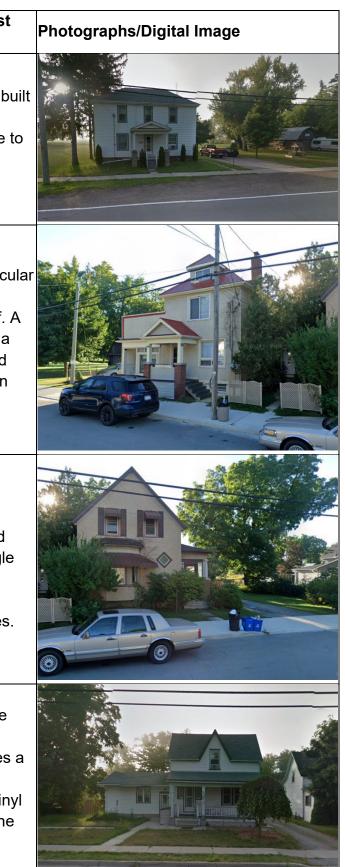


BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 88	Institutional	3027 Homestead Drive	Designated under Part IV of the OHA (By-Law No. 15- 169);	Glanford Hall, located at 3027 Homestead Drive in Hamilton, Ontario, is a representative example of the Edwardian Classicism style of building desig that was commonly used in early-20th century Ontario. The Hall was constructed in 1904 as the first permanent home for the Glanford Township Council and served that function into the 1990s following the amalgamation of the Glanford and Binbrook Townships. The building has also been home to Mount Hope's library on several occasions since 1966. The library outgratis small facilities by the 1970s, but has since returned to Glanford Hall in the 1990s, where it currently resides. Glanford Hall is a physical remnant of the early development of the community of Mount Hope and the early township structure of Wentworth County. Since 1904, the property has served the community in a public manner.
BHR 89	Residence	3102/3110 Homestead Drive	Inventoried	The extant structure at 3102 Homestead Drive is a large single storey commercial building operated by Mount Hope Machine Ltd. Aerial imagery the property from 1995 shows a ghost print of a foundation at the southeas corner of the property. It is likely that this structure was an inventoried residence and was removed prior to 1995 but after being inventoried.
BHR 90	Institutional	3091 Homestead Drive	Inventoried	The structure at 3091 Homestead Drive is a three-sheet curling facility constructed of a long Quonset hut steel structure with a two-storey concret addition to the rear. The Glanford Curling Club constructed the building in 1959.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 91	Residence	3117 Homestead Drive	Inventoried	The property contains a two-storey vernacular Georgian style residence buin in 1851. The residence is clad in vinyl siding with a hop roof, stone foundation and central pedimented porch. A one-storey addition is visible to the rear of the structure. A line of mature trees is present bisecting the property immediately north of the residence.
BHR 92	Residence	3127 Homestead Drive	Inventoried	The two-storey vinyl clad residence at 3127 Homestead Drive is a vernacu Edwardian structure built in 1879. The residence features a hip roof with overhanging eaves, and a projecting bay on the façade with a gable roof. A one-storey addition has been added to the north side of the structure as a store front. The residence is built up against the lot line along Homestead Drive. The building is connected to 3129 Homestead drive via an addition between the two original structures.
BHR 93	Residence	3129 Homestead Drive	Inventoried	The two-storey stucco clad residence at 3129 Homestead Drive is a vernacular structure built in 1872. The façade features a bay window and diamond port hole window. The residence features a gable roof and single storey addition to the southside of the building. The residence is built up against the lot line along Homestead Drive. The building is connected to 3127 Homestead drive via an addition between the two original structures.
BHR 94	Residence	3141 Homestead Drive	Inventoried	The property contains a one-and-a-half storey brick Ontario Cottage style residence. The structure pre-dates 1907 as building is illustrated at this location on the 1907 topographic map of the area. The residence features side gable roof, front porch, central gable peak, stone foundation and an entrance offset with symmetrical portions to the opposite window. Two viny clad extensions have been added; one to the rear of the structure, and the second on the north side.



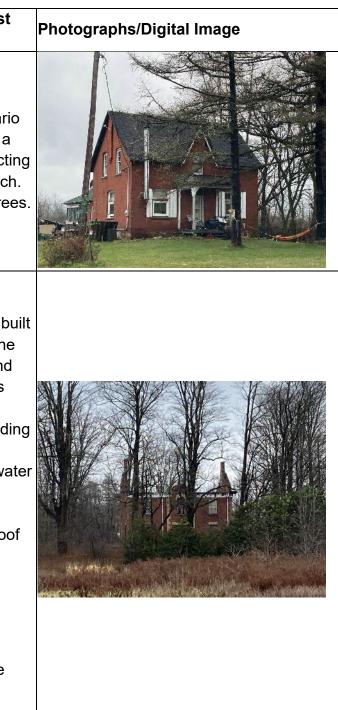


BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
BHR 95	Residence	3145 Homestead Drive	Inventoried	The property contains a one-and-a-half storey brick Ontario Cottage style residence, smaller in mass than many others in the area. The structure pre- dates 1907 as building is illustrated at this location on the 1907 topographi map of the area. The residence features a side gable roof and a symmetric façade with a enclosed porch and an entrance flanked by two window openings. A single one-storey vinyl clad extension has been added to the rear of the structure
BHR 96	Residence/Comm ercial	3155 Homestead Drive	Inventoried	This two-storey vernacular brick structure has a gambrel roof and one-stor additions on all sides. Most notably, an automotive service garage is locate along the north façade of the building with its entrance facing Homestead Drive. The additions along the east, west and south facades are considerable smaller but hide the original features of the residence.
CHL 1	Farmscape	919 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1875, the one-and-a-half storey wood frame residence is representative of the Ontario Cottage style. Clad in wood siding with a side gable roof, the three-bay front facade with central entrance features a steeply pitched gable above with flanking windows, transom, and a round- headed window. A barn is located at the rear of the lot and a split rail fence and mature trees line the front of the property.
CHL 2	Farmscape	956 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1870, the one-and-a-half storey wood frame residence is representative of a vernacular Farm House. It is built to a T-shaped plan, with a cross gable roof and enclosed covered porch. The residence is set o topography raised above Fiddlers Green Road.





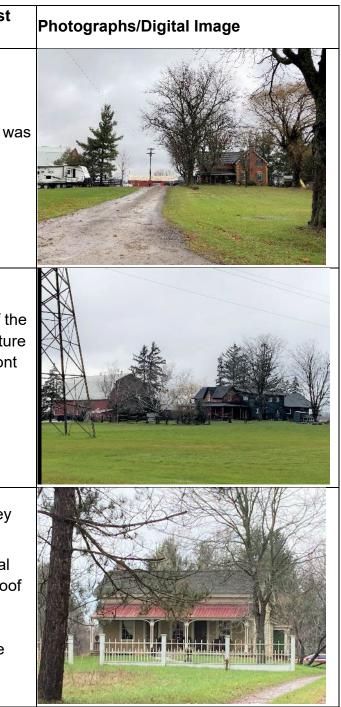
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 3	Farmstead	1394 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1904, the farm house is a representative vernacular Ontario Cottage. The one-and-a-half storey building is clad in red brick, features a side gable roof, three-bay front facade with a central entrance and projectir centre gable above and flanking windows, as well as a covered front porch The property contains outbuildings to the rear and a number of mature tree
CHL 4	Farmscape	1541 Fiddlers Green Road, Ancaster; Forest Lane Farms House; Russel Christie House	Designated under Part IV of the OHA (By-Law No. 84-55); LACAC Report Volume 3 (n.d.)	Reasons for Designation (1984): - This building is one of the best remaining examples of a house [Neo- Classical and Georgian inspired] in the Scotch Block of Ancaster. It was bu by John Squire Russel, one of the original Scottish Families to settle in the Scotch Block. The house was built in 1860 and only the Russel family and Mr. Christie, today's owner, have lived in this house and generally it is as originally built; - All exposed brick and stone facades of the walls and foundations, includir the garage area; - The cornices, soffits, fascia, brackets, built in eaves troughs, and rain wat leaders; - All exterior windows including sash, frames, sills, and shutters; - All exterior wooden architectural fenestrations such as the side porch roo structure and the side doors with jambs and sills; - The front door including the side lights and fan light and all the related woodwork and sill; - The three exterior chimneys; - Interior attributes. The residence, along with the barn, appear to have recently burned quite severely, as noted in the photograph.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
				Constructed in 1865, this is a pre-Confederation era structure. The propert style, type or expression is early and representative of the Gothic Revival Cottage style of architecture.
CHL 5		127 Carluka Daad		The wood frame residence is one-and-a-half storeys with a side gable roof with a projecting front gable, and a three-bay façade with a central entrance and flanking windows.
	Farmscape	137 Carluke Road West, Ancaster	Registered Non-Designated	A long driveway leads to the house, setback deep in the lot. The property contains detached outbuildings, mature trees, and water courses running through the lot.
				The property is associated with a potentially significant theme (pre- Confederation development). Further research has the potential to yield information that contributes to an understanding of the community of Ancaster.
CHL 6	Farmscape	37 Carluke Road East, Ancaster; Cranston Farms	Inventoried; LACAC Report Volume 6 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1850, the one-and-a-half storey vernacular farm house features a side gable roof and a symmetrical façade with two gable peaks. Windows are located beneath both gable peaks. A covered porch spans th façade and wraps around to the side elevations. The property is an active farmstead with a barn, concrete silo, outbuildings and agricultural fields. Mature trees surround the farm house.
CHL 7	Farmscape	54 Carluke Road East, Ancaster; Hays House	Inventoried; LACAC Report Volume 6 (1985); City of Hamilton AEGD Windshield Survey (2021)	Constructed between 1842 and 1870, the original owner of the property wa James Forsythe. The vernacular Neo-Classical and Gothic Revival inspired residence displays a high degree of craftsmanship. The one-and-a-half storey brick structure features a stone foundation, dichromatic brickwork including quoins, a side gable roof with return eaves, flat-headed windows with brick voussoirs and stone lug sills, a projecting central frontispiece with a gable roof and ground-floor entrance with a transom and sidelights. The residence, barn and outbuildings are setback from the road in a stand of mature trees.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 8	Farmscape	153 Carluke Road East, Ancaster; Cranston Farms	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1880, the property contains a two-storey vernacular farm house influenced by the Italianate architectural style. The brick structure we built to an L-shaped plan and features a cross gable roof clad in steel, dichromatic brickwork, and projecting brick chimneys. The property is an active farmstead and includes a barn and outbuildings.
CHL 9	Farmscape	189 Carluke Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1865, the farm house on the property is representative of th Ontario Cottage architecture style. The one-and-a-half storey brick structur features a side gable roof with a projecting front gable, and a covered front porch. The property contains detached historic outbuildings, including a Central Ontario barn, as well as a wood split rail fence. The collection of buildings are setback from the road.
CHL 10	Farmscape	67 Butter Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1870, the property is historically associated with the Fahey family (1875 Illustrated Historical Atlas of the County of Wentworth). The vernacular residence is reflective of the Loyalist Cottage architectural style. The wood frame structure is a single storey with a low side gable roc clad in cedar shingles with flanking brick chimneys, and a covered front porch. The structure is clad in wood siding. A decorative metal fence is located in front of the residence. The property is characterized by mature trees and detached agricultural accessory structures.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 11	Farmscape	157 Butter Road East, Ancaster	Inventoried	Constructed in 1850, the property's style, type or expression is early and representative of the Ontario Farm House style of architecture. The one-an a-half storey wood frame residence features a cross-gable roof, red brick exterior, and an enclosed front porch. The property includes two detached accessory structures, an orchard, and a water course running through the northern end of the lot. The property is associated with a potentially significant theme (pre- Confederation development). Further research has the potential to yield, information that contributes to an understanding of the community of Ancaster.
CHL 12	Farmscape	363 Butter Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1908, the one-and-a-half storey red brick residence is reflective of the Ontario Cottage architecture style. It features a side gable roof with a central front gable, and a covered porch. The property contains an active farmstead composed of a barn and detached accessory structure as well as a pond, metal perimeter fence, and mature trees lining Butter Road East and surrounding the collection of buildings on the property. The residence has a narrow setback.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 13	Streetscape	Butter Road East between Highway 6 and Glancaster Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	This portion of Butter Road East is characterized as an evolved-dynamic streetscape CHL. It is a scenic rural transportation corridor with a narrow right-of-way framed by dense vegetation and varying topography.
CHL 14	Streetscape	Southcote Road from Butter Road East to Airport Lands, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	This portion of Southcote Road is characterized as an evolved-dynamic streetscape CHL. It is a rural scenic streetscape composed of a gravel roa with a narrow right-of-way lined with vegetation, and varying topography.
CHL 15	Farmscape	167 Book Road East, Ancaster; Book House	Inventoried; Documentation & Salvage Report (Megan Hobson, 2018)	The former Book House, a two-storey Georgian style residence on the property constructed in 1831, was removed in 2018. Ruins of the structure reported to have once been the oldest two-storey house between Niagara and Windsor, appear to remain on the property (Green, 2004).



		-		
CHL 16	Cemetery	281 Book Road East, Ancaster; Book Cemetery	Designated under Part IV of the OHA (By-Law No. 18-079)	The Book Cemetery's heritage attributes derive from its archaeological, buil heritage and cultural heritage landscape values associated with a nineteenth-century Euro-Canadian pioneer cemetery, as detailed in the
				following: Archaeological Resources and Areas of High Archaeological Potential: - Archaeological value of features and artifacts related to the original definition and organization of the 19th century Euro-Canadian cemetery; - Archaeological value of human remains contained within the cemetery; - Archaeological value in remaining sub-surface funerary artifacts; and, - Archaeological value of potential for Native archaeological sites. Built Heritage Features: - All existing 19th and 20th century stone markers and monuments, and their surviving icons and inscriptions; and, - The variety of styles and materials represented in the markers and
				monuments, together with their individual material composition. Cultural Heritage Landscape Features - The grid design layout and placement of all graves, markers, and monuments; - The well-defined boundary markers delineating the cemetery; and,





BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
				- The grassed areas, ground vegetation, and trees located within the cemetery, exclusive of the graves, markers, and monuments.
CHL 17	Cemetery	330 Book Road East; Ancaster Pet Cemetery	Identified in the field	The Ancaster Pet Cemetery, founded c.2000 and run by Gateway, is a rura approximately 3.5 acre, pet cemetery located on a narrow rectangular lot th is landscaped and heavily treed. The cemetery is somewhat setback from the road, with a parking lot located on the north side of the lot, adjacent to Book Road East.
CHL 18	Farmscape	394 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1882, the residence on the property is representative of the Italianate architectural style. The two-storey red brick building features a stone foundation, a low hipped roof, projecting front bay, and dichromatic brickwork with quoining and brick voussoirs. The residence is set on the top of a rise in elevation on a large, open, grassed lot with a significant setback from Book Road East, providing picturesque views to the residence. Mature trees are growing beside and behind the residence and a pond is located adjacent to the north property boundary.
				Constructed in 1840, the property's style, type or expression is early and representative of the Gothic Revival Cottage style of architecture. The property is important in supporting the character of the area and is functionally, visually and historically linked to its surroundings.
CHL 19	Farmscape	451 Book Road East, Ancaster	Registered Non-Designated	The wood frame farm house is one-and-a-half storeys with a three-bay façade, a side gable roof with a projecting centre front gable and window below, a central entrance and flanking windows, a covered front porch, and two side additions. The residence is part of an agricultural complex on the property composed of multiple detached accessory buildings, mature trees and a water body and stream running east of the house and outbuildings.
				The property is associated with a potentially significant theme (pre- Confederation development) and activity (produce farming). Further researce has the potential to yield, information that contributes to an understanding of the community of Ancaster.

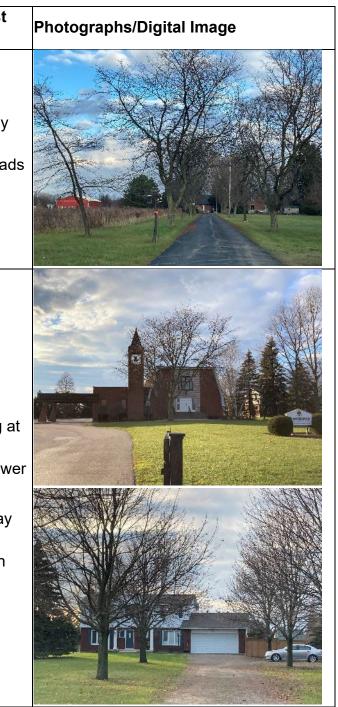




BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 20	Settlement	Southcote, Ancaster (approximate boundaries based on City of Hamilton Windshield Survey of concentration of pre- 1950 heritage buildings)	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	The historic settlement area of Southcote is an evolved-dynamic settlement CHL The early development of the settlement at the southeast and southwest corners of the intersection of Book Road East and Southcote Road are recorded on the 1875 Illustrated Historical Atlas of the County of Wentworth, although the area was not named. Notably, a school house wa located at the intersection at this time. By 1907, the settlement was named Southcote on topographic mapping of the area, with development extendin down Southcote Road, south of Book Road East. Today, the landscape is characterized by pre-1950 residential structures that date to the original lat nineteenth century establishment of the settlement.
CHL 21	Streetscape	Southcote Road from Book Road East to Airport Lands, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	This portion of Southcote Road is characterized as an evolved-dynamic streetscape CHL. It is a rural scenic streetscape composed of a gravel road with a narrow right-of-way, flat topography and mature trees lining the side the road. The north portion of the CHL is lined with residences, many of which date to the late nineteenth century establishment of the settlement o Southcote.
CHL 22	Farmscape	1224 Smith Road, Ancaster; Garfield Smith House	Inventoried; LACAC Report (1981); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1867, the property is believed to be associated with the Sm family. Reflective of the Ontario Cottage and Italianate architectural styles, the property displays a high degree of craftsmanship through its decorative brickwork. The one-and-a-half storey brick residence features a stone foundation, dichromatic brickwork including quoins and window detail, a three-bay facade with a central frontispiece with a gable roof and a ground floor entrance. The Farmscape includes a Central Ontario barn, as well as other detached accessory structures, a water body and mature trees.



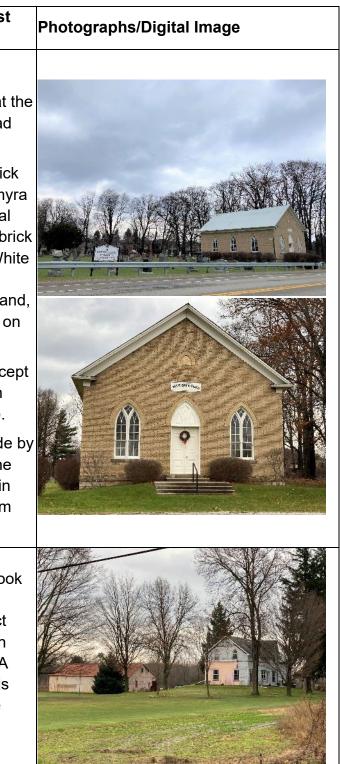
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 23	Farmscape	931 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1875, the residence on the property is setback significantly from Southcote Road, making it challenging to see. One detached outbuilding is located north of the residence, and a tree-lined driveway lead to the residence, accessed from Southcote Road.
CHL 24	Place of Worship	572 & 582 Southcote Road, Ancaster; Netherland Reformed Church; Providence Canadian Reformed Church	Inventoried, Volume 7A (only 572 Southcote Road)	Constructed in 1975, the property has historical associations with the Christian Reformed Church. The Providence congregation held their first worship on the property in 2007. The two-storey red brick church building a 582 Southcote Road is believed to have been constructed c.2007 in a contemporary style with Modern influences. The façade features a bell towe and covered porte-cochère. The adjacent vernacular two-storey residence at 572 Southcote Road may be the manse for the Reverenced of the Providence Canadian Reformed Church. Both structures are significantly setback from Southcote Road on grassed lawns with mature trees.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
				Garner's Corners Cemetery is a small Methodist/United cemetery with 130 monuments that was opened in 1842.
CHL 25	Cemetery	520 Southcote Road, Ancaster; Garner's Corners Cemetery	Inventoried	In 1831, a deed written on sheepskin parchment was drawn up between Israel Dawdy and the Canadian Wesleyan Methodists, also known as the Ryanites, for a plot of land at the northwest corner of the intersection of present day Southcote and Garner Roads. By the 1840s, the Church was owned and operated by a group of New Connexion Methodists, who renamed it Zion Methodist. The first burial took place in 1842 for Christeen Jarvis. By 1889, the church had closed, and the building was sold to Leona Back who used the lumber in the construction of his barn. The cemetery has remained in use, and is owned and maintained by the City of Hamilton.
CHL 26	Farmscape	14-44 Garner Road East, Ancaster	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1850, the residence on the property is representative of a Gothic Revival Ontario Cottage style of architecture. The one-and-a-half storey wood frame structure is clad in stucco and features a central gable peak on the façade and flanking chimneys. Overgrown cedar hedges are growing in front of the façade, making it challenging to view the residence. The property includes a collection of detached agricultural structures and mature trees.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
			Inventoried, Volume 7A; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report Volume 5 (Janet Wetmore, 1984)	The Gothic Revival White Brick Church was built by members of the Methodist Episcopal faith, the majority of whom were United Empire Loyalists and early settlers of Ancaster Township. They worshiped first at th Ebenezer Chapel built c.1830 at a now unknown location on Garner Road East.
CHL 27		Ancaster; White Brick /United Church and Cemetery; Fiddler's		Elder Emerson Bristol (1813-1895) was the catalyst behind the White Brick Church. He had arrived in Ancaster Township with his parents from Palmyra Township in New York State in 1829. He went on to become an influential circuit rider with the Methodist Episcopal Church, planning the new little bric church and giving \$100 of his own money toward its construction. The White Brick Church was built on land donated by John Rymal (1819-1859) and dedicated on October 11, 1857. A Sabbath School was formed in 1861, and as in Sunday services, males sat on one side of the church and females on the other.
				With the union of Methodist churches in 1884, the church was closed excep for Sunday School and quarterly meetings held until 1920. Since then an anniversary service has been held each year on the first Sunday in June.
				The church still contains the original, enormous wood-burning stove made a McQueston & Co., Hamilton, C.W. and is lit by coal-oil lamps. Many of the early congregation, including Emerson Bristol and his family, are buried in the adjacent White Brick Cemetery. The cemetery is classified as medium size, was opened in 1845 and contains 420 monuments.
CHL 28	Farmscape	254 Garner Road East, Ancaster; Kendrick House	Inventoried; 140 Garner Road East, Hamilton HIA (MHBC, 2018); LACAC Report Volume 4 (Wanda Zsiros, 1983); City of Hamilton AEGD Windshield Survey (2021)	storey vernacular farm house with a side gable roof. The Heritage Impact Assessment completed for the property in 2018 indicates that it has been significantly altered and is no longer representative of a particular style. A

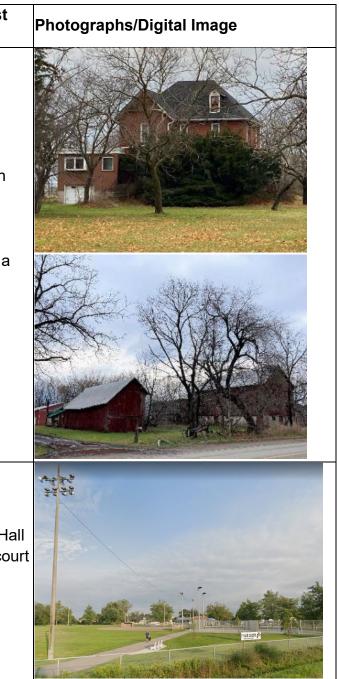


BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 29	Farmscape	474 Garner Road East, Ancaster	Identified in the field	The property appears to contain a remnant early twentieth century farmstead. The presence of mature vegetation makes it challenging to view the property from Garner Road East, however the residence appears to be one-and-a-half storey red brick structure with a cross gable roof, a front gable peak, a concrete foundation, and square window openings with brick lintels and rusticated stone sills. A barn clad in sheet metal and built to a rectangular plan with a side gable roof is located to the rear of the residence The property is densely covered with mature trees.
				Constructed in 1880, the farm house on the property appears to be a vernacular expression of the Neoclassical style. The severed lots fronting onto Garner Road East make it challenging to view the property, however i is a nineteenth century farmstead.
CHL 30	Farmscape	832 Garner Road East, Ancaster	Inventoried; LACAC Report Volume 3 (n.d.)	The one-and-a-half storey red brick farm house features a side gable roof and a four-bay façade. It appears that a rear addition has been constructed on the south elevation. A barn and other outbuildings are located to the rea of the farm house. All structures on the property are significantly setback from Garner Road East.



BHR or CHL #	Type of Property Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
			The first group of Methodists in Ancaster Township met in 1796. Their log chapel was erected in 1808 near the site of the present church. The chapel was the first meeting house of any denomination in the Township and became the base for the Ancaster Circuit.	
			On August 28, 1829, the Bowman Chapel was the site of the first Canadian Conference of the Methodist Episcopal Church. From this meeting of 30 church leaders, including Egerton Ryerson, who was ordained at the conference, came such significant developments as the creation of a Book Room (later Ryerson Press), the establishment of an Upper Canada Academy (later Victoria College), and the publishing of the Christian Guardian (later the United Church Observer). The conference also resulted in the founding of Canada's first Total Abstinence or Temperance Society.	
ICHI 31	876 & 880 Garner Road East, Ancaster; Bowman United Place of Worship / Church and Cemetery Church and Cemetery; Bowman Chapel; The Old Slide; The Sliding	Inventoried, Volume 7A; LACAC Report (n.d.)	The Bowman Chapel had been built on the lot line between land owned by Peter Bowman, a Methodist Episcopal, and Squire Henry Hagel, a local Justice of the Peace with leanings toward the Canadian Wesleyans or Ryanites. In 1830, disturbances caused by the Ryanites during services became so severe that a group led by Peter Bowman surreptitiously moved the chapel to Bowman's land. This prompted a lawsuit by Henry Hagel and another early morning move of the building onto Hagel's property. Hence the little log chapel became known as "The Sliding Church" or "The Old Slide".	
	Church		A new brick chapel was built squarely on the lot line in 1845. This church succumbed to fire in April 1881, but Church stewards collected enough subscriptions that same day to build the present Bowman Church in the Gothic Revival style, featuring Gothic stained glass windows. The present church was dedicated in March 1882, with the cornerstone of the 1845 church encased in a wall. A chair built in 1809 for the Rev. Andrew Prindel still remains in the church.	
			Bowman Cemetery is considered a medium sized cemetery with 250 monuments. The earliest known burial was in 1803 for Susannah Smith. The cemetery includes iron railings and a hedgerow and marble and granite monuments laid flat and upright, as well as cairns and columns. At the rear corner of the cemetery is a collection of 38 stones now laid flat, contained within a ground level stone wall. The site holds many of the oldest stones within the cemetery, including that of Susannah Smith.	

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 32	Farmscape	927 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1896, the vernacular farm house is reflective of the Queen Anne Revival architectural style. The two-and-a-half storey brown brick residence features a hip roof and dormer windows and a projecting front gable bay. A one-storey addition has been constructed on the west elevation, as well as a larger rear addition. The farm house is located on a nineteenth century farmstead, with a collection of barns and outbuildings located to its east, with a narrow setback from Garner Road East. The property is heavily treed.
CHL 33	Park	3027 Homestead Drive, Glanbrook	Registered Non-Designated	Mount Hope Park is a recreational area behind the designated Glanford Ha (BHR 88). The park contains a play structure, baseball diamond, tennis cou and other amenities. The park also houses a Youth Centre and the 779 Black Knight Squadron of the Air Cadets.



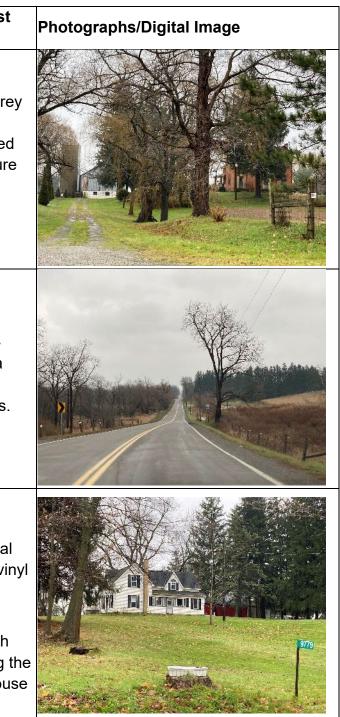
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
	Farmscape		Inventoried; Glancaster Road EA CHR (AECOM, 2021); City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1888, the property, a nineteenth century farmstead, may be associated with Robert Lovell. The 1875 Illustrated Historical Atlas of the County of Wentworth illustrates a farm house on the property in the general vicinity of the present structure.	
CHL 34				The farm house, set on raised topography, is a two-storey, red brick, Italianate structure with a frontispiece and a fieldstone foundation. Two brick chimneys flank the frontispiece and the windows appear to be paired and segmentally arched. The farmhouse displays a high degree of craftsmanship through its intricately designed wood verandah that spans half the length of the façade. The verandah includes wood fretwork and wood support posts. There is also bargeboard in the front gable.	
				The property includes barns clad in vertical wood siding and metal gable roofs, as well as a long driveway and agriculture fields, consistent with early twentieth century agricultural patterns. The property defines, maintains and supports the agricultural character of the area, and illustrates the evolution of farming on the property.	f
CHL 35	Farmscape	1205 Glancaster Road, Glanbrook	Inventoried	The collection of buildings on the property are setback a significant distance from Glancaster Road and are further screened by mature vegetation, making it challenging to view. A review of aerial imagery has indicated that the property is composed of an agricultural landscape with a residence built to a square plan with a gable roof. A long tree-lined driveway leads to the residence and a collection of outbuildings are located to the east of the house. A wind break of trees shields the residence from the east. The remainder of the property is composed of agricultural fields.	

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
				Constructed in 1850, the farmstead is associated with the Smith and Deha families.
CHL 36	Farmscape	1292 Glancaster Road, Ancaster; Smith House	Inventoried; LACAC Report Volume 4 (Tina Agnello,1983); City of Hamilton AEGD Windshield Survey (2021)	The vernacular Italianate red brick farm house on the property displays a high degree of craftsmanship, notably demonstrated in its brickwork. The one-and-a-half storey structure features a cross-gable roof, dichromatic decorative brickwork, and a covered porch. A workshop is located south of the residence and a split rail fence lines the landscaped lawn in front of the residence.
				The property is located on raised topography at the prominent intersection Glancaster Road and Butter Road East and helps define the historic character of the intersection and streetscape.
CHL 37	Farmscape	11731 (Jancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1918, the residence on the property is a vernacular Edwardian style structure. The two-and-a-half storey residence is built to a L-shaped plan and features a hip roof. The façade includes a projecting ba with a pedimented gable with a window. A covered porch wraps around the southwest corner of the residence.
				A garage is located south of the residence, and the buildings on the proper are surrounded by mature trees. The topography of the property is raised.
				MPAC data indicates that the residence on the property was constructed in 1930, however previous LACAC research suggests it was built c.1854-58. The property is associated with the original owner, John Mitchell, as well a the Braun family, who continue to operate Braun's Nursery on the property
CHL 38	Farmscape	2004 Giancaster Road, Ancaster; Braun Nursery Ltd	Inventoried; LACAC Report Volume 4 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	The residence on the property is a vernacular farm house influenced by the Gothic Revival and Italianate styles. The one-and-a-half storey red brick residence features a side gable roof, projecting front gable with a lancet window below and dichromatic brickwork, including quoining. The property displays a high degree of craftsmanship for its decorative brickwork.
				The property contains a number of outbuildings and is largely dedicated to the operation of the nursery. The property is located close to the road at th prominent intersection of Carluke Road East, Glancaster Road and White Church Road West.



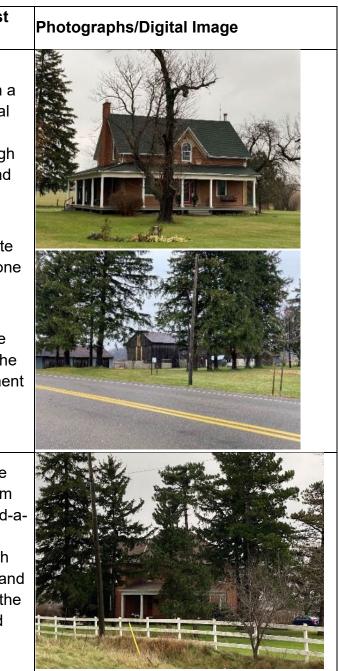
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 39	Farmscape	3071 Glancaster Road, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Constructed in 1870, the farm house on the property is reflective of the Queen Anne Revival architectural style. The red brick two-and-a-half stores structure features a cross gable roof with a central projecting bay on the facade. The entrance on the façade features decorative casing. A covered porch is located at the rear of the residence. Given the presence of mature vegetation and the setback of the buildings, no further details could be confirmed. The farmscape also contains a Central Ontario barn, two concrete silos, grain bins, other outbuildings, and a tree-lined driveway.
CHL 40	Streetscape	White Church Road West (Glancaster Road to Highway 6)	Identified in the field	This portion of White Church Road West is characterized as an evolved- dynamic streetscape CHL. It is a rural scenic streetscape composed of a two-lane asphalt road with rolling topography, lined with mature trees, vegetation, a collection of nineteenth century farmscapes, and fencerows.
CHL 41	Farmscape	9779 White Church Road West, Glanbrook; Carpenter House	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	 The farm house on the property was constructed in 1860 by the original owner, Brian Carpenter. The style of the residence is representative of a vernacular Gothic Revival Farm House. The wood frame one-and-a-half storey structure is clad in vin siding and features a stone foundation, cross-gable roof, covered (now enclosed) porch, and a projecting gable peak. It is recorded that there may be multiple houses on the property, although this could not be confirmed in the field due to dense tree cover screening the property. Agricultural outbuildings are located to the south of the farm house and the property contains many mature trees and raised topography.



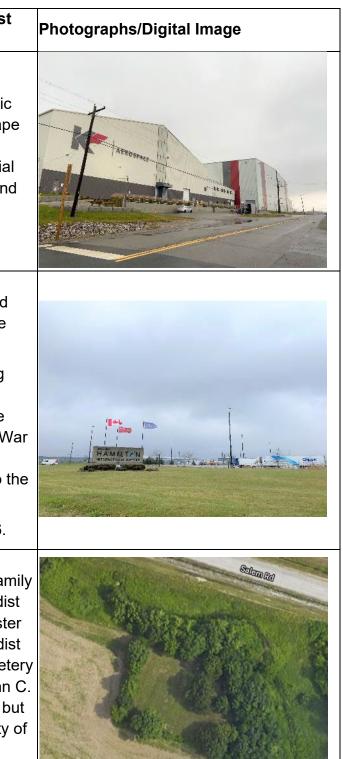


BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
CHL 42	Farmscape	9490 White Church Road West, Glanbrook	Identified in the field	The property contains a nineteenth century farmstead set on raised topography. The buildings are clustered with a minimal setback from White Church Road West. The farm house is a one-and-a-half storey wood frame vernacular structure with Gothic Revival influences that likely dates to the turn of the twentieth century. It features a cross gable roof with projecting bays with gable peaks, a bay window, and is clad in vinyl siding. The barn or the property appears to be a Central Ontario barn with a gambrel roof. An additional outbuilding and remnant silo are located to the west of the barn. A livestock yard is located east of the collection of buildings on lower topography adjacent to White Church Road West.	
				On mapping (see Figure 8 and 9, Appendix D), the portion of the property that contains the residence and barns appears to be classified as 9370 White Church Road West (CHL 44), despite the placement of fire numbers in front of the property that read 9490.	
		9485 White Church Road West, Glanbrook; Smith- Pearce Farm	Inventoried; LACAC Report (1984)	The property is believed to have been constructed between 1842 and 1870 for the original owner, Cylus Smith.	
CHL 43	Farmscape			The farm house, barns and outbuildings on the farmscape are setback a significant distance from the road, making them challenging to assess. The residence appears to be a wood frame one-and-a-half storey vernacular structure reflective of the Neo-Classical style. It features a side gable roof with a chimney and a symmetrical three-bay façade clad in aluminum siding.	

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
	Farmscape		Inventoried; City of Hamilton AEGD Windshield Survey (2021)	The property consists of a nineteenth century agricultural farmscape with a farm house constructed in 1868 by the Dickenson Brothers for the original owner, Ferguson Smith. The farm house is a vernacular one-and-a-half storey red and yellow brick Ontario Cottage. The residence displays a high degree of craftsmanship for its dichromatic brickwork including quoins and drip moulds. It also features a side gable roof, a brick chimney, a centre gable peak and a wraparound porch supported on columns.
CHL 44				The farm house and barns are clustered with a narrow setback from White Church Road West. The barns feature wood barn board cladding and stone foundations. A water course traverses the property.
				On mapping (see Figure 8and 9, Appendix D), the address of 9370 White Church Road West also appears to include the residence and barns on the property at 9490 White Church Road West (CHL 42), despite the placemer of fire numbers.
CHL 45	Farmscape	9349 White Church Road West, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	The property consists of an early twentieth century agricultural farmscape owned by Ernest Whaley with a farm house constructed in 1911. The farm house is a representative example of Edwardian Classicism. The two-and-a half storey red brick structure features a stone foundation, a hip roof, a projecting bay with a pedimented gable, and a wraparound covered porch supported by columns. A large Central Ontario barn with a gambrel roof an an outbuilding are located south of the residence. The area surrounding the farm house contains a number of mature trees and a wood fence painted white.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 46	Streetscape	Airport Road West, south of airport lands, Glanbrook	nventoried; Hamilton's leritage Vol. 3; Hamilton apid Transit A-Line CHAR ASI, 2012) nventoried; Dickenson Road A, Upper James Street to Blancaster Road CHR (ASI, 019); Twenty Road West lan of Subdivision, Blanbrook CHSR (Golder, 019)	This portion of Airport Road West is characterized as an evolved-dynamic streetscape CHL. It is a two-lane paved arterial / collector road streetscape that travels south of the John C. Munro Hamilton International Airport landscape, and is characterized by contemporary institutional, commercial and public transportation complexes, including all airport-related lands and structures.
CHL 47	Transportation	9300 Airport Road, Glanbrook; John C. Munro Hamilton International Airport	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	The John C. Munro Hamilton International Airport landscape, as a commercial and public transportation complex, includes all airport-related lands and structures. Today, the airport consists of two runways, a single terminal, various outbuildings, and encompasses 484 hectares. An airport was first built on this site in 1940 to meet military needs during World War II, only becoming a civilian airport after 1963. Much of the landscape has since been altered as a result of facility expansion but the property does retain a number of older hangar buildings. The Canadian Wa Plane Heritage Museum was originally housed in one of these older structures but is now located at 9280 Airport Road (CHL 49), adjacent to the Airport. More information on the history of the Airport is included in Section 4.3.6.
CHL 48	Cemetery	Salem Cemetery (Salem Road within the boundaries of The John C. Munro Hamilton International Airport, Glanbrook)		In 1811, Jacob Smoak registered the Crown Patent for Lot 1 in the 4th Concession of Glanford Township. Part of this property was used as a fam cemetery as early as 1834. By the 1870s, the land also hosted a Methodis Episcopal Church. Known as the Salem Church, it was part of the Ancaste Episcopal Circuit until 1884, when it became part of the Glanford Methodis Circuit. The church was closed between 1903 and 1905. The small cemeter with 37 monuments was restored in 1940. With the expansion of the John Munro International Airport, the cemetery was thought to be threatened, built has survived relatively untouched, and is now under the care of the City Hamilton.



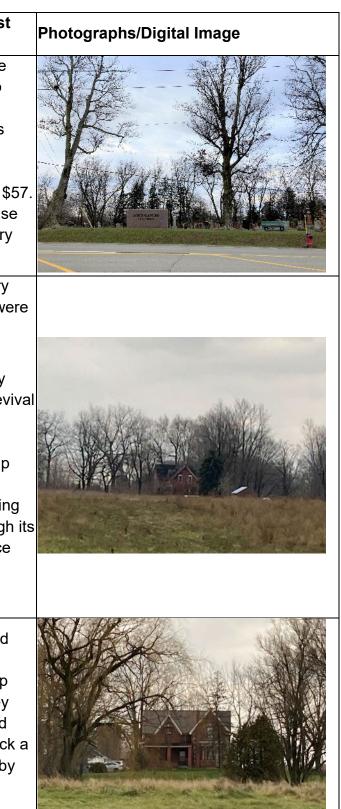
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 49	Institutional	9280 Airport Road, Glanbrook; Canadian War Plane Heritage Museum	Identified in the field	In 1972, the Canadian Warplane Heritage Museum was founded on the easide of the present John C. Munro Hamilton International Airport lands as a non-profit organization whose mandate included preserving a complete collection of aircraft flown by Canadians and the Canadian military from the beginning of World War II to present. On February 15, 1993, a fire destroyed Hangar 3 and one of the Museum hangars. As a result of the fire and significance of the site, Regional Council earmarked \$12 million to develop new museum. The new Museum opened to the public on March 2, 1996. The Museum is located directly adjacent to the Airport (CHL 47).
CHL 50	Streetscape	Airport Road between East Cargo Road and Homestead Drive, Glanbrook	dentified in the field nventoried; Hamilton Rapid ransit A-Line CHAR (ASI, 012)	This portion of Airport Road West is characterized as an evolved-dynamic streetscape CHL. It is a two-lane paved arterial / collector road streetscape of small post-war bungalows representative of a mid-twentieth century neighbourhood. This streetscape is typical of a neighbourhood of workers' houses and may be associated with the Airport (CHL 47). The houses all si on large lots setback from the road suggesting that the extant structures ma have been constructed on earlier lots and evidences an early-twentieth century land use. The streetscape retains much of its original character.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 51	Farmscape	9867 Dickenson Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); City of Hamilton AEGD Windshield Survey (2021)	The property consists of a nineteenth century agricultural farmscape that was originally owned by Silas Smith. MPAC information notes the farm house on the property was constructed in 1871, however LACAC research indicates it was built in 1848. The residence is a two-storey wood frame vernacular structure clad in red brick and vinyl siding. It features a side gab roof and centrally placed entrance covered with a pedimented roof. The structure appears to have been modified, and potentially altered through th addition of a second storey. A rear addition projects from the south elevation A Central Ontario barn with a gambrel roof is located south of the farm house. The remainder of the property is composed of agricultural fields. Th long driveway, circulation route, and agricultural fields are consistent with nineteenth-century agricultural patterns. The property is proposed for redevelopment by Broccolini.
CHL 52	Farmscape	9236 Dickenson Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Documentation & Salvage Report (Megan Hobson, 2020); City of Hamilton AEGD Windshield Survey (2021)	The vernacular Farm House on the property was owned and constructed in 1892 (date identified by LACAC) by Amos French. The one-and-a-half storey, wood frame, L-shaped residence features a stor foundation, a cross gable roof clad in steel, a corbelled chimney, and a single-storey addition on the west elevation. A long driveway leads from Dickenson Road to the residence. The driveway and tree-lined property are consistent with nineteenth-century settlement patterns. Demolition is pending for this property.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 53	Cemetery	9050 Dickenson Road West, Glanbrook; North Glanford Cemetery; "Mud" Church Cemetery; Dickenson Cemetery	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	The Glanford Congregational Church was established in 1843, and in the following year it purchased half an acre of property from Samuel Hess to establish a cemetery. The earliest known burial at the North Glanford Cemetery occurred in 1845 for Mary Ann Terryberry. A mud building was erected on the property, which gave the church its common name, "Mud Church". This building served the congregation for 20 years, until 1865, when it was closed. In 1873, the land was sold to Edward Dickenson for \$5 The Dickenson family maintained the cemetery from their date of purchase to 1953, when the responsibility was transferred to the Glanford Cemetery Board. Today, the medium size cemetery contains 300 monuments.
				The one-and-a-half storey red brick farm house on the nineteenth century farmscape was constructed c.1850 for Obedia Smith. The Smith family we some of the first settlers in Glanford Township.
CHL 54	Farmscape	9751 Twenty Road West, Glanbrook	Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Twenty Road West Plan of Subdivision in Glanbrook CHIA (Golder, 2020); City of Hamilton AEGD Windshield Survey (2021)	The Twenty Road West Plan of Subdivision in Glanbrook CHIA (Golder, 2020) confirmed the property to have CHVI for its mid-nineteenth century brick farm house built to a high degree of craftsmanship in the Gothic Revivestyle, for its association with the pioneering and locally prominent Smith family, the potential it has to yield information that contributes to an understanding of the early agricultural development of Glanford Township and the affluence of farmers in the community during the mid-nineteenth century, and its contextual value as the property is important in maintaining and supporting the rural character of the Twenty Road West block through open fields, relict orchards, and dispersed farmsteads set back a distance from the road.
CHL 55	Farmscape	9575 Twenty Road West, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	The vernacular Farm House on the property was constructed in 1871 and exhibits influences representative of the Gothic Revival and Italianate architectural styles. The property displays a high degree of craftsmanship most notably through the decorative brickwork. The one-and-a-half storey brick farm house features a cross-gable roof, covered porch, and red and yellow dichromatic brickwork including quoining. The residence is set back significant distance from Twenty Road West and the property is flanked by rows of mature trees.



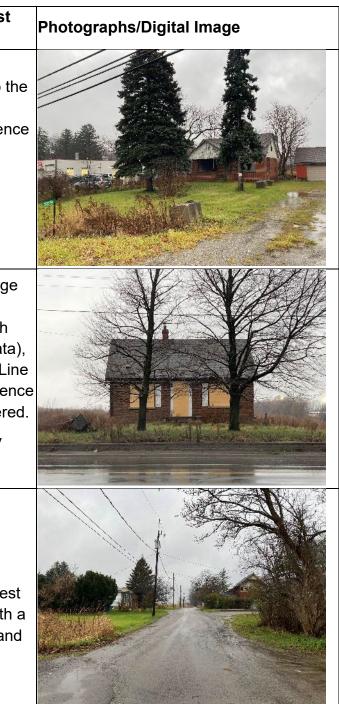
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
				The two-storey red brick farm house on the nineteenth century farmscape was constructed in 1900 for William Marshall.	
CHL 56	Farmscape	9511 Twenty Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Upper West Side Draft Plan of Industrial Subdivision in Glanbrook CHIA (Golder, 2018); City of Hamilton AEGD Windshield Survey (2021)	The Upper West Side Draft Plan of Industrial Subdivision in Glanbrook CHIA (Golder, 2018) confirmed the property to possess CHVI as a rare or unusual example of a late-nineteenth century brick farmhouse built in the Italianate- style, for its high degree of craftsmanship in its masonry construction, for its association with the Marshall family who played a significant role in the overall development of Glanford Township, for the potential it has to yield information that contributes to an understanding of the early agricultural development of Glanbrook and the affluence of farmers in the community during the mid-nineteenth century, and its contextual value as the property is important in maintaining and supporting the rural character of the Twenty Road West block through its open fields, relict orchards, and dispersed farmsteads set back a distance from the road. The property is vacant and proposed for demolition.	
CHL 57	Farmscape	9445 Twenty Road West, Glanbrook; Marshall House; Rose Farm	Registered Non-Designated; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Upper West Side Draft Plan of Industrial Subdivision in Glanbrook CHIA (Golder, 2018); City of Hamilton AEGD Windshield Survey (2021)	The one-and-a-half storey red and yellow brick farm house on the nineteenth century farmscape was constructed in 1874 for Alexander Marshall Jr., which is identified by a date stone on the house that reads "Alex. Marshall A.D. 1874". Alex Marshall Jr. lived on the property with his wife Marcia and children and named the house 'Rose Farm'. The Marshall family played a significant role in the development of Glanford Township. The Upper West Side Draft Plan of Industrial Subdivision in Glanbrook CHIA (Golder, 2018) confirmed the property to possess CHVI as a representative example of a side-hall or gabled ell Gothic Revival farmhouse, for its high degree of craftsmanship in its masonry detailing, for its association with the Marshall family who played a significant role in the overall development of Glanford Township, for the potential it has to yield information that contributes to an understanding of the early agricultural development of Glanbrook and the affluence of farmers in the community during the midnineteenth century, and its contextual value as the property is important in maintaining and supporting the rural character of the Twenty Road West block through its open fields, relict orchards, and dispersed farmsteads set back a distance from the road.	

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)	Photographs/Digital Image
				The two-and-a-half storey red and yellow brick farm house on the nineteenth century farmscape was constructed c.1860 for George Marshall. The Marshall family played a significant role in the development of Glanford Township.	et
CHL 58	Farmscane	9285 Twenty Road West, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Twenty Road West Plan of Subdivision in Glanbrook CHIA (Golder, 2020); City of Hamilton AEGD Windshield Survey (2021)	The Twenty Road West Plan of Subdivision in Glanbrook CHIA (Golder, 2020) confirmed the property to have CHVI for its mid-nineteenth century brick farm house built to a high degree of craftsmanship (decorative dichromatic brickwork) in the Gothic Revival style, for its association with pioneering and locally prominent Marshall family, for the potential it has to yield information that contributes to an understanding of the early agricultural development of Glanbrook and the affluence of farmers in the community during the mid-nineteenth century, and its contextual value as the property is important in maintaining and supporting the rural character of the Twenty Road West block through its open fields, relict orchards, and dispersed farmsteads set back a distance from the road.	
				The property is vacant and proposed for demolition.	
CHL 59	IFarmscane	1902 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	This property represents a former nineteenth century farmscape, however all buildings have been removed, including the remnant stone fence and posts. Only agricultural fields remain. Clusters of mature vegetation were observed indicating the rough location of the former structures on the property. A small watercourse runs through the lot and under Twenty Road West.	
CHL 60	Farmscane	2028 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The vernacular one-storey twentieth century wood frame residence sits on a small rise of land south of a stream that runs roughly east-west. Mature plantings border the stream with large willow trees located on the south side. The portion of the property in front of the house retains is natural topography and early character. The land to the south has been severed and altered by the placement of a hydro line corridor.	

BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 61	Streetscape	2072-2084 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	This landscape is characterized by a small cluster of post-war residences typical of the land use that occurred during and after the initial post-war construction boom. These houses are likely remnants of the development of the area as it transitioned from rural properties and farm complexes to a pe urban landscape with commercial enterprises. These residences would hav been typical of the area before that transition.
CHL 62	Farmscape	2081 Upper James Street, Glanbrook; Stoneholm Farm	Registered Non-Designated; LACAC Report (1984); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The property is a nineteenth century farmscape. The Ontario Cottage was constructed in 1853 by George Coon for the Fletcher family. The residence is a one-and-a-half storey stone block structure with a symmetrical three-ba façade, side gable roof, centrally placed window in the gable peak above th door with bargeboard and chimneys on each gable end. The centrally place door, sidelights, transom and pediment over the entrance remain. The farm complex, which is representative of early farming practices, consists of two large barns with gable roofs, a smaller barn/driveshed, mature conifers and deciduous trees along the frontage, and a decorative wrought iron fence and gate at Upper James Street. The landscape of the farm appears to have retained its original / early features and topography.
CHL 63	Estate	2173 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The small stone house, constructed in 1850, sits in an area of low topography on the heavily wooded estate property, and is almost complete obscured when viewed from the road. It appears to have a hip roof with two dormer windows on the facade. The property is accessed via a long dirt driveway that includes decorative gates at the entrance with a sign that reads, in part, 'Insane Asylum'. These gates may have been moved here from another location. A review of aerial imagery indicates that there is at least one other structure on the property at the rear of the house. The lot is heavily wooded and includes a small watercourse at the northwest corner.



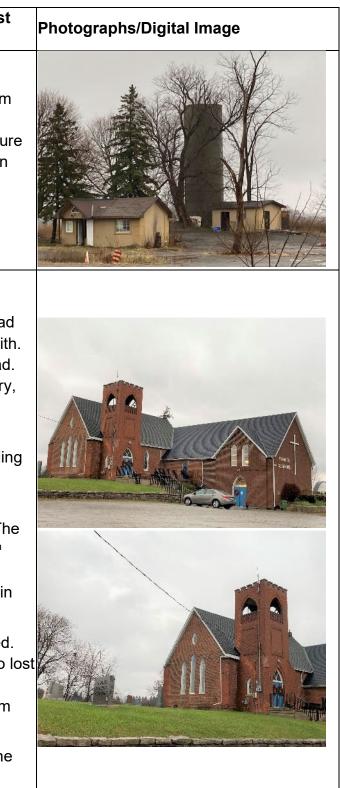
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 64	Farmscape	2219 Upper James Street, Glanbrook	Inventoried	Constructed c.1940, the one-storey wood frame bungalow contributes to the streetscape of small-to-medium-scale residences from the mid-twentieth century along this section of Upper James Street. In this case, the residence was likely constructed early in the post-war boom. It is a remnant of the development of the area as it transitioned from rural properties and farm complexes to a peri-urban landscape with commercial enterprises. The residence is boarded and vacant.
CHL 65	Farmscape	2240 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The former nineteenth century farmscape that occupied the property (large barn with a gambrel roof and silo, large shed with double doors and a gambrel roof, modern frame garage) was largely demolished in 2021 with only the one-storey residential bungalow, constructed in 1940 (MPAC data remaining. Previous heritage reporting for the Hamilton Rapid Transit A-Lir CHAR completed by ASI in 2012 (SgD28) indicated that the extant resider may be the farmscape's original 1820s farm house, however heavily altere The residence is boarded and vacant and redevelopment of the property appears to have begun.
CHL 66	Streetscape	Talbot Lane west of Upper James Street, Glanbrook	Inventoried	Talbot Lane is an evolved-dynamic late-nineteenth and early-twentieth century residential streetscape included in the North Glanford Historic Settlement Area. It is a narrow, single lane, tar and chip local road constructed to a rural cross-section that extends approximately 200 m wes from Upper James Street before terminating. The streetscape is lined with small number of large and small lot residences dating to the nineteenth and twentieth century.



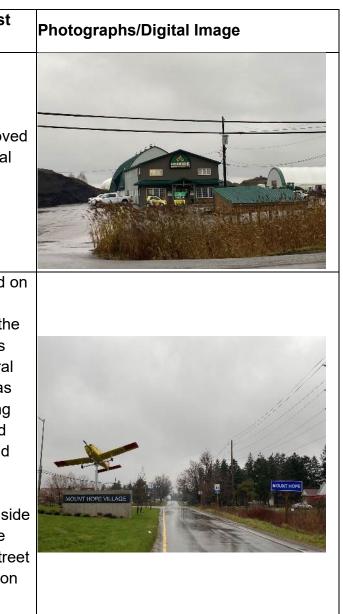
BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 67	Streetscape	2273-2285 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	This landscape is characterized by a cluster of small to medium scale residences that date between 1953 and 1965, and are typical of the land us that occurred during and after the initial post-war construction boom. The structures are built on similar sized lots with uniform setbacks from Upper James Street. These houses are likely remnants of the development of the area as it transitioned from rural properties and farm complexes to a peri- urban landscape with commercial enterprises. These residences would hav been typical of the area before that transition.
CHL 68	Streetscape	2332 and 2340 Upper James Street between Talbot Lane and Dickenson Road, Glanbrook	Inventoried	The landscape is composed of two houses built between the 1860s and 1870s in the settlement of North Glanford. These structures, together with four former frame structures, once formed a streetscape between Talbot Lane and Dickenson Road in North Glanford. Both of these properties are long, narrow parcels, typical of the original eighteenth and nineteenth centu lots. The one-and-a-half storey brick residence at 2332 Upper James Street is located in close proximity to the road, and its counterpart at 2340 Upper James Street is also brick with a projecting bay and gable. These houses a typical of a mid-nineteenth century rural / village streetscape.
CHL 69	Settlement	North Glanford (1875 Illustrated Historical Atlas of the County of Wentworth boundaries)	Inventoried	The settlement of North Glanford is an evolved-dynamic landscape focused around the intersection of present day Upper James Street and Dickenson Road. Historically, the settlement had no clearly defined boundaries, but extended along both sides of Upper James Street from Twenty Road to on mile south of Dickenson Road. It was a small community with little business activity and was likely shown on early county maps as the North Glanford post office was located here. Settlement began in the 1860s, and included number of residences, a small hotel, an inn/tavern, blacksmith shop, school house, church and cemetery (CHL 53).



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 70	Farmscape	2543 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The property represents a remnant farm complex. It appears that the farm house and barns have been moved and the ruins of a silo and small contemporary utilitarian structures are all that is left on the property. Mature deciduous trees remain that appear to have demarcated the line between farm yard and field.
				Redevelopment of the property appears to have begun.
				In 1850, the Rev. Robert Merritt, a travelling missionary of the Church of England, invited a small group of Glanford Township churchgoers to a meeting in Jacob Terryberry's Hotel on the Hamilton-Caledonia plank road near Mount Hope. The objective was to build a church of the Anglican faith Joseph Choate offered an acre of land on the east side of the Plank Road. On January 28, 1851, a building committee was formed; Jacob Terryberry, Jacob Kern, Joseph B. Choate, Joseph Bates, David Choate, Samuel McKee, members.
	Place of Worship / Cemetery	[/] Glanbrook; St. Paul's	Inventoried, Volume 7A; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	On September 30, 1851, an agreement was drawn up between the building committee and Robert Blair and John Simple for the construction of the Gothic Revival church. The building committee supplied the materials.
CHL 71				New oak seats were installed in the church in 1910 and are still in use. The bell tower and new entrance were built in 1926 to commemorate the 75 th anniversary of the church. Electric lights, a gift of the A.S. Turner family, were installed in 1932, replacing the seven coal oil lamps that had been in use since 1861.
				In 1943, a quarter-acre of additional land for the cemetery was purchased. Space was given for the burial of the 14 Royal Air Force servicemen who lo their lives while training at Mount Hope Navigational School. Services of remembrance have been held yearly since then. The cemetery is medium sized and contains 300 monuments.
				A parish hall was added in 1951, with a further extension completed in the late 1990s.



BHR or CHL #	Type of Property	Location	Heritage Recognition	Description of Known or Potential Cultural Heritage Value or Interest (CHVI)
CHL 72	Agricultural	2907 Upper James Street, Glanbrook	Inventoried; LACAC 1984	This property once comprised a nineteenth century agricultural farm complex, however it appears that the original structures have been remove to accommodate the current commercial land use. From a review of aerial imagery, the property contains a number of outbuildings. It could not be confirmed if any are original to the former farmscape.
CHL 73	Settlement	Mount Hope (Homestead Drive)	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	The settlement of Mount Hope is an evolved-dynamic landscape focused of Homestead Drive. Settled in 1810, Mount Hope was originally known as Swazie's Corners and then Hines Corners, after the individuals that ran the hotel at the southwest corner of Homestead Drive and Airport Road. This landscape derives its character from three main periods: (1) an intact rural nineteenth-century landscape containing farm houses and extant fields as well as village settlement along both sides of Homestead Drive; (2) strong evidence of structures and landscapes related to the Royal Air Force and Royal Canadian Air Force navigation school and associated activities and ancillary structures that led to the development of the John C. Munro Hamilton International Airport; and (3) the evolved early to mid-twentieth century landscape of a small village. Many of the properties on the west sid of Homestead Drive retain their original house / field alignments. In some cases, historic barns are extant. The properties on the east side of the stree back onto Upper James Street and most do not include fields, but do sit on large lots. Mature trees are found on both sides of the street.



APPENDIX C

Impact Assessment



BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 1	Residence	558 Fiddlers Green Road, Ancaster; Smith/Rowley House	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Heritage Research Report (Carolyn Hawley, 1980)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	rail fence), resulting in minimal If a portion of the property is ac (CHER) is recommended to ev Regulation 9/06. If the property proposed to be directly adverse Heritage Impact Assessment (The impacts of vibrations may assessment to ensure there ar resulting mitigation measures s needed.
BHR 2	Residence	588 Fiddlers Green Road, Ancaster	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report (n.d.)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI a impacted by the road design, th

Table 3: Impacts and Preliminary Mitigation Strategies for Built Heritage Resources and Cultural Heritage Landscapes within the Study Area

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uisition and vibration impacts on the subject If required, property acquisition is anticipated to hay cause a temporary impact to the property. urned to pre-construction conditions (i.e., the split al irreversible negative impacts to the property.

acquired, a Cultural Heritage Evaluation Report evaluate the property's CHVI using Ontario rty is determined to possess CHVI and is rsely impacted by the road design, then a Cultural (CHIA) is recommended.

y need to be determined through an engineering are no negative impacts to the resource. Any s should be implemented prior to construction, as

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isition and vibration impacts on the subject f required, property acquisition is anticipated to ay cause a temporary impact to the property. urned to pre-construction conditions, resulting in impacts to the property.

acquired, a CHER is recommended to evaluate ntario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

			<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
lidence	Road Ancaster	(2021)	Alternative 2 (Amended network) Indirect: The residence on the subject property is located within the 40m vibration buffer from the proposed widening of Fiddlers Green Road to a minor 4-lane arterial road.	assessment to ensure that there resulting mitigation measures sh
ide	ence	ence Road Ancaster	ance 1032 Fiddlers Green AFGD Windshield Survey	AEGD Windshield Survey Road, Ancaster (2021) AEGD Windshield Survey (2021) The residence on the subject property is located within the 40m vibration buffer from the proposed widening of Fiddlers Green

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bject property should be avoided.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 4	Residence	1039 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing)No impacts are anticipated.Alternative 2 (Amended network)Direct:The residence on the subject property islocated on a corner lot with minor 4-lanearterial roads proposed adjacent to thenorth (Book Road East) and west (FiddlersGreen Road) sides of the lot.Encroachment and property acquisition isexpected as a result of the road widening.Indirect:Measured from the road corridor, theresidence on the subject property is locatedwithin the 40m vibration buffer. Therefore,the potential for indirect impacts due tovibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Encroachment, property acquis property should be avoided. Giv notably from Book Road East (a proposed minor 4-lane arterial r right-of-way (ROW). If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, the The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.
BHR 5	Residence	1297 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the west side of the lot (Fiddlers Green Road), and new minor collector roads are proposed to be introduced adjacent to the north and east property boundaries. Minor encroachment and property acquisition is expected as a result of the road widening and new minor collector roads. Indirect: Measured from the Fiddlers Green Road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Encroachment, property acquis property should be avoided. If r be minimal. The road work may The landscape should be return minimal irreversible negative im If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.



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isition and vibration impacts on the subject Given the residence's minimal setback, most (approximately 8 m), it is recommended that the I roads be accommodated within the existing

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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isition and vibration impacts on the subject f required, property acquisition is anticipated to ay cause a temporary impact to the property. Irrned to pre-construction conditions, resulting in impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 6	Residence	1694 Fiddlers Green Road, Ancaster	Inventoried, LACAC Heritage Report (1983)	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	minimal irreversible negative im If a portion of the property is act the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 7	Commercial	1770 Fiddlers Green Road, Ancaster; Oakrun Farm Bakery	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The residence on the subject property is located approximately 330 m from the minor 4-lane arterial roads proposed to meet at the intersection of Fiddlers Green Road and Carluke Road West. The subject property is composed of a large industrial bakery complex and the residence is situated on the west side of the lot.	<u>Alternative 1 (Do nothing) & Alte</u> None required.

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isition and vibration impacts on the subject f required, property acquisition is anticipated to ay cause a temporary impact to the property. Irrned to pre-construction conditions, resulting in impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

<u> Alternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 8	Residence	166 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north side of the lot (Book Road East). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. If re be minimal. The road work may The landscape should be return minimal irreversible negative im If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI ar impacted by the road design, th The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl
BHR 9	Residence	343 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the south side of the lot (Book Road East). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	needed.Alternative 1 (Do nothing)None required.Alternative 2 (Amended networkEncroachment, property acquisiproperty should be avoided. If rebe minimal. The road work mayThe landscape should be returnminimal irreversible negative imIf a portion of the property is accthe property's CHVI using Ontardetermined to possess CHVI animpacted by the road design, theThe impacts of vibrations may nassessment to ensure there areresulting mitigation measures shneeded.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	Alternative 1 (Do nothing) None required.
BHR 10	Institutional / Place of Worship	522 Book Road East, Ancaster; Christadelphian Heritage College	Inventoried (Places of Education)	<u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north side (Book Road East) and east side (Southcote Road) of the property, and a roundabout is proposed at the intersection of Book Road East and Southcote Road. Property acquisition and encroachment is expected.	Alternative 2 (Amended networ Encroachment and property act should be avoided and the prop should be moved to a less sens removed from the AEGD TMP of If property is acquired and the r recommended to evaluate the p If the property is determined to adversely impacted by the road
BHR 11	Residence	558 Book Road East, Ancaster	Inventoried	 <u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: Minor 4-lane arterial roads are proposed at the intersection of Book Road East and Southcote Road, adjacent to the subject property, and a roundabout is proposed on the northwest corner of the property. Property acquisition and encroachment is proposed. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated. 	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Property acquisition, encroachr and the proposed roundabout a should be moved to a less sens removed from the AEGD TMP of If the minor 4-lane arterial roads recommended to evaluate the p If the property is determined to adversely impacted by the road The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.

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acquisition impacts on the subject property oposed roundabout on the subject property nsitive property, potentially to the east, or P network.

e roundabout is introduced, a CHER is e property's CHVI using Ontario Regulation 9/06. to possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

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hment, and vibration impacts should be avoided t at the crossroads of the Southcote settlement nsitive location, potentially to the east, or P network.

ids and roundabout are introduced, a CHER is property's CHVI using Ontario Regulation 9/06. to possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 12	Residence	1071 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network)	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. The the property. The landscape sho resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the The impacts of vibrations may n assessment to ensure there are
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.



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isition and vibration impacts on the subject he road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 13	Residence	1085 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the west side of the property (Southcote Road). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore,	Alternative 2 (Amended networl Encroachment, property acquis property should be avoided. The the property. The landscape sho resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th The impacts of vibrations may r assessment to ensure there are
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures s needed.
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 14	Residence	911 Smith Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Indirect: A 2-lane collector road is proposed adjacent to the west side of the property (Smith Road). Encroachment is not anticipated. Measured from the road	<u>Alternative 2 (Amended network</u> Avoid any vibration impacts. Th the property. The landscape sho resulting in minimal irreversible The impacts of vibrations may r
				corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	assessment to ensure there are resulting mitigation measures s needed.

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isition and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

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The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 15	Residence	4 Garner Road East, Ancaster; Clink House	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report Volume 2 (Carolyn Hawley, 1980); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: As the residence is located on a corner lot, a major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East), and a minor 4-lane arterial road is proposed adjacent to the west side of the lot (Fiddlers Green Road). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore,	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment and vibration imp avoided. Given the residence's Road East (approximately 7 m), and minor 4-lane arterial roads way (ROW). Should it be determined that the the road widening, encroachme Evaluation Report (CHER) is re- using Ontario Regulation 9/06. I and is proposed to be directly a Cultural Heritage Impact Assess The impacts of vibrations may m assessment to ensure there are
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.

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mpacts on the subject property should be 's minimal setback, most notably from Garner n), it is recommended that the proposed major Is be accommodated within the existing right-of-

here is no other technically feasible solution for nent should be minimized. A Cultural Heritage recommended to evaluate the property's CHVI 5. If the property is determined to possess CHVI adversely impacted by the road design, then a essment (CHIA) is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 16	Residence	45 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The subject property is situated with a deep setback (65 m) from Garner Road East and is buffered by a 1950s subdivision on either side. Given this setback, no impacts are anticipated to result from the major 4-lane arterial road (Garner Road East) proposed to the south of the property.	Alternative 1 (Do nothing) & Alternative 1
BHR 17	Place of Worship	70 Garner Road East, Ancaster; Ancaster Christian Reformed Church	Inventoried, Volume 7A; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Property acquisition and vibration avoided. If a portion of the property is ac (CHER) is recommended to evan Regulation 9/06. If the property proposed to be directly adverse Heritage Impact Assessment (C The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.
BHR 18	Residence	114 Garner Road East, Ancaster; Book House	Inventoried; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report Volume 6 (1985); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East), and a new 2-lane collector road is proposed adjacent to the west property boundary. Encroachment and property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Encroachment and property act avoided. The road work may ca landscape should be returned t minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.

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ation impacts on the subject property should be

acquired, a Cultural Heritage Evaluation Report evaluate the property's CHVI using Ontario rty is determined to possess CHVI and is rsely impacted by the road design, then a Cultural (CHIA) is recommended.

y need to be determined through an engineering are no negative impacts to the resource. Any s should be implemented prior to construction, as

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acquisition impacts on the property should be cause a temporary impact to the property. The d to pre-construction conditions, resulting in impacts to the property.

pacted, a CHER is recommended to evaluate the p Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
<u>СНL #</u> ВНR 19	Residence	243 Garner Road East, Ancaster (former address 243	Designated under Part IV of	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may car landscape should be returned to minimal irreversible negative im As the subject property is design the lot is acquired, a CHIA is rec and assess mitigation measures impacts to the property's CHVI.
				within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	assessment to ensure there are resulting mitigation measures sh needed.

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

ignated under Part IV of the OHA, if a portion of recommended to consider alternative options res in order to avoid or reduce any negative /I.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 20	Residence	378 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East), and an Employment Supportive Centre is proposed for the southeast and southwest corners of Garner Road East and Highway 6. Encroachment and property acquisition is anticipated. Indirect: Measured from the road corridor, the structure on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 2 (Amended network Encroachment, property acquise property should be avoided. The the property. The landscape sho resulting in minimal irreversible of the Employment Supportive (If property is acquired and impa Supportive Centre, a CHER is r using Ontario Regulation 9/06. I and is proposed to be directly a recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sho needed.
BHR 21	Residence	581 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East), and an Employment Supportive Centre is proposed impacting the northeast corner of Garner Road East and Southcote Road. Encroachment and property acquisition is anticipated. Indirect: Measured from the road corridor, the structure on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. The the property. The landscape sho resulting in minimal irreversible of the Employment Supportive (If property is acquired and impa Supportive Centre, a CHER is r using Ontario Regulation 9/06. I and is proposed to be directly a recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl needed.

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isition and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property. Development e Centre should avoid the property.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 22	Residence	701 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.
				residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	
BHR 23	Residence	716 Garner Road East, Ancaster; Rymal House	Inventoried; LACAC Report Volume 5 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may n assessment to ensure there are resulting mitigation measures sh needed.



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required. Alternative 2 (Amended network
BHR 24	Residence	792 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located	Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.
				within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	assessment to ensure there are resulting mitigation measures sl needed.
				Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct:	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc
BHR 25	Residence	819 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening.	avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F
				Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	possess CHVI and is proposed is recommended.



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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 26	Residence	910 Garner Road	Inventoried; LACAC Report Volume 5 (n.d.)	Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Encroachment and property	Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im
		East, Ancaster		acquisition is expected as a result of the road widening.	If property is acquired and impa property's CHVI using Ontario F
				Indirect: Measured from the road corridor, the	possess CHVI and is proposed is recommended.
				residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl needed.
				Alternative 1 (Do nothing)	<u>Alternative 1 (Do nothing)</u> None required.
BHR 27	Commercial	944 Garner Road East, Ancaster; Bennett's Apple & Cider	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A new minor collector road is proposed crossing the subject property from east to west, through the orchards. Encroachment and property acquisition is expected as a result of the introduced road.	Alternative 2 (Amended network Encroachment and property acc avoided and the proposed mino property should be moved to a l AEGD TMP network. If property is acquired and the n recommended to evaluate the p If the property is determined to adversely impacted by the road

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

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reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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acquisition impacts on the property should be nor collector road traveling through the subject a less sensitive property, or removed from the

e minor collector road is introduced, a CHER is e property's CHVI using Ontario Regulation 9/06. o possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 28	Residential	992 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore,	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may n assessment to ensure there are
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 29	Residence	1021 Garner Road East, Ancaster; The Lampman Stone House	the OHA (By-Law No. 20- 244); Site Specific Policy Area A in UHOP, Vol 2, Map B.2.6- 1; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); CHIA (Detritus Consulting Ltd., 2014); LACAC Report Volume 3 (n.d.)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im As the subject property is desig the lot is acquired, a CHIA is re- and assess mitigation measures impacts to the property's CHVI. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl needed.
BHR 30	Residence	1157 Garner Road East, Ancaster	Inventoried; Glancaster Road EA CHR (AECOM, 2021); Previously Identified by the Ancaster Village Heritage Committee	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 31	Institutional	92 Glancaster Road, Ancaster; Hamilton District Christian High School	Inventoried (Places of Education)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A 4-lane minor arterial road is proposed adjacent to the east side of the subject property (Glancaster Road). Minor property acquisition is expected as a result of the road widening.	<u>Alternative 1 (Do nothing)</u> None required. <u>Alternative 2 (Amended network</u> Property acquisition impacts on work may cause a temporary im parallel to the road ROW. The la construction conditions, resulting the property.
BHR 32	Residence	549 Glancaster Road, Glanbrook		Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The subject property is situated on a lot east of Glancaster Road, as such it is not anticipated to be impacted by the 4-lane minor arterial road proposed for Glancaster Road.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
BHR 33	Farmscape	1640 Glancaster Road, Ancaster		Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Glancaster Road is proposed to remain as a rural road. Property acquisition is not anticipated as road widening is not expected.	<u>Alternative 1 (Do nothing) & Alte</u> None required.



<u>ork)</u> on the property should be avoided. The road impact to the grassed portion of the property e landscape should be returned to preting in minimal irreversible negative impacts to

Alternative 2 (Amended network)

<u> Alternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 34		9630 White Church Road West, Glanbrook	Inventoried; LACAC Report (1985); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south (White Church Road West) side of the subject property, and a new minor collector road is proposed parallel to a portion of the east side of the property. Encroachment and property acquisition is expected due to the road widening and newly introduced road. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended networ</u> Encroachment, property acquis should be avoided and the prop the subject property should be from the AEGD TMP network. If property is acquired and impa property's CHVI using Ontario I possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.
BHR 35	IResidence	9890 Airport Road West, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the south (Airport Road West) side of the subject property. Property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Property acquisition and vibration The road work may cause a ter wall). The landscape should be resulting in minimal irreversible If property is acquired and impa- property's CHVI using Ontario I possess CHVI and is proposed is recommended. The impacts of vibrations may re assessment to ensure there are resulting mitigation measures s needed.

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isition, and vibration impacts on the property oposed minor collector road traveling adjacent to e moved to a less sensitive property, or removed

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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ation impacts on the property should be avoided. emporary impact to the property (i.e., dry stone be returned to pre-construction conditions, le negative impacts to the property.

pacted, a CHER is recommended to evaluate the p Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
BHR 36	Residence	9174 Airport Road, Glanbrook	Inventoried; LACAC Report (1984)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end west of this property. As such, no impacts are anticipated.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 37	Residence	9142 Airport Road, Glanbrook	Inventoried; LACAC (1985)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end west of this property. As such, no impacts are anticipated.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 38	Residence	9132 Airport Road, Glanbrook	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end west of this property. As such, no impacts are anticipated.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 39	Residence	9046 Airport Road, Glanbrook	Identified in the field	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end west of this property. As such, no impacts are anticipated.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 40	Residence	9032 Airport Road, Glanbrook	Identified in the field	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end west of this property. As such, no impacts are anticipated.	Alternative 1 (Do nothing) & Alternative 2 (Amended network) None required.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	Alternative 1 (Do nothing) None required.
	Desideres	9188 Dickenson	EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south (Dickenson Road West) side of the subject property. Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 2 (Amended network Encroachment, property acquis should be avoided. The road we property. The landscape should resulting in minimal irreversible
BHR 41	Residence	Road West, Glanbrook			If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.
					The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl needed.
		9070 Dickenson idence Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
BHR 42	Residence			Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south (Dickenson Road West) side of the subject property. Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 2 (Amended network Encroachment, property acquisiti be avoided as the residence on t road work may cause a temporal be returned to pre-construction c negative impacts to the property. If property is acquired and impact
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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 43	Residence	9036 Dickenson Road West, Glanbrook	Previously Identified; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the south (Dickenson Road West) side of the subject property. Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquist should be avoided. The road we property. The landscape should resulting in minimal irreversible If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures should
BHR 44	Residence	2098 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. The the property. The landscape sho resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI an impacted by the road design, th The impacts of vibrations may n assessment to ensure there are resulting mitigation measures sho needed.



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL #		2104 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Anticipated ImpactAlternative 1 (Do nothing)No impacts are anticipated.Alternative 2 (Amended network)Direct:A major 6-lane arterial road is proposedadjacent to the east side of the lot (UpperJames Street). Encroachment and potentialproperty acquisition is expected as a resultof the road widening.Indirect:Measured from the road corridor, the	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment, property acquisi property should be avoided. The the property. The landscape sho
				residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 46	Residence	2112 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street), and an Employment Supportive Centre is proposed to potentially impact the south side of the property. Encroachment and potential property acquisition is anticipated. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.
BHR 47	Residence	2118 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street), and an Employment Supportive Centre is proposed to potentially impact the property. Encroachment and potential property acquisition is anticipated. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquis property should be avoided. The the property. The landscape she resulting in minimal irreversible Construction of the Employment and be moved slightly further so impacts to the subject property. If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, the The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 48	Residence	2129 Upper James Street, Glanbrook	Identified in the field	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potentia property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided. The road we property. The landscape should resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Ontal determined to possess CHVI ar impacted by the road design, th
BHR 49	Place of Worship	2149 Upper James Street, Glanbrook; Church on the Rock	Inventoried	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potentia property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is active the property's CHVI using Ontai determined to possess CHVI ar impacted by the road design, the The impacts of vibrations may re-

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
BHR 50	IResidence	2235 Upper James Street, Glanbrook	Registered Non-Designated; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potentia property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the

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isition, and vibration impacts on the subject s the residence on the lot has a very minimal k may cause a temporary impact to the property. Irned to pre-construction conditions, resulting in impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 51	Residence	2254 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street), and a new minor collector road is proposed parallel to the north property boundary, terminating at a T- intersection at Upper James Street. Encroachment and potential property acquisition is expected as a result of the road widening and newly introduced minor collector road. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment, property acquis property should be avoided. The the property. The landscape sho resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sho needed.
BHR 52	Residence	2264 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 53	Residence	2270 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 54	Residence	2272 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI ar impacted by the road design, th



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iisition, and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 55	Commercial	2273 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI ar impacted by the road design, th
BHR 56	Residence	2274 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI an impacted by the road design, th
BHR 57	Residence	19 Talbot Lane, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: No AEGD TMP Update road network changes are proposed to impact this property.	<u>Alternative 1 (Do nothing) & Alte</u> None required.



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acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 58	Residence	24 Talbot Lane, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: No AEGD TMP Update road network changes are proposed to impact this property.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
BHR 59	Residence	38 Talbot Lane, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: No AEGD TMP Update road network changes are proposed to impact this property.	<u>Alternative 1 (Do nothing) & Alte</u> None required.

<u> Alternative 2 (Amended network)</u>

<u> Alternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 60	Residence	2277 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 61	Residence	2280 Upper James Street, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 62	Residence	2284 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 63	Residence	2285 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. The the property. The landscape sho resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI an impacted by the road design, th



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acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 64	Residence	2288 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 65	Residence	2292 Upper James Street, Glanbrook; Former North Glanford Post Office	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is act the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 66	Residence	2294 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI an impacted by the road design, th



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 67	Residence	2300 Upper James Street, Glanbrook	West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); LACAC Report (1984);	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore,	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the
				the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 68	Residence	2310 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
BHR 69	Residence	2332 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI ar impacted by the road design, th



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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				· · ·	Alternative 1 (Do nothing)
			Inventoried; Twenty Road	Alternative 1 (Do nothing)	None required.
				No impacts are anticipated. Alternative 2 (Amended network)	Alternative 2 (Amended networ Encroachment and property ac
	Desidence	2340 Upper James	West Plan of Subdivision in	Direct:	should be avoided. The road we
BHR 70	Residence	Street, Glanbrook	Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit	A major 6-lane arterial road is proposed	property. The landscape should
			A-Line CHAR (ASI, 2012)	adjacent to the east side of the lot (Opper	resulting in minimal irreversible
				James Street). Encroachment and potential property acquisition is expected as a result of the read widening	the property's CHVI using Onta
				of the road widening.	determined to possess CHVI ar
					impacted by the road design, th
					<u>Alternative 1 (Do nothing)</u> None required.
	Residence	2492 Upper James Street, Glanbrook	Identified in the field	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	Alternative 2 (Amended network
				Alternative 2 (Amended network)	Encroachment and property ac
				Direct:	should be avoided. The road we
BHR 71				A major 6-lane arterial road is proposed	property. The landscape should
				adjacent to the east side of the lot (Upper	resulting in minimal irreversible
				James Street). Encroachment and potential	If a portion of the property is ac
				property acquisition is expected as a result	the property's CHVI using Onta
				of the road widening.	determined to possess CHVI ar
					impacted by the road design, th
					Alternative 1 (Do nothing)
				Alternative 1 (Do nothing)	None required.
				No impacts are anticipated.	Alternative 2 (Amended network
				Alternative 2 (Amended network)	Encroachment and property ac
		2502 Upper James		Alternative 2 (Amended network) Direct:	should be avoided. The road we
BHR 72	Residence	Street, Glanbrook	Identified in the field	A major 6-lane arterial road is proposed	property. The landscape should
				adjacent to the east side of the lot (Upper	resulting in minimal irreversible
				James Street). Encroachment and potential	If a portion of the property is ac
				property acquisition is expected as a result	the property's CHVI using Onta
				of the road widening.	determined to possess CHVI ar
					impacted by the road design, th

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acquisition impacts on the subject property work may cause a temporary impact to the ald be returned to pre-construction conditions, le negative impacts to the property.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
BHR 73	Residence	2624 Upper James Street, Glanbrook	Identified in the field	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potentia property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the The impacts of vibrations may n assessment to ensure there are
				vibration is anticipated.	resulting mitigation measures sh needed.



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acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing)	Alternative 1 (Do nothing)
				No impacts are anticipated.	None required.
				<u>Alternative 2 (Amended network)</u> Direct:	Alternative 2 (Amended network Encroachment, property acquis
				A major 6-lane arterial road is proposed	property should be avoided. The
				adjacent to the east side of the lot (Upper	the property. The landscape sho
		2724 Upper James		James Street). Encroachment and potential	resulting in minimal irreversible
BHR 74	Residential	Street, Glanbrook	Identified in the field	property acquisition is expected as a result	If a portion of the property is ac
		,		of the road widening.	the property's CHVI using Ontai
				Indirect:	determined to possess CHVI ar
				Measured from the road corridor, the	impacted by the road design, th
				residence on the subject property is located	The impacts of vibrations may r
				within the 40m vibration buffer. Therefore,	assessment to ensure there are
				the potential for indirect impacts due to	resulting mitigation measures sl
				vibration is anticipated.	needed.
		2910 Upper James Street, Glanbrook	Identified in the field		Alternative 1 (Do nothing)
				<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	None required.
				<u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed	Alternative 2 (Amended network
					Encroachment and property acc
					should be avoided. The road wo
BHR 75	Residence				property. The landscape should
				adjacent to the east side of the lot (Upper	resulting in minimal irreversible
				James Street). Encroachment and potential property acquisition is expected as a result	
				of the road widening.	the property's CHVI using Ontai
				or the road widening.	determined to possess CHVI ar
					impacted by the road design, th
				Alternative 1 (Do nothing) & Alternative 2 (Amended network)	
		2958 Homestead		No impacts anticipated at this time:	Alternative 1 (Do nothing) & Alternative 1 (Do nothing)
BHR 76	Residence	Drive, Glanbrook	Identified in the field	Homestead Drive is proposed to remain as	
		,		a 2-lane collector road within the AEGD	
				TMP Update.	
				Alternative 1 (Do nothing) & Alternative 2	
				(Amended network)	
BHR 77	Residence	2990 Homestead	Inventoried; LACAC Report	No impacts anticipated at this time:	Alternative 1 (Do nothing) & Alternative
		Drive, Glanbrook	(1984)	Homestead Drive is proposed to remain as	None required.
				a 2-lane collector road within the AEGD	
				TMP Update.	



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<u> Alternative 2 (Amended network)</u>

Iternative 2 (Amended network)

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
BHR 78	Residence	3000 Homestead Drive, Glanbrook	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 79	Residence	3002 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 80	Residence	3034 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 81	Place of Worship	3076 Homestead Drive, Glanbrook; Mount Hope United Church	Inventoried, Volume 7A; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 82	Commercial	3114 Homestead Drive, Glanbrook	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 83	Residence	3116 Homestead Drive, Glanbrook	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
BHR 84	Residence	3136 Homestead Drive, Glanbrook	Inventoried; LACAC Report (1984)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 85	Commercial	3154 Homestead Drive, Glanbrook	Identified in the field	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 87	Residence	2983 Homestead Drive	Inventoried		<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 88	Institutional	3027 Homestead Drive	Designated under Part IV of the OHA (By-Law No. 15- 169);		<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 89	Residence	3102/3110 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 90	Institutional	3091 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
BHR 91	Residence	3117 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 92	Residence	3127 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 93	Residence	3129 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 94	Residence	3141 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 95	Residence	3145 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.
BHR 96	Residence/Comm ercial	3155 Homestead Drive	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road within the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alternative 2 (Amended network)</u> None required.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 1	Farmscape	919 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the west side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence and barn on the subject property are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	rail fence), resulting in minimal i If a portion of the property is acc the property's CHVI using Ontal determined to possess CHVI ar impacted by the road design, th The impacts of vibrations may r
CHL 2	Farmscape	956 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment, property acquisi property should be avoided. If re be minimal. The road work may The landscape should be return minimal irreversible negative im If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI and impacted by the road design. the

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isition and vibration impacts on the subject f required, property acquisition is anticipated to ay cause a temporary impact to the property. Irned to pre-construction conditions (i.e., the split al irreversible negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

re no negative impacts to the resources. Any should be implemented prior to construction, as

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re no negative impacts to the resource. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 3	Farmstead	1394 Fiddlers Green Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Fiddlers Green Road). Minor encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	minimal irreversible negative im If a portion of the property is acc the property's CHVI using Ontal determined to possess CHVI ar impacted by the road design, th
CHL 4	Farmscape	1541 Fiddlers Green Road, Ancaster; Forest Lane Farms House; Russel Christie House	Designated under Part IV of the OHA (By-Law No. 84-55); LACAC Report Volume 3 (n.d.)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the west side of the lot (Fiddlers Green Road), and new minor collector roads are proposed to be introduced adjacent to the east and south property boundaries as well as cutting across the subject property from east to west. Encroachment and property acquisition is expected as a result of the introduction of new minor collector roads and the widening of Fiddlers Green Road.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networl Encroachment and property acc should be avoided and the prop property should be relocated to AEGD TMP network. As the subject property is desig the lot is acquired, a CHIA is re- and assess mitigation measures impacts to the property's CHVI. Note:

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isition and vibration impacts on the subject f required, property acquisition is anticipated to ay cause a temporary impact to the property. Irrned to pre-construction conditions, resulting in impacts to the property.

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rend to be determined through an engineering reno negative impacts to the resources. Any should be implemented prior to construction, as

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equisition impacts on the subject property oposed minor collector road bisecting the subject to a less sensitive property or removed from the

ignated under Part IV of the OHA, if a portion of recommended to consider alternative options res in order to avoid or reduce any negative /I.

is property have been severely damaged by fire. eritage Planning Staff should be had to nation will remain on the property or be removed.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 5	Farmscape	137 Carluke Road West, Ancaster	Registered Non-Designated	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the west side of the lot (Fiddlers Green Road). Minor property acquisition is expected as a result of the road widening resulting in impacts to only the west boundary of the fields on this large agricultural property. The built heritage resources that comprise the farmscape are located approximately 500 m from the Fiddlers Green Road ROW.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Property acquisition on the sub If required, property acquisition is built heritage resources that com a temporary impact to the proper construction conditions, resulting property.
CHL 6	Farmscape	37 Carluke Road East, Ancaster; Cranston Farms	Inventoried; LACAC Report Volume 6 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the west (Fiddlers Green Road) and south (Carluke Road East) sides of the property, and new minor collector roads are proposed to be introduced adjacent to the north and east property boundaries. Minor encroachment and property acquisition is expected as a result of the road widening and introduction of the minor collector roads.	

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bject property should be avoided.

n is anticipated to be minimal with no impacts to the omprise the farmscape. The road work may cause perty. The landscape should be returned to preng in minimal irreversible negative impacts to the

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cquisition impacts on the subject property should ty acquisition is anticipated to be minimal. The road mpact to the property. The landscape should be onditions, resulting in minimal irreversible negative

acquired and the minor collector road is mended to evaluate the property's CHVI using e property is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				• •	Alternative 1 (Do nothing)
			Inventoriad: LACAC Papart	Alternative 1 (Do nothing) No impacts are anticipated.	None required. <u>Alternative 2 (Amended network</u> Encroachment and property acqu
CHL 7	Farmscape	54 Carluke Road East, Ancaster; Hays House	Inventoried; LACAC Report Volume 6 (1985); City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the north side of the lot (Carluke Road East). Minor encroachment	be avoided. If required, property a work may cause a temporary imp returned to pre-construction conc impacts to the property.
				and potential property acquisition is expected as a result of the road widening.	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, th
			Inventoried; City of Hamilton AEGD Windshield Survey (2021)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 8	Farmscape	153 Carluke Road East, Ancaster; Cranston Farms		Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south side of the property (Carluke Road East) and a new minor collector road is proposed to be introduced adjacent to the west property boundary. Minor encroachment and potential property acquisition is expected as a result of the road widening and introduction of the minor collector road.	Ontario Regulation 9/06. If the p proposed to be directly adverse recommended.
CHL 9	Farmscape	189 Carluke Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the south side of the lot (Carluke Road East). Minor encroachment and potential property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided. If required, p minimal. The road work may can landscape should be returned to fence), resulting in minimal irrev If a portion of the property is acc the property's CHVI using Ontan determined to possess CHVI an impacted by the road design, th



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equisition impacts on the subject property should by acquisition is anticipated to be minimal. The road mpact to the property. The landscape should be nditions, resulting in minimal irreversible negative

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

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cquisition impacts on the subject property , property acquisition is anticipated to be cause a temporary impact to the property. The to pre-construction conditions, resulting in mpacts to the property.

acquired and the minor collector road is mended to evaluate the property's CHVI using property is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

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cquisition impacts on the subject property , property acquisition is anticipated to be cause a temporary impact to the property. The to pre-construction conditions (i.e. the split rail eversible negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 10	Farmscape	67 Butter Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> A minor 4-lane arterial road is proposed adjacent to the south side of the lot (Butter Road East). Minor encroachment and potential property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networl Encroachment and property acq be avoided. If required, property work may cause a temporary imp returned to pre-construction cond impacts to the property. If a portion of the property is ac introduced, a CHER is recomm Ontario Regulation 9/06. If the p proposed to be directly adverse recommended.
CHL 11	Farmscape	157 Butter Road East, Ancaster	Inventoried	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A new minor collector road is proposed adjacent to the west boundary of the property and Butter Road East is proposed to become a 4-lane arterial road. Minor encroachment and potential property acquisition is expected as a result of the introduction of the minor collector road and widening of Butter Road East.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Encroachment and property acq be avoided. If required, property work may cause a temporary imp returned to pre-construction cond impacts to the property. If a portion of the property is ac introduced, a CHER is recomm Ontario Regulation 9/06. If the p proposed to be directly adverse recommended.



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equisition impacts on the subject property should ty acquisition is anticipated to be minimal. The road mpact to the property. The landscape should be anditions, resulting in minimal irreversible negative

acquired and the 4-lane arterial road is mended to evaluate the property's CHVI using e property is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

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equisition impacts on the subject property should ty acquisition is anticipated to be minimal. The road mpact to the property. The landscape should be anditions, resulting in minimal irreversible negative

acquired and the minor collector road is mended to evaluate the property's CHVI using e property is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
					Alternative 1 (Do nothing)
CHL 12			Inventoried; City of Hamilton AEGD Windshield Survey (2021)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	None required. Alternative 2 (Amended network
	Farmscape	363 Butter Road East, Ancaster		Alternative 2 (Amended network) Direct: A new minor collector road is proposed traveling through a portion of the south half of the east side of property. Encroachment and potential property acquisition is expected as a result of the introduction of the minor collector road.	Encroachment and property acc should be avoided. If required, p minimal. The road work may ca landscape should be returned to minimal irreversible negative im If a portion of the property is acc introduced, a CHER is recommended.
CHL 13	Streetscape	Butter Road East between Highway 6 and Glancaster Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: One new minor collector road is proposed to intersect with this portion of the Butter Road East streetscape in a T-intersection approximately 250 m east of Airport Road, interrupting the existing configuration of the scenic rural corridor.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network The introduction of a new inters should be avoided. If the introduction of the minor c recommended that the rural cha rural cross-section. Where cons impacts and tree removal along landscaping with native tree spe impacts and restore the streets condition.
CHL 14	Streetscape	Southcote Road from Butter Road East to Airport Lands, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: One new minor collector road is proposed to intersect with this portion of the Southcote Road streetscape in a T- intersection approximately 500 m north of Butter Road East, interrupting the existing configuration of the scenic rural corridor.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network The introduction of a new inters should be avoided. If the introduction of the minor c recommended that the rural cha rural cross-section. Where cons impacts and tree removal along landscaping with native tree spe impacts and restore the streets condition.



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Icquisition impacts on the subject property I, property acquisition is anticipated to be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

icquired and the minor collector road is mended to evaluate the property's CHVI using property is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

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rsection to the Butter Road East streetscape

collector road cannot be avoided, it is haracter of Butter Road East be maintained to a nstruction is anticipated to result in grading ng the sides of the road ROW, post-construction pecies should be employed to mitigate visual scape as close as possible to an as-found

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rsection to the Southcote Road streetscape

r collector road cannot be avoided, it is haracter of Southcote Road be maintained to a nstruction is anticipated to result in grading ng the sides of the road ROW, post-construction species should be employed to mitigate visual tscape as close as possible to an as-found

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 15	Farmscape	167 Book Road East, Ancaster; Book House	Inventoried; Documentation & Salvage Report (Megan Hobson, 2018)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: A new minor collector road is proposed to pass through the agricultural field at the southwest corner of the subject property, and a minor 4-lane arterial road is proposed adjacent to the south side of the lot (Book Road East). The built heritage resources once located on this property were located a significant distance to the northeast and have been demolished. As such, no impacts are anticipated.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
CHL 16	Cemetery	281 Book Road East, Ancaster; Book Cemetery	Designated under Part IV of the OHA (By-Law No. 18-079)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The Book Cemetery is situated setback approximately 70 m from Book Road East on a significant rise in topography. Given this setback and the natural topography of the area, no impacts are anticipated to result from the minor 4-lane arterial road (Book Road East) proposed to the south of the cemetery.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
CHL 17	Cemetery	330 Book Road East; Ancaster Pet Cemetery	Identified in the field	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The Ancaster Pet Cemetery is situated setback slightly from Book Road East. Given this setback, no impacts are anticipated to result from the minor 4-lane arterial road (Book Road East) proposed to the north of the cemetery.	<u>Alternative 1 (Do nothing) & Alte</u> None required.

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BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 18	Farmscape	394 Book Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the north side of the lot (Book Road East) and a new 2-lane collector road is proposed through the north half of the property parallel to the east lot line. Property acquisition is expected as a result of the road widening and newly introduced road. A pond is located adjacent to the north property boundary and has the potential to be impacted by the Book Road East widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided and the prop the subject property should be t sensitive property, or removed f If property is acquired and the 2 recommended to evaluate the p If the property is determined to adversely impacted by the road
CHL 19	Farmscape	451 Book Road East, Ancaster	Registered Non-Designated	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south side of the lot (Book Road East), and a new 2-lane collector road is proposed traveling adjacent to the west property boundary before curving through the lot to the east, half way up the property. Property acquisition is expected as a result of the road widening and newly introduced 2-lane collector road.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided and the prop the subject property should be r or removed from the AEGD TMI If property is acquired and the 2 recommended to evaluate the p If the property is determined to adversely impacted by the road

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acquisition impacts on the subject property oposed 2-lane collector road traveling through e terminated before the property, moved to a less d from the AEGD TMP network.

e 2-lane collector road is introduced, a CHER is e property's CHVI using Ontario Regulation 9/06. o possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

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acquisition impacts on the subject property oposed 2-lane collector road traveling through e moved to a less sensitive property to the east, MP network.

e 2-lane collector road is introduced, a CHER is e property's CHVI using Ontario Regulation 9/06. o possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 20	Settlement	Southcote, Ancaster (approximate boundaries based on City of Hamilton Windshield Survey of concentration of pre-1950 heritage buildings)	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: Minor 4-lane arterial roads and a roundabout are proposed at the intersection of Book Road East and Southcote Road. Property acquisition, including the potential removal of nineteenth century structures, encroachment, and an alteration of the streetscape and historic road alignment is proposed.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Property acquisition, encroachn historic road alignment should b crossroads of the Southcote set location, potentially to the east, recommended that the rural cro Book Road East and Southcote If the minor 4-lane arterial roads recommended to evaluate the la 9/06. If the landscape is determ directly adversely impacted by t
CHL 21	Streetscape	Southcote Road from Book Road East to Airport Lands, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed for Southcote Road, south of Book Road East, a roundabout is proposed at the intersection of Book Road East and Southcote Road, and a new minor arterial road alignment is proposed at the southern end of the streetscape. Property acquisition, including the potential removal of nineteenth century structures, encroachment, and an alteration of the streetscape and historic road alignment is proposed.	historic road alignment should be crossroads of the Southcote set location, potentially to the east, recommended that the rural cro

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hment, and alteration of the streetscape and d be avoided and the proposed roundabout at the settlement should be moved to a less sensitive st, or removed from the AEGD TMP network. It is pross section and existing road alignment of the Road be maintained.

ds and roundabout are introduced, a CHER is a landscape's CHVI using Ontario Regulation mined to possess CHVI and is proposed to be y the road design, then a CHIA is recommended.

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hment, and alteration of the streetscape and d be avoided. The proposed roundabout at the settlement should be moved to a less sensitive st, or removed from the AEGD TMP network. It is cross section and existing road alignment of ed.

d, new minor arterial road alignment, and CHER is recommended to evaluate the rio Regulation 9/06. If the landscape is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 22	Farmscape	1224 Smith Road, Ancaster; Garfield Smith House	Inventoried; LACAC Report (1981); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Indirect: A 2-lane collector road is proposed adjacent to the east side of the property (Smith Road). Encroachment is not anticipated. Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Avoid any vibration impacts. The the property. The landscape sho resulting in minimal irreversible The impacts of vibrations may n assessment to ensure there are resulting mitigation measures sh needed.
CHL 23	Farmscape	931 Southcote Road, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road (Southcote Road) is proposed adjacent to the west property boundary, a 2-lane collector road is proposed adjacent to the west property boundary (Smith Road), and a new 2-lane collector road is proposed parallel to the north property boundary. Encroachment and property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment and property acc should be avoided. The road wo property. The landscape should resulting in minimal irreversible If a portion of the property is acc the property's CHVI using Ontai determined to possess CHVI an impacted by the road design, th

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The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

reed to be determined through an engineering re no negative impacts to the resources. Any should be implemented prior to construction, as

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acquisition impacts on the subject property work may cause a temporary impact to the ald be returned to pre-construction conditions, le negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 24	Place of Worship	572 & 582 Southcote Road, Ancaster; Netherland Reformed Church; Providence Canadian Reformed Church	Inventoried, Volume 7A (only 572 Southcote Road) Indi A m adja (Sou Sup sour Roa Enc exp Indi Mea stru loca The	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the east side of the lot (Southcote Road), and an Employment Supportive Centre is proposed for the southeast and southwest corners of Garner Road East and Southcote Road. Encroachment and property acquisition is expected for 572 Southcote Road. Indirect: Measured from the road corridor, the structures on the subject properties are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If property is acquired and impa Supportive Centre, a CHER is a using Ontario Regulation 9/06. and is proposed to be directly a recommended. The impacts of vibrations may a assessment to ensure there are
				Alternative 1 (Do nothing)	<u>Alternative 1 (Do nothing)</u> None required.
CHL 25	Cemetery	520 Southcote Road, Ancaster; Garner's Corners Cemetery	Inventoried	No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the cemetery (Garner Road East), and an Employment Supportive Centre is proposed impacting the northwest corner of Garner Road East and Southcote Road. Encroachment and property acquisition is possible.	Alternative 2 (Amended networl Encroachment and property acc An Employment Supportive Cer archaeological assessment will disturbance in the vicinity of the temporary impact to the propert construction conditions, resultin the property. If property is acquired and impa Supportive Centre, a CHER is r using Ontario Regulation 9/06. and is proposed to be directly a recommended.

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isition and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property. Development e Centre should avoid 572 Southcote Road.

pacted by the development of the Employment s recommended to evaluate the property's CHVI b. If the property is determined to possess CHVI adversely impacted, then a CHIA is

re no negative impacts to the resource. Any should be implemented prior to construction, as

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ecquisition on the cemetery should be avoided. The entre is not recommended on this property. An ill be required prior to commencing any ground the cemetery. The road work may cause a terty. The landscape should be returned to preting in minimal irreversible negative impacts to

pacted by the development of the Employment s recommended to evaluate the property's CHVI b. If the property is determined to possess CHVI r adversely impacted, then a CHIA is

Alternative 1 (Do nothing) None required. Alternative 2 (Amended network
Alternative 2 (Amended network
Allemative Z (Amendeu network
Encroachment and vibration imp
avoided. Given the residence's
arner and minor 4-lane arterial roads
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side Should it be determined that the
the road widening, encroachme
^o using Ontario Regulation 9/06. I
and is proposed to be directly a
Cultural Heritage Impact Assess
cated
ore, The impacts of vibrations may n
resulting mitigation measures sl needed.
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mpacts on the subject property should be 's minimal setback, most notably from Garner m), it is recommended that the proposed major is be accommodated within the existing right-of-

here is no other technically feasible solution for nent should be minimized. A Cultural Heritage recommended to evaluate the property's CHVI 5. If the property is determined to possess CHVI adversely impacted by the road design, then a essment (CHIA) is recommended.

re no negative impacts to the resource. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
			Inventoried, Volume 7A; Ancaster Elevated Water Reservoir Class EA CHRA (ASI, 2018); LACAC Report Volume 5 (Janet Wetmore, 1984)	Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 27	Place of Worship / Cemetery	99 Garner Road East, Ancaster; White Brick United Church and Cemetery; Fiddler's Green Road Cemetery		Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the south side of the church and cemetery (Garner Road East). Encroachment and property acquisition is possible. Indirect: Measured from the road corridor, the church on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 2 (Amended network Encroachment and property acc is not recommended given the p setback of the church and ceme archaeological assessment will disturbance in the vicinity of the If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures so
CHL 28	Farmscape	254 Garner Road East, Ancaster; Kendrick House	Inventoried; 140 Garner Road East, Hamilton HIA (MHBC, 2018); LACAC Report Volume 4 (Wanda Zsiros, 1983); City of Hamilton AEGD Windshield Survey (2021)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Property acquisition should be a temporary impact to the propert construction conditions, resultin the property. If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.

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ecquisition on the property should be avoided and e presence of a cemetery and the very minimal metery from the road ROW (under 10 m). An ill be required prior to commencing any ground the cemetery.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

re no negative impacts to the resources. Any should be implemented prior to construction, as

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e avoided. The road work may cause a erty. The landscape should be returned to preting in minimal irreversible negative impacts to

pacted, a CHER is recommended to evaluate the c Regulation 9/06. If the property is determined to d to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 29	Farmscape	474 Garner Road East, Ancaster	Identified in the field	Anticipated impact Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the north side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence and barn on the subject property are located within the 40m vibration buffer.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. The road work may car landscape should be returned to minimal irreversible negative im If property is acquired and impa- property's CHVI using Ontario R possess CHVI and is proposed is recommended. The impacts of vibrations may n assessment to ensure there are
				Therefore, the potential for indirect impacts due to vibration is anticipated.	resulting mitigation measures sh needed.

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

reed to be determined through an engineering re no negative impacts to the resources. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 30	Farmscape	832 Garner Road East, Ancaster	Inventoried; LACAC Report Volume 3 (n.d.)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A new minor collector road is proposed crossing the subject property from east to west, south of the collection of outbuildings Encroachment and property acquisition is expected as a result of the introduced road.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided and the proposed mino property should be moved to a AEGD TMP network. If property is acquired and the r recommended to evaluate the p If the property is determined to adversely impacted by the road
CHL 31	Place of Worship / Cemetery	876 & 880 Garner Road East, Ancaster; Bowman United Church and Cemetery; Bowman Chapel; The Old Slide; The Sliding Church	Inventoried, Volume 7A; LACAC Report (n.d.)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the north side of the church and cemetery (Garner Road East). Encroachment and property acquisition is possible. Indirect: Measured from the road corridor, the church on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc is not recommended given the p setback of the church and ceme archaeological assessment will disturbance in the vicinity of the If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.

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acquisition impacts on the property should be nor collector road traveling through the subject a less sensitive property, or removed from the

e minor collector road is introduced, a CHER is e property's CHVI using Ontario Regulation 9/06. o possess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

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acquisition on the property should be avoided and e presence of a cemetery and the very minimal metery from the road ROW (under 10 m). An ill be required prior to commencing any ground the cemetery.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

rend to be determined through an engineering reno negative impacts to the resources. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 32	Farmscape	927 Garner Road East, Ancaster	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A major 4-lane arterial road is proposed adjacent to the south side of the lot (Garner Road East). Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the barns on the subject property are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to	Alternative 2 (Amended networl Encroachment and property acc avoided. The road work may ca landscape should be returned to minimal irreversible negative im If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s
CHL 33	Park	3027 Homestead Drive, Glanbrook	Registered Non-Designated	vibration is anticipated. <u>Alternative 1 (Do nothing) & Alternative 2</u> <u>(Amended network)</u> No impacts anticipated at this time: Homestead Drive is proposed to remain as a 2-lane collector road and Upper James Street to the east is to remain a 4-lane arterial road within the AEGD TMP Update.	None required.
CHL 34	Farmscape	204 Glancaster Road, Ancaster	Inventoried; Glancaster Road EA CHR (AECOM, 2021); City of Hamilton AEGD Windshield Survey (2021)	No impacts are anticipated for this property Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The subject property is buffered by a hydro easement from the 4-lane minor arterial road proposed for Glancaster Road. As such, no impacts are anticipated.	Alternative 1 (Do nothing) & Alte

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acquisition impacts on the property should be cause a temporary impact to the property. The I to pre-construction conditions, resulting in impacts to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

reed to be determined through an engineering re no negative impacts to the resources. Any should be implemented prior to construction, as

Iternative 2 (Amended network)

<u> Alternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 35	Farmscape	1205 Glancaster Road, Glanbrook	Inventoried	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A new 4-lane minor arterial road is proposed parallel to the north boundary of the subject property. Encroachment and property acquisition is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided and the proposed 4-lar subject property should be mov from the AEGD TMP network. If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.
CHL 36	Farmscape	1292 Glancaster Road, Ancaster; Smith House	Inventoried; LACAC Report Volume 4 (Tina Agnello,1983); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Butter Road East and Southcote Road are proposed to remain as minor collector roads. Property acquisition is not anticipated as road widening is not expected.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
CHL 37	Farmscape	1731 Glancaster Road, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: Glancaster Road is proposed to remain as a minor collector road. Property acquisition is not anticipated as road widening is not expected.	<u>Alternative 1 (Do nothing) & Alta</u> None required.

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acquisition impacts on the property should be ane minor arterial road traveling adjacent to the oved to a less sensitive property, or removed

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

<u>Iternative 2 (Amended network)</u>

<u> Alternative 2 (Amended network)</u>

	İ		Anticipated Impact	Proposed Mitigation Measure
			Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
	2004 Glancaster Road, Ancaster; Braun Nursery Ltd.	Inventoried; LACAC Report Volume 4 (n.d.); City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A minor collector road is proposed adjacent to the east (Glancaster Road) side of the subject property and a minor 4-lane arterial road is proposed adjacent to the south (Carluke Road East) side. Minor property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore,	road work may cause a temporal should be returned to pre-constr irreversible negative impacts to If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may n assessment to ensure there are resulting mitigation measures sh
			vibration is anticipated.	needed. Alternative 1 (Do nothing)
Farmscane	3071 Glancaster Road, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north (Carluke Road East) side of the subject property. Minor property acquisition is expected as a result of the road widening.	None required. <u>Alternative 2 (Amended network</u> Property acquisition impacts on road work may cause a temporal should be returned to pre-construint irreversible negative impacts to If property is acquired and impact property's CHVI using Ontario Re- possess CHVI and is proposed to
•	White Church Road West (Glancaster Road to Highway 6)	Identified in the field	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: White Church Road West is proposed to be widened to a minor 4-lane arterial road,	recommended. <u>Alternative 1 (Do nothing)</u> None required. <u>Alternative 2 (Amended network</u> It is recommended that the rural the ROW, and existing alignmer If the minor 4-lane arterial road evaluate the landscape's CHVI
	Farmscape	FarmscapeRoad, Ancaster; Braun Nursery Ltd.Farmscape3071 Glancaster Road, GlanbrookStreetscapeWhite Church Road West (Glancaster	Farmscape 2004 Glancaster Road, Ancaster; Braun Nursery Ltd. Volume 4 (n.d.); City of Hamilton AEGD Windshield Survey (2021) Farmscape 3071 Glancaster Road, Glanbrook Inventoried; City of Hamilton AEGD Windshield Survey (2021) Farmscape 3071 Glancaster Road, Glanbrook Inventoried; City of Hamilton AEGD Windshield Survey (2021) Streetscape White Church Road West (Glancaster Identified in the field	Farmscape2004 Glancaster Road, Ancaster; Braun Nursery Ltd.Inventoried; LACAC Report Volume 4 (n.d.); City of Hamilton AEGD Windshield Survey (2021)to the east (Glancaster Road) side of the subject property and a minor 4-lane arterial road is proposed adjacent to the south (Carluke Road East) side. Minor property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.Farmscape3071 Glancaster Road, GlanbrookInventoried; City of Hamilton AEGD Windshield Survey (2021)Alternative 1 (Do nothing) Direct: A minor 4-lane arterial road is proposed adjacent to the soubject property. Minor property acquisition is expected as a result of the road wide ing.FarmscapeWhite Church Road West (Glancaster Road to Highway 6)Interfield in the fieldAlternative 1 (Do nothing) Direct: A Minor 4-lane arterial road is proposed adjacent to the north (Carluke Road East) side of the subject property. Minor property acquisition is expected as a result of the road widening.StreetscapeWhite Church Road West (Glancaster Road to Highway 6)Identified in the fieldAlternative 2 (Amended network) Direct: Minor 4-lane arterial road is proposed to be widening.



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on the property should be minor or avoided. The orary impact to the property. The landscape struction conditions, resulting in minimal to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

re no negative impacts to the resource. Any should be implemented prior to construction, as

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on the property should be minor or avoided. The orary impact to the property. The landscape istruction conditions, resulting in minimal to the property.

acted, a CHER is recommended to evaluate the Regulation 9/06. If the property is determined to to be directly adversely impacted, then a CHIA is

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ral cross section, topography, mature trees lining nent of White Church Road West be maintained.

Id is introduced, a CHER is recommended to /I using Ontario Regulation 9/06. If the pssess CHVI and is proposed to be directly ad design, then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 41	Farmscape	9779 White Church Road West, Glanbrook; Carpenter House	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north (White Church Road West) side of the subject property. Minor property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Property acquisition impacts on road work may cause a tempora should be returned to pre-const irreversible negative impacts to If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed
CHL 42	Farmscape	9490 White Church Road West, Glanbrook	Identified in the field	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the south (White Church Road West) side of the subject property, and new minor collector roads are proposed parallel to the east and west sides of the property as well as crossing it from east to west. Encroachment and property acquisition is expected as a result of the road widening and newly introduced minor collector roads Indirect: Measured from the road corridor, the buildings on the subject property are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	and across the subject lot shoul removed from the AEGD TMP r If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may n assessment to ensure there are resulting mitigation measures sh needed.

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on the property should be minor or avoided. The orary impact to the property. The landscape istruction conditions, resulting in minimal to the property.

pacted, a CHER is recommended to evaluate the Regulation 9/06. If the property is determined to d to be directly adversely impacted, then a CHIA

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isition, and vibration impacts on the property oposed minor collector road traveling adjacent to ould be moved to a less sensitive property, or P network.

pacted, a CHER is recommended to evaluate the Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

reed to be determined through an engineering re no negative impacts to the resources. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 43	Farmscape	9485 White Church Road West, Glanbrook; Smith- Pearce Farm	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north (White Church Road West) side of the subject property. Minor property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Property acquisition impacts on road work may cause a tempora should be returned to pre-const irreversible negative impacts to If property is acquired and impa- property's CHVI using Ontario F possess CHVI and is proposed is recommended.
CHL 44	Farmscape	9370 White Church Road West, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the south (White Church Road West) side of the subject property, and new minor collector roads are proposed parallel to the west side of the property, as well as crossing it from east to west before aligning south to terminate at White Church Road West. Encroachment and property acquisition is expected as a result of the road widening and newly introduced minor collector roads. Indirect: Measured from the road corridor, the buildings on the subject property are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	should be avoided and the prop and across the subject lot shoul removed from the AEGD TMP r If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures so needed.

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on the property should be minor or avoided. The orary impact to the property. The landscape istruction conditions, resulting in minimal to the property.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

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isition, and vibration impacts on the property oposed minor collector road traveling adjacent to ould be moved to a less sensitive property, or P network.

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

re no negative impacts to the resources. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 45	Farmscape	9349 White Church Road West, Glanbrook	Inventoried; City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: A minor 4-lane arterial road is proposed adjacent to the north (White Church Road West) side of the subject property. Encroachment and property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the building on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 2 (Amended network Encroachment, property acquis should be avoided. The road we property (i.e., wood fence). The construction conditions, resulting the property. If property is acquired and impar property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures s needed.
CHL 46	Streetscape	Airport Road West, south of airport lands, Glanbrook	Inventoried; Hamilton's Heritage Vol. 3; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: Airport Road West, south of the airport lands, is proposed to be widened from a 2- lane arterial/collector road to a minor 4-lane arterial road, resulting in an alteration of the streetscape.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networl This portion of Airport Road We streetscape CHL supporting con transportation complexes, inclue The widening of Airport Road We nature of the streetscape and th such, mitigation measures are r
CHL 47	Transportation	9300 Airport Road, Glanbrook; John C. Munro International Airport	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The property that comprises the John C. Munro International Airport is not anticipated to be impacted by any of the road network changes proposed through the AEGD TMP Update.	<u>Alternative 1 (Do nothing) & Alte</u> None required.

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isition, and vibration impacts on the property work may cause a temporary impact to the ne landscape should be returned to preting in minimal irreversible negative impacts to

pacted, a CHER is recommended to evaluate the Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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Vest is characterized as an evolved, dynamic contemporary institutional, commercial and public luding all airport-related lands and structures. West is consistent with the evolved, dynamic the growing capacity of the airport lands. As a not required.

<u>Iternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 48	Cemetery	Salem Cemetery (Salem Road within the boundaries of The John C. Munro Hamilton International Airport, Glanbrook)	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The Salem Cemetery is located entirely within the property that comprises the John C. Munro Hamilton International Airport. As such, it is not anticipated to be impacted by road network changes proposed through the AEGD TMP Update.	None required.
CHL 49	Institutional	9280 Airport Road, Glanbrook; Canadian War Plane Heritage Museum	Identified in the field	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The Canadian War Plane Heritage Museum is located adjacent to the property that comprises the John C. Munro International Airport and is not anticipated to be impacted by any of the road network changes proposed through the AEGD TMP Update.	None required.
CHL 50	Streetscape	Airport Road between East Cargo Road and Homestead Drive, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: The minor 4-lane arterial road proposed for Airport Road West is planned to end at the west side of this CHL. As such, no impacts are anticipated to the streetscape.	<u>Alternative 1 (Do nothing) & Alte</u> None required.
CHL 51	Farmscape	9867 Dickenson Road West, Glanbrook	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A new 4-lane minor arterial road is proposed parallel to the north boundary of the subject property, and a potential minor collector road is proposed to align traveling north-south through the property. Encroachment and property acquisition is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided and the potential minor north-south through the subject property, potentially parallel to the AEGD TMP network. If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.



<u> Alternative 2 (Amended network)</u>

<u> Alternative 2 (Amended network)</u>

<u>Iternative 2 (Amended network)</u>

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acquisition impacts on the property should be or collector road proposed to align traveling ct property should be moved to a less sensitive o the east side of the lot, or removed from the

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
			EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Documentation & Salvage Report (Megan Hobson, 2020); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 52		9236 Dickenson Road West, Glanbrook		Alternative 2 (Amended network) Direct: A new 4-lane collector and minor collector road is proposed passing through the northeast corner of the subject property, and a minor 4-lane arterial road is proposed adjacent to the south (Dickenson Road West) side of the subject property. Encroachment and property acquisition is anticipated.	Alternative 2 (Amended network Encroachment and property acc avoided and the new 4-lane coll passing through the northeast c to a less sensitive property or re A Documentation & Salvage Re the property, anticipating its der acquired and impacted, discuss Planners to determine if a CHEI
CHL 53		Road West, Glanbrook; North Glanford Cemetery; "Mud" Church	Inventoried; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 4-lane arterial road is proposed adjacent to the south side of the cemetery (Dickenson Road West). Encroachment and property acquisition is possible.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment and property acc is not recommended given the p from the ROW (5 m). An archae commencing any ground disturb If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended.
CHL 54	Farmscape	9751 Twenty Road West, Glanbrook	West Plan of Subdivision in Glanbrook CHIA (Golder,	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: The potential alignment of a 2-lane collector road is proposed parallel to the west property boundary, and a new 2-lane collector road is proposed parallel to the south property boundary. Encroachment and property acquisition is expected as a result of the introduction of new roads.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided and the roads proposed moved to a less sensitive prope The Twenty Road West Plan of confirmed to property to have C of the subject property is acquir with the City's Heritage Planner recommended.

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cquisition impacts on the property should be ollector and minor collector roads proposed corner of the subject property should be moved removed from the AEGD TMP network.

Report has been prepared for the residence on emolition. If a portion of the subject property is ssions should be had with the City's Heritage ER and/or CHIA is recommended.

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cquisition on the property should be avoided and e presence of a cemetery with a narrow setback aeological assessment will be required prior to urbance in the vicinity of the cemetery.

pacted, a CHER is recommended to evaluate the Regulation 9/06. If the property is determined to d to be directly adversely impacted, then a CHIA

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cquisition impacts on the property should be ed adjacent to the subject property should be perty or removed from the AEGD TMP network.

of Subdivision, Glanbrook CHIA (Golder, 2020) CHVI, and anticipated its demolition. If a portion lired and impacted, discussions should be had ers to determine if a project-specific CHIA is

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 55	Farmscape	9575 Twenty Road West, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: An Employment Supportive Centre is proposed on the property east of the subject property and has the potential to impact a small portion of the northeast corner of the subject property, resulting in potential encroachment and property acquisition.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc result of the development of the avoided. The construction of the temporary impact to the propert construction conditions, resulting the property. If plans for the Employment Sup property, a CHER is recommend Ontario Regulation 9/06. If the p

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Acquisition impacts on the subject property as a the Employment Supportive Centre should be the Employment Supportive Centre may cause a terty. The landscape should be returned to preting in minimal irreversible negative impacts to

Supportive Centre should shift further onto the ended to evaluate the property's CHVI using e property is determined to possess CHVI and is sely impacted, then a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 56	Harmscane	9511 Twenty Road West, Glanbrook	Plan of Industrial Subdivision in Glanbrook CHIA (Golder,	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: The alignment of a new 4-lane minor arterial road is proposed passing through the property from north to south, directly beside the farm house and barn. A 2-lane collector road is also proposed crossing the centre of the property from east to west. An Employment Supportive Centre is proposed on the north side of the lot, adjacent to Twenty Road West. Encroachment and property acquisition is anticipated.	(Golder, 2018) confirmed to pro
CHL 57		9445 Twenty Road West, Glanbrook; Marshall House; Rose Farm	Registered Non-Designated; Dickenson Road EA, Upper James Street to Glancaster Road CHR (ASI, 2019); Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Upper West Side Draft Plan of Industrial Subdivision in Glanbrook CHIA (Golder, 2018); City of Hamilton AEGD Windshield Survey (2021)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: The alignment of a new 2-lane collector road is proposed crossing the centre of the	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment and property acc avoided. The Employment Supp proposed passing through the s sensitive property, or removed f The Upper West Side Draft Plar (Golder, 2018) confirmed to pro- demolition. If the subject proper should be had with the City's He specific CHIA is recommended.

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cquisition impacts on the property should be pportive Centre and the roads proposed passing hould be moved to a less sensitive property or network.

lan of Industrial Subdivision, Glanbrook CHIA roperty to have CHVI, and anticipated its erty is acquired and impacted, discussions Heritage Planners to determine if a projected.

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cquisition impacts on the property should be pportive Centre, roundabout, and roads subject property should be moved to a less d from the AEGD TMP network.

an of Industrial Subdivision, Glanbrook CHIA roperty to have CHVI, and anticipated its erty is acquired and impacted, discussions Heritage Planners to determine if a projectd.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				<u>Alternative 1 (Do nothing)</u> No impacts are anticipated.	<u>Alternative 1 (Do nothing)</u> None required.
CHL 58	Farmscape	9285 Twenty Road West, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision, Glanbrook CHSR (Golder, 2019); Twenty Road West Plan of Subdivision in Glanbrook CHIA (Golder, 2020); City of Hamilton AEGD Windshield Survey (2021)	Alternative 2 (Amended network) Direct: The alignment of a new 2-lane collector road and potential alignment of a minor collector road are proposed passing through the property from north to south, and a new 4-lane collector road and minor collector road is proposed parallel to the south property boundary. Encroachment and property acquisition is expected as a result of the road introduction.	Alternative 2 (Amended networ Encroachment and property act avoided. The new roads propos subject property should be mov from the AEGD TMP network. The Twenty Road West Plan of confirmed to property to have C subject property is acquired and City's Heritage Planners to deter recommended.
CHL 59	Farmscape	1902 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A minor 4-lane arterial road is proposed adjacent to the north (Twenty Road West) side of the subject property, and a 4-lane collector road is proposed adjacent to the east (Upper James Street) side of the property. Minor property acquisition is expected along the south side of the lot as a result of the Twenty Road West widening	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Property acquisition impacts sh temporary impact to the propert construction conditions, resulting the property. All of the built heritage resource comprised the nineteenth centur portions of the subject property should be had with the City's Here recommended to evaluate the property is determined directly adversely impacted, if a

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acquisition impacts on the property should be osed passing through and adjacent to the oved to a less sensitive property or removed

of Subdivision, Glanbrook CHIA (Golder, 2020) CHVI, and anticipated its demolition. If the nd impacted, discussions should be had with the etermine if a project-specific CHIA is

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should be avoided. The road work may cause a erty. The landscape should be returned to preting in minimal irreversible negative impacts to

ces and many of the landscape elements that tury subject farmscape have been demolished. If ty are acquired and impacted, discussions Heritage Planners to determine if: a CHER is e property's CHVI using Ontario Regulation 9/06; ned to possess CHVI and is proposed to be f a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 60	Farmscape	2028 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is act the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
CHL 61	Streetscape	2072-2084 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the streetscape (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residences that comprise the streetscape are located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. <u>Alternative 2 (Amended network</u> Encroachment, property acquisis streetscape should be avoided. to the properties. The landscape conditions, resulting in minimal streetscape. If a portion of the properties are evaluate the landscape's CHVI is determined to possess CHVI impacted by the road design, th The impacts of vibrations may r assessment to ensure there are resulting mitigation measures sl needed.

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isition and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

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isition and vibration impacts on the subject d. The road work may cause temporary impacts ape should be returned to pre-construction al irreversible negative impacts to the

re acquired, a CHER is recommended to /I using Ontario Regulation 9/06. If the property /I and is proposed to be directly adversely then a CHIA is recommended.

reed to be determined through an engineering re no negative impacts to the resource. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
CHL 62	Farmscape	2081 Upper James Street, Glanbrook; Stoneholm Farm	Registered Non-Designated; LACAC Report (1984); Hamilton Rapid Transit A-Line	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is acc the property's CHVI using Ontar determined to possess CHVI an impacted by the road design, the

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isition and vibration impacts on the subject The road work may cause a temporary impact to wrought iron fence and gate). The landscape struction conditions, resulting in minimal to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

re no negative impacts to the resource. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 63	Estate	2173 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening.	If a portion of the property is ac- the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th
CHL 64	Farmscape	2219 Upper James Street, Glanbrook	Inventoried	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	If a portion of the property is ac the property's CHVI using Onta determined to possess CHVI ar impacted by the road design, th

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acquisition impacts on the subject property work may cause a temporary impact to the es). The landscape should be returned to preting in minimal irreversible negative impacts to

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

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iisition, and vibration impacts on the subject The road work may cause a temporary impact to should be returned to pre-construction conditions, le negative impacts to the property.

acquired, a CHER is recommended to evaluate tario Regulation 9/06. If the property is and is proposed to be directly adversely then a CHIA is recommended.

rend to be determined through an engineering reno negative impacts to the resource. Any should be implemented prior to construction, as

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 65	Farmscape	2240 Upper James Street, Glanbrook	Inventoried; Twenty Road West Plan of Subdivision in Glanbrook CHSR (Golder, 2019); Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Direct: A major 6-lane arterial road is proposed adjacent to the east side of the lot (Upper James Street) and a new minor collector road is proposed crossing the subject property, entering in the northwest corner and terminating at a T-intersection on the west side of the lot at Upper James Street. The potential alignment of a collector road is also noted parallel to the north property boundary. Encroachment and potential property acquisition is anticipated. Indirect: Measured from the road corridor, the residence on the subject property is located within the 40m vibration buffer. Therefore, the potential for indirect impacts due to vibration is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended networ Encroachment, property acquis property should be avoided and to the subject property should b removed from the AEGD TMP i All of the built heritage resource comprised the nineteenth centu Only a vacant, post-war bungal are acquired and impacted, dise Planners to determine if: a CHE CHVI using Ontario Regulation possess CHVI and is proposed recommended.
CHL 66	Streetscape	Talbot Lane west of Upper James Street, Glanbrook	Inventoried	Alternative 1 (Do nothing) & Alternative 2 (Amended network) No impacts anticipated at this time: No AEGD TMP Update road network changes are proposed to impact this streetscape.	<u>Alternative 1 (Do nothing) & Alta</u> None required.

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isition, and vibration impacts on the subject nd the new roads proposed within and adjacent be moved to a less sensitive property, or P network.

rces and many of the landscape elements that ntury subject farmscape have been demolished. alow remains. If portions of the subject property iscussions should be had with the City's Heritage HER is recommended to evaluate the property's on 9/06; and if the property is determined to ed to be directly adversely impacted, if a CHIA is

<u>Iternative 2 (Amended network)</u>

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 67	Streetscape	2273-2285 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the streetscape (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided. The road wo properties. The landscape shou resulting in minimal irreversible If a portion of the properties that CHER is recommended to evalu Regulation 9/06. If the landscap proposed to be directly adverse recommended.
CHL 68	Streetscape	2332 and 2340 Upper James Street between Talbot Lane and Dickenson Road, Glanbrook	Inventoried	Alternative 1 (Do nothing) No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the east side of the streetscape (Upper James Street). Encroachment and potential property acquisition is expected as a result of the road widening.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided. The road wo properties. The landscape shou resulting in minimal irreversible If a portion of the properties that CHER is recommended to evalu Regulation 9/06. If the landscap proposed to be directly adverse recommended.



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acquisition impacts on the subject streetscape work may cause a temporary impact to the ould be returned to pre-construction conditions, le negative impacts to the properties.

nat comprise the streetscape are acquired, a aluate the streetscape's CHVI using Ontario ape is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

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acquisition impacts on the subject streetscape work may cause a temporary impact to the ould be returned to pre-construction conditions, le negative impacts to the properties.

nat comprise the streetscape are acquired, a aluate the streetscape's CHVI using Ontario ape is determined to possess CHVI and is sely impacted by the road design, then a CHIA is

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
CHL 69	Settlement	North Glanford (1875 Illustrated Historical Atlas of the County of Wentworth boundaries)	Inventoried	Alternative 1 (Do nothing)No impacts are anticipated.Alternative 2 (Amended network)Direct:At the crossroads of the nineteenth centurysettlement of North Glanford, a minor 4-lanearterial road is proposed for DickensonRoad West, intersecting with Upper JamesStreet, a proposed major 6-lane arterialroad. Property acquisition, includingpotential encroachment on a nineteenthcentury structure and cemetery, and analteration of the streetscape is proposedprimarily along Dickenson Road West.	<u>Alternative 1 (Do nothing)</u> None required. <u>Alternative 2 (Amended networl</u> Property acquisition, encroachn be avoided. It is recommended West be maintained.
CHL 70	Farmscape	2543 Upper James Street, Glanbrook	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc should be avoided. All of the built heritage resource comprised the subject farmscap of a silo and small contemporar property are acquired and impa Heritage Planners to determine property's CHVI using Ontario F determined to possess CHVI ar impacted, if a CHIA is recomme

<u>ork)</u> hment, and alteration of the streetscape should ed that the rural cross section of Dickenson Road

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ces and many of the landscape elements that ape have been demolished aside from the ruins ary utilitarian structures. If portions of the subject bacted, discussions should be had with the City's he if: a CHER is recommended to evaluate the b Regulation 9/06; and if the property is and is proposed to be directly adversely mended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measure
				<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct:	<u>Alternative 1 (Do nothing)</u> None required. <u>Alternative 2 (Amended network</u> Encroachment and property acc is not recommended given the
CHL 71	Place of Worship / Cemetery	2865 & 2869 Upper James Street, Glanbrook; St. Paul's Anglican Church & Cemetery	Inventoried, Volume 7A; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	A major 6-lane arterial road is proposed adjacent to the west side of the church and cemetery (Upper James Street).	setback from the ROW (under 1 required prior to commencing a cemetery. If property is acquired and impa property's CHVI using Ontario F possess CHVI and is proposed is recommended. The impacts of vibrations may r assessment to ensure there are resulting mitigation measures so
CHL 72	Adricultural	2907 Upper James Street, Glanbrook	Inventoried; LACAC 1984	<u>Alternative 1 (Do nothing)</u> No impacts are anticipated. <u>Alternative 2 (Amended network)</u> Direct: A major 6-lane arterial road is proposed adjacent to the west side of the lot (Upper James Street). Encroachment and potential property acquisition is anticipated.	Alternative 1 (Do nothing) None required. Alternative 2 (Amended network Encroachment and property acc avoided. All of the built heritage resource once comprised the nineteenth portions of the subject property should be had with the City's He recommended to evaluate the p and if the property is determine directly adversely impacted, if a

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equisition on the property should be avoided and e presence of a cemetery and its very minimal r 10 m). An archaeological assessment will be any ground disturbance in the vicinity of the

pacted, a CHER is recommended to evaluate the o Regulation 9/06. If the property is determined to ed to be directly adversely impacted, then a CHIA

re no negative impacts to the resources. Any should be implemented prior to construction, as

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ecquisition impacts on the property should be

ces and many of the landscape elements that th century farmscape have been demolished. If ty are acquired and impacted, discussions Heritage Planners to determine if: a CHER is property's CHVI using Ontario Regulation 9/06; hed to possess CHVI and is proposed to be f a CHIA is recommended.

BHR or CHL #	Type of Property	Location	Heritage Recognition	Type and Description of Potential / Anticipated Impact	Proposed Mitigation Measures
CHL 73	Settlement	Mount Hope (Homestead Drive)	Inventoried; Hamilton Rapid Transit A-Line CHAR (ASI, 2012)	Alternative 1 (Do nothing) No impacts are anticipated. Alternative 2 (Amended network) Indirect: The settlement of Mount Hope is focused on both sides of Homestead Drive, which is proposed within the AEGD TMP network as a 2-lane collector road. The existing road is currently configured as a 2-lane collector road constructed to a rural cross-section with narrow gravel shoulders, allowing little room for widening. The introduction of a rural cross-section would indirectly impact the evolved, rural, nineteenth-century landscape of the village.	Homestead Drive ROW be main If the minor 2-lane collector road cross section, a CHER is recom

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ral cross section and mature trees lining the aintained.

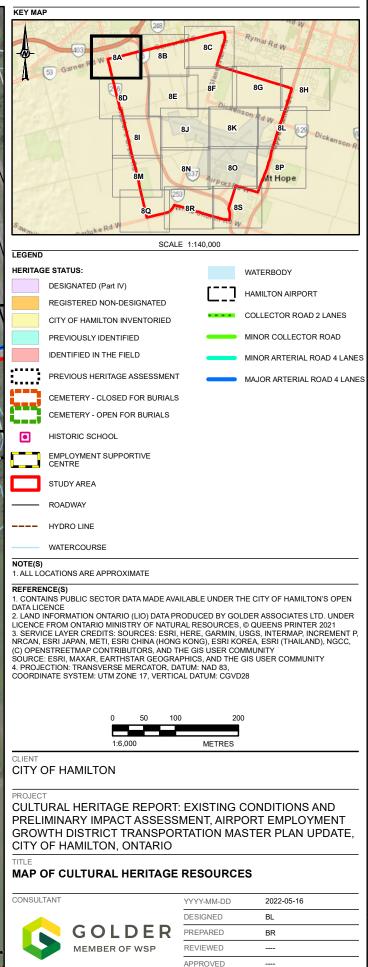
bad is to be widened or constructed to an urban ommended to evaluate the landscape's CHVI 6. If the landscape is determined to possess lirectly adversely impacted by the road design,

APPENDIX D

Cultural Heritage Resources Mapping







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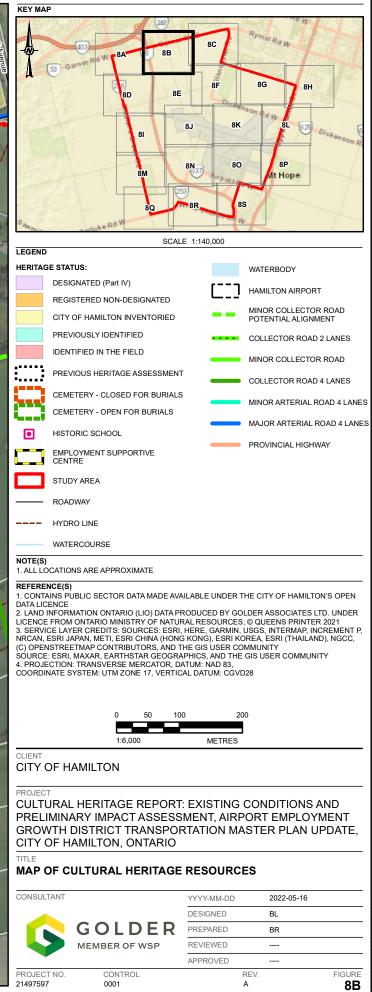
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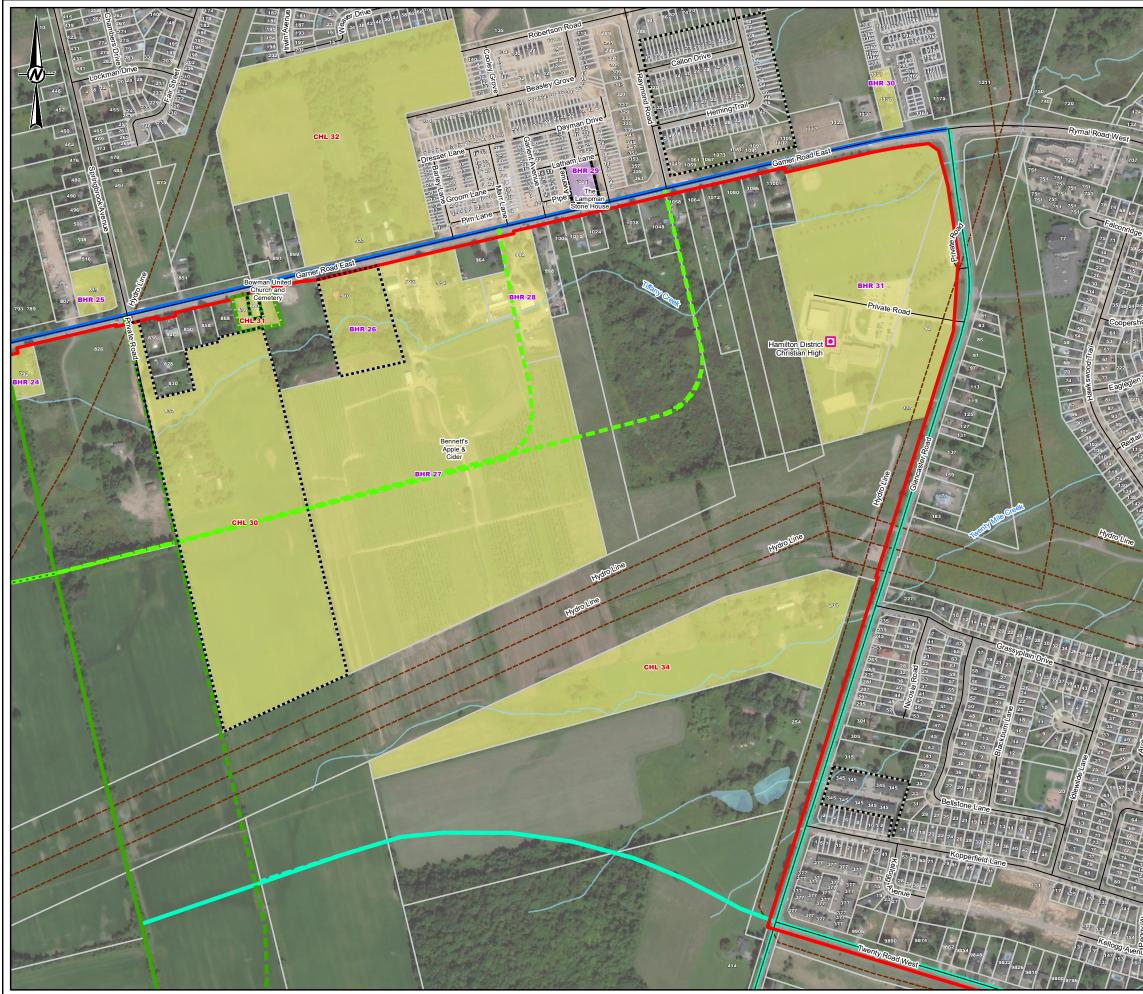
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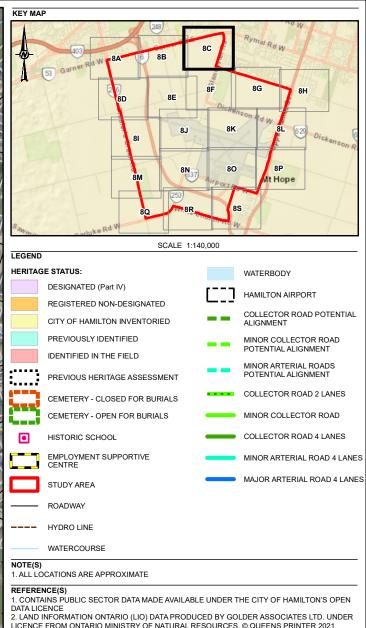




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PROJECT CULTURAL HERITAGE REPORT: EXISTING CONDITIONS AND PRELIMINARY IMPACT ASSESSMENT, AIRPORT EMPLOYMENT GROWTH DISTRICT TRANSPORTATION MASTER PLAN UPDATE, CITY OF HAMILTON, ONTARIO TITLE

MAP OF CULTURAL HERITAGE RESOURCES

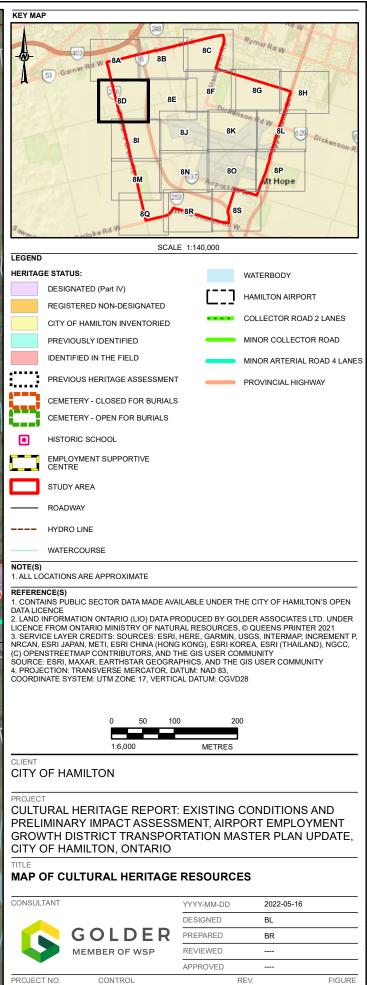
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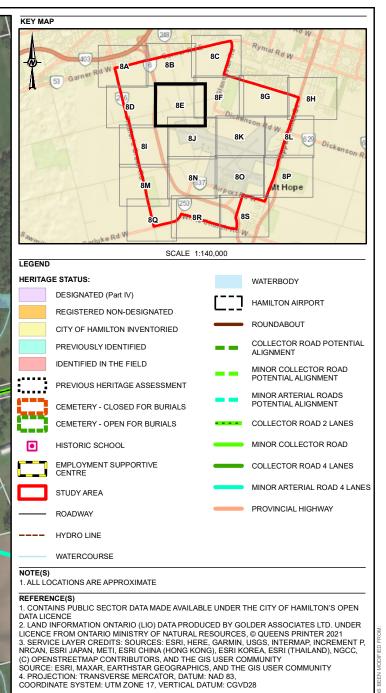
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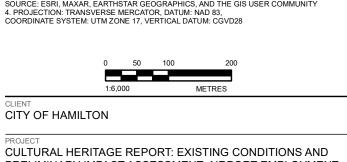
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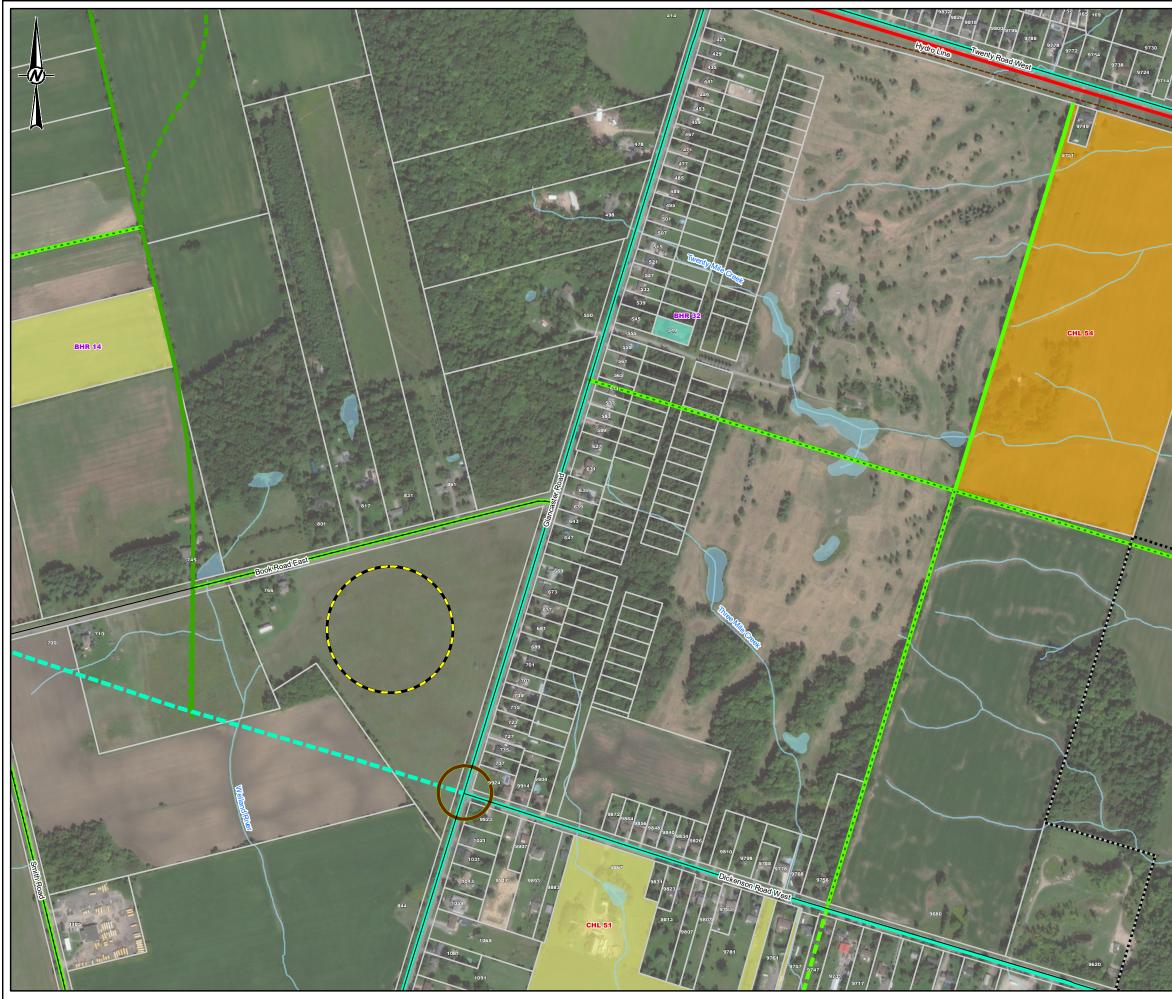
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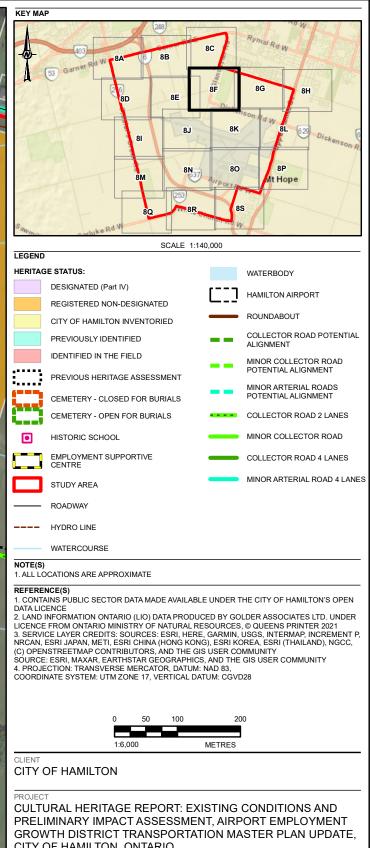
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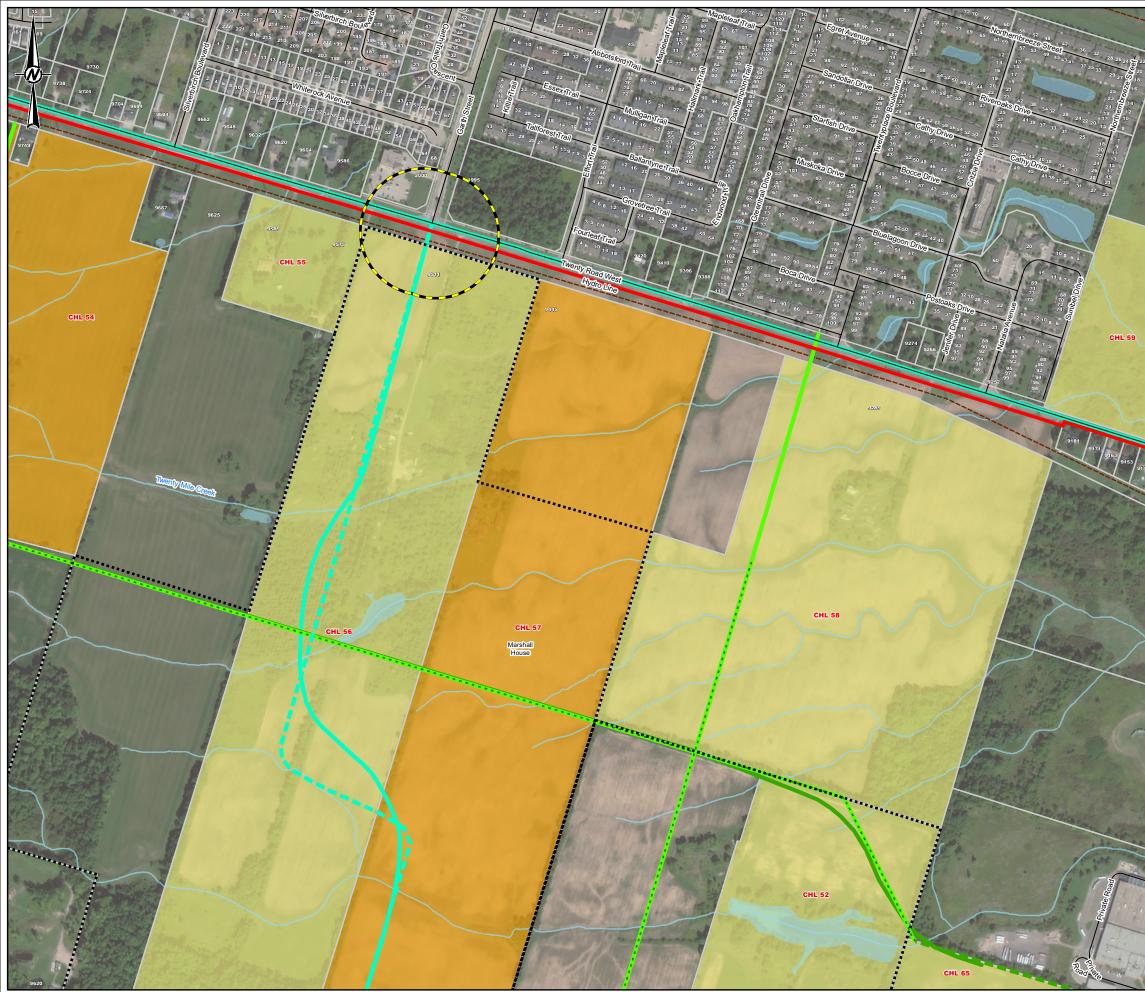
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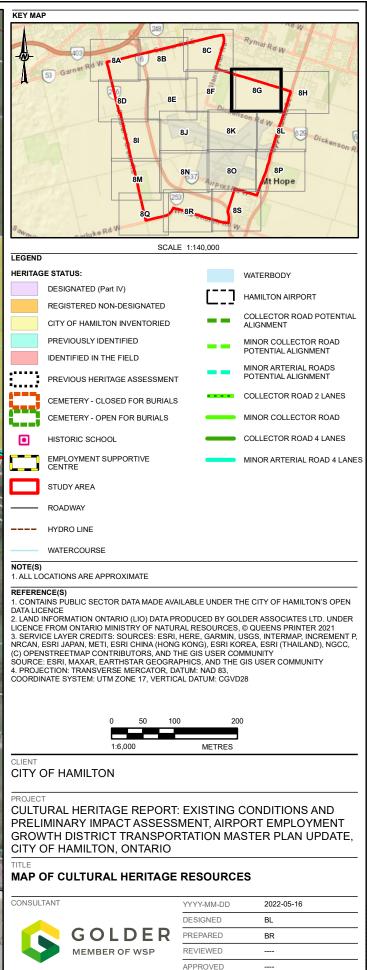
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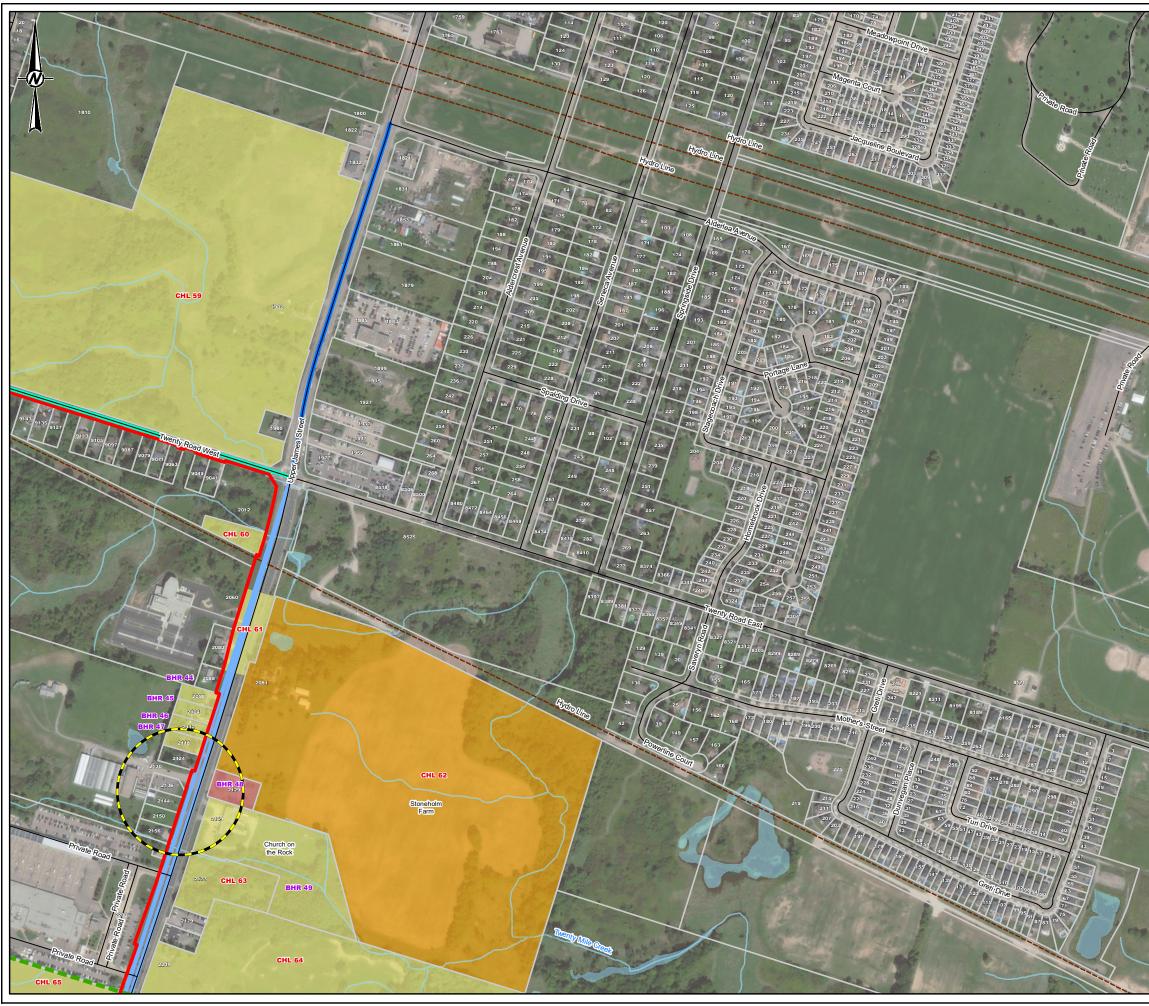


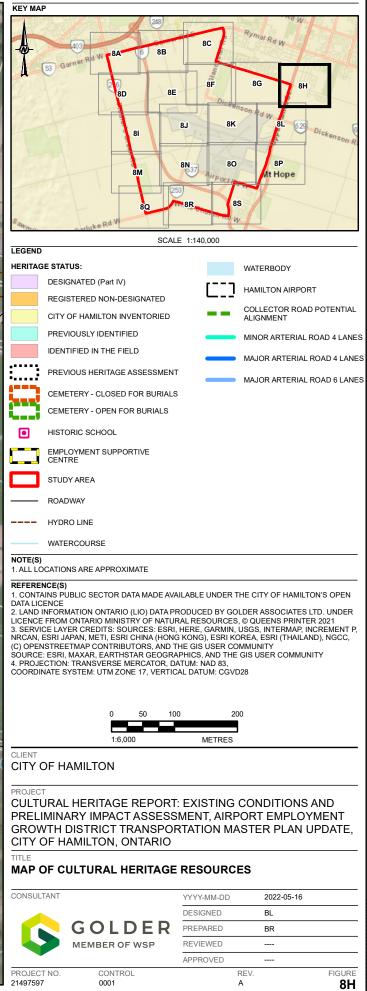
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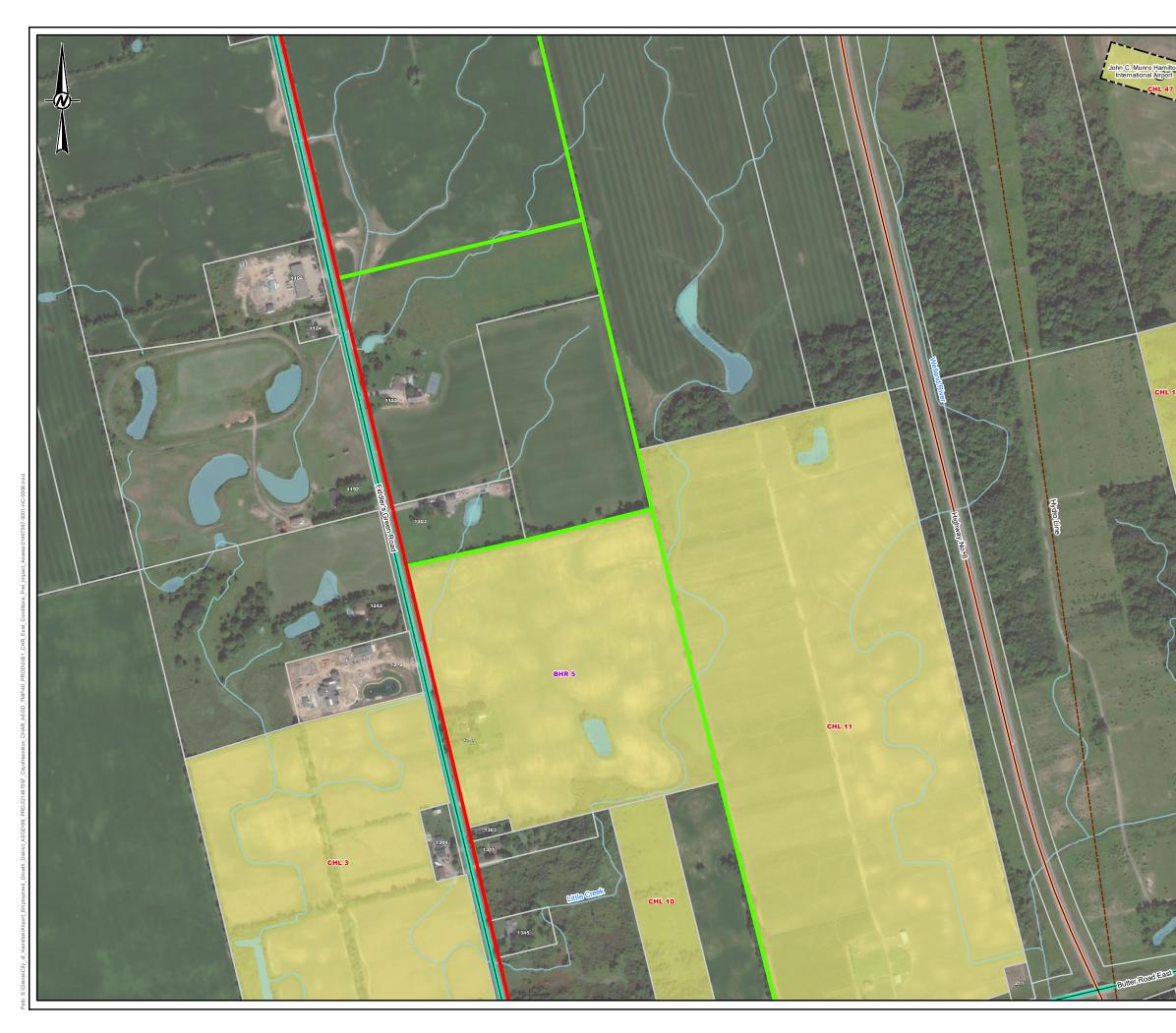
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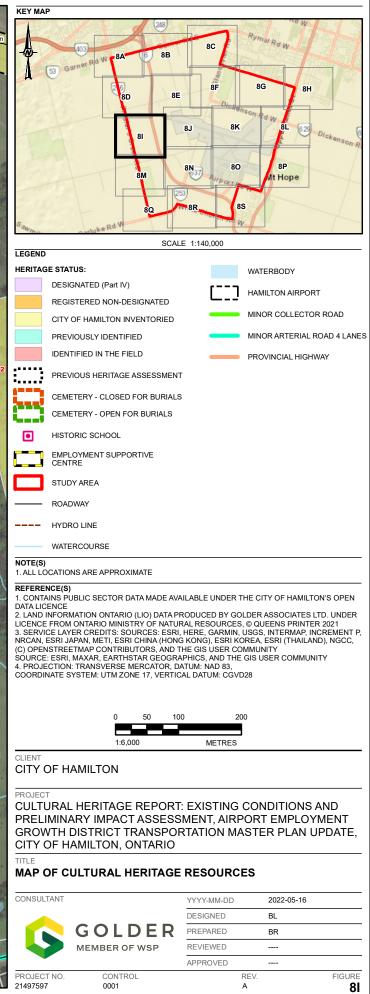
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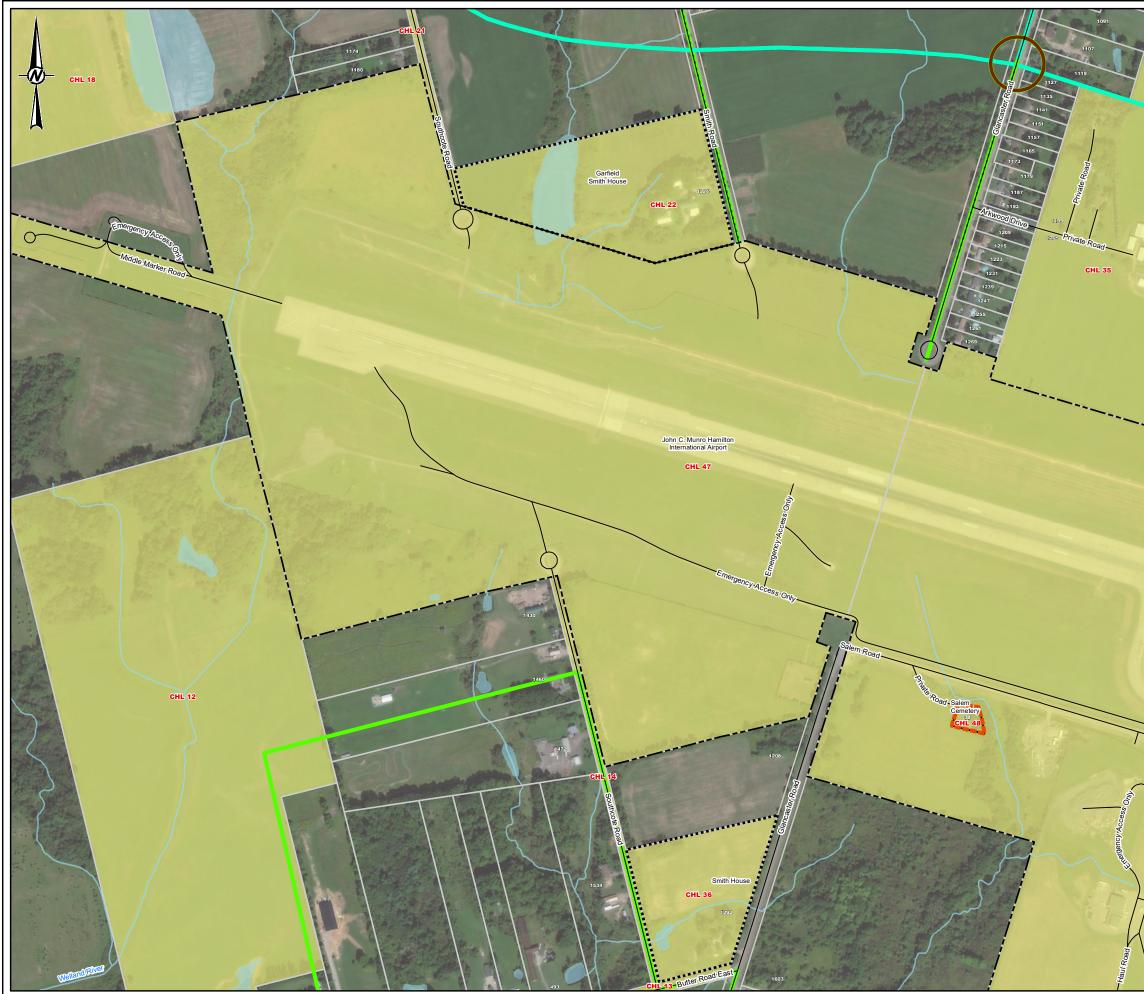
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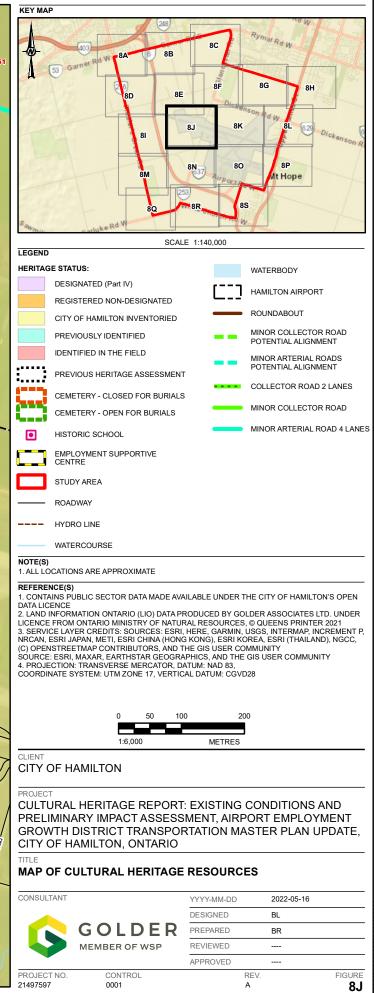




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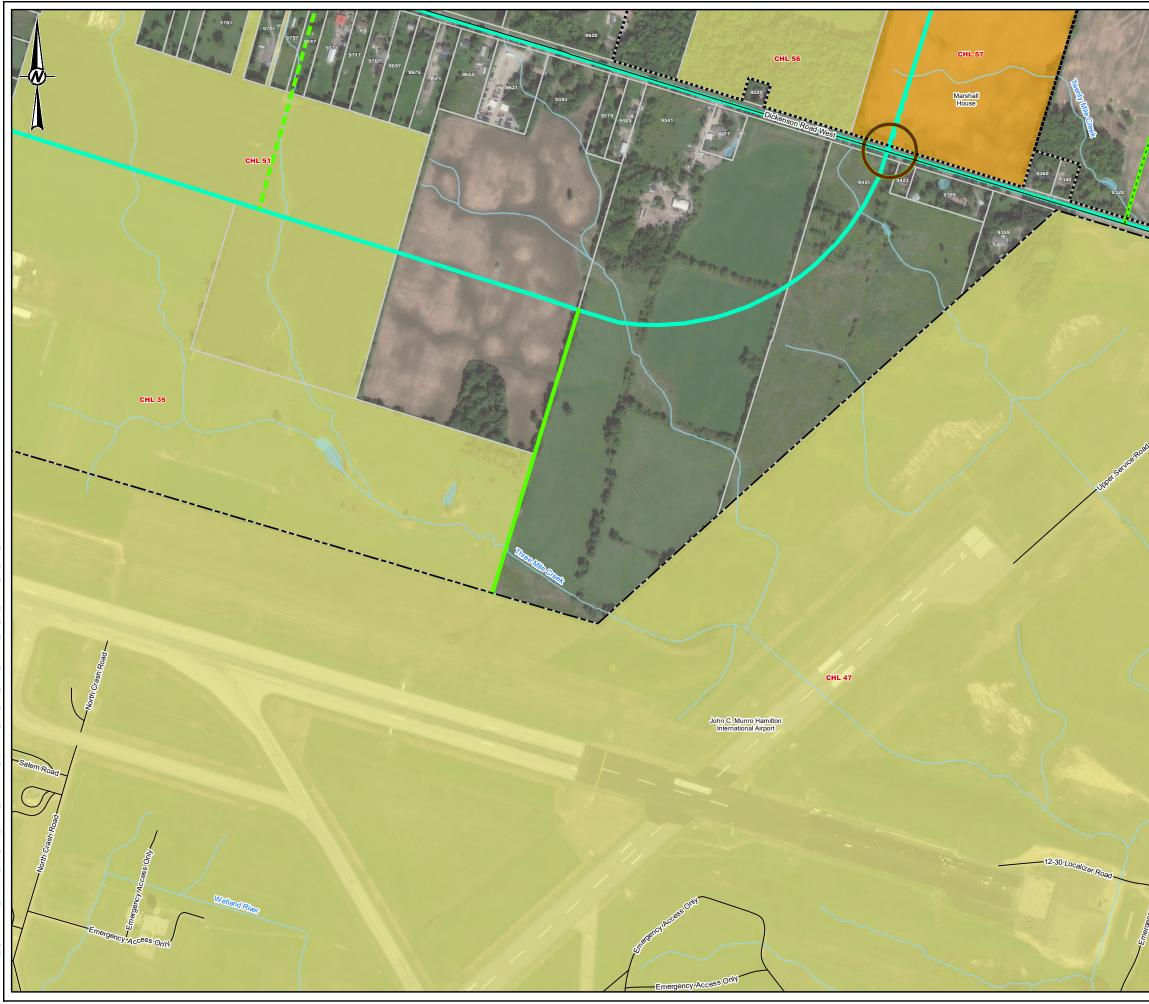
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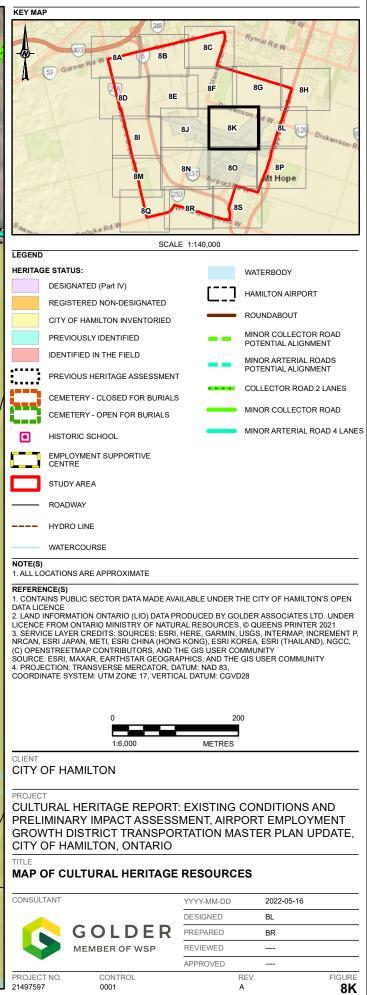




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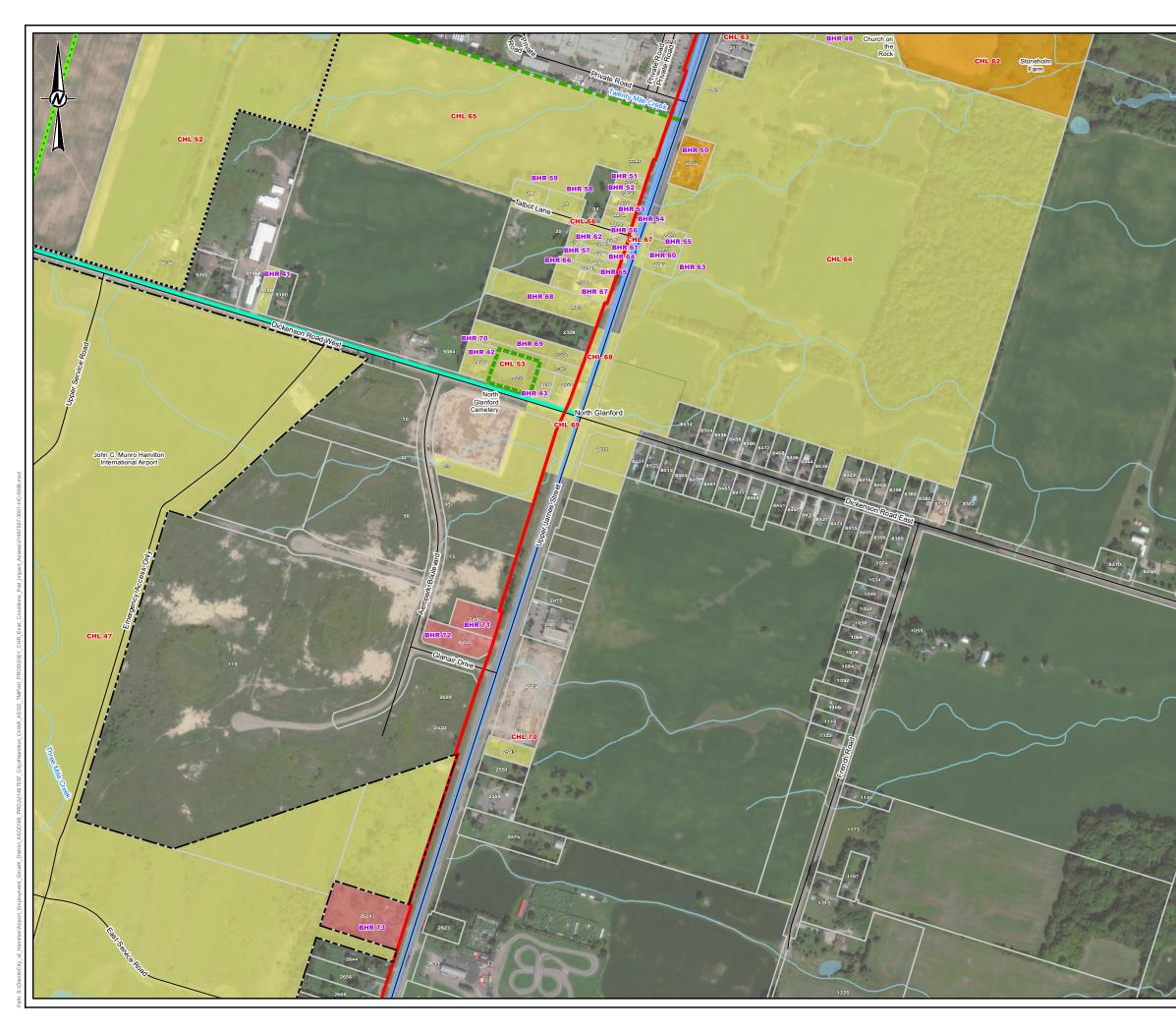
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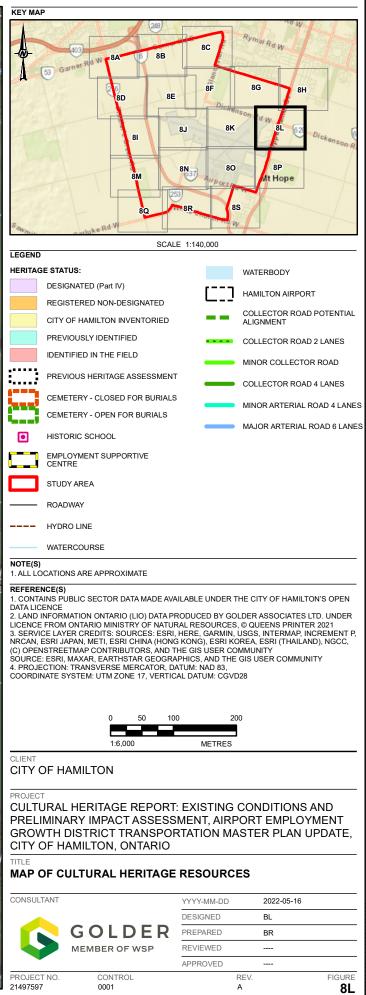




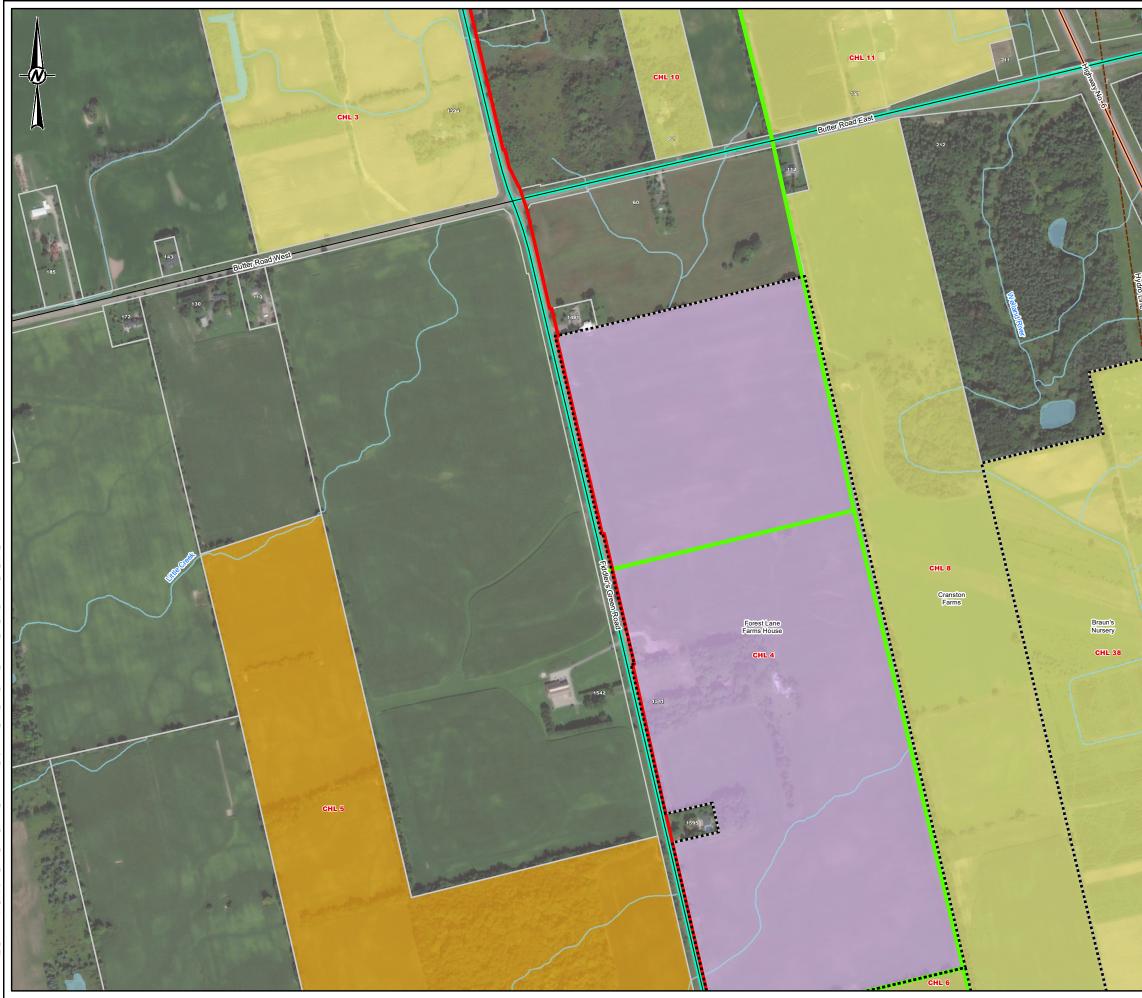
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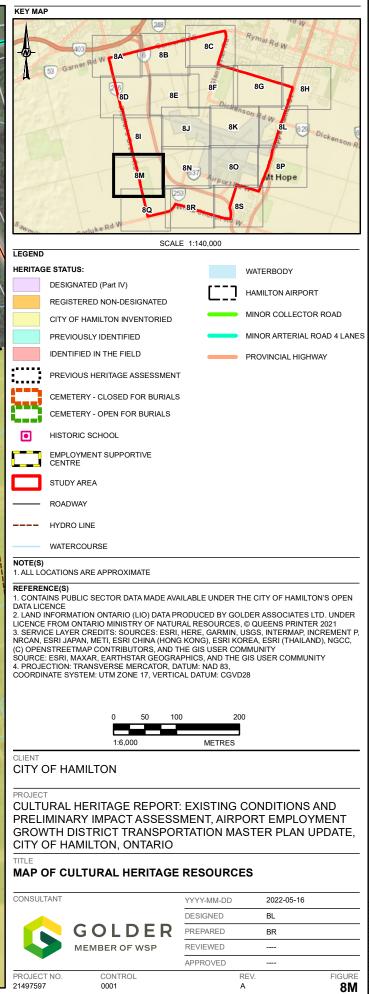
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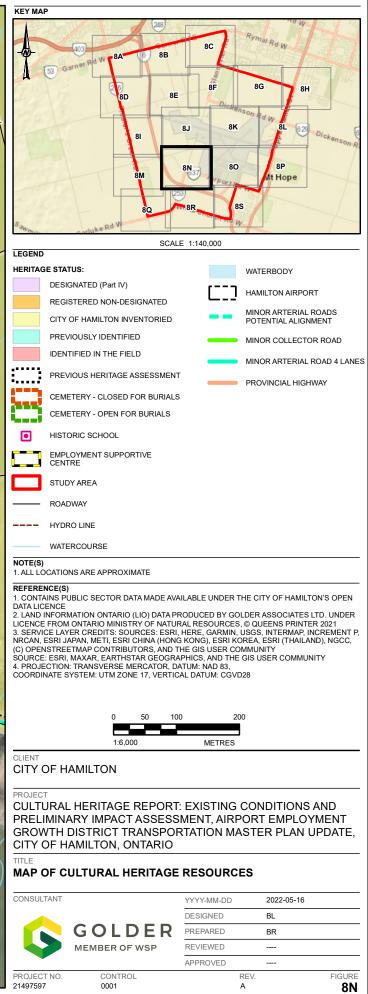




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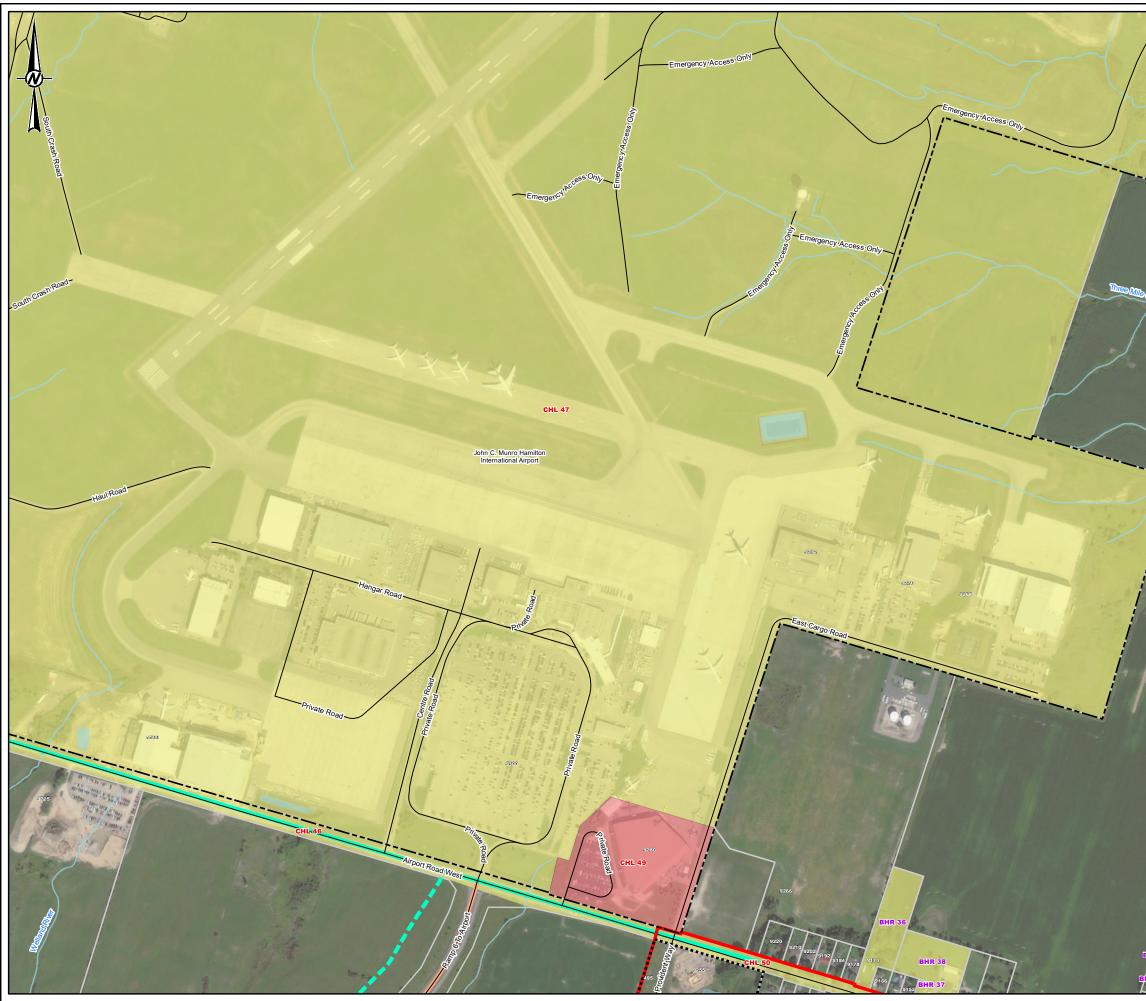
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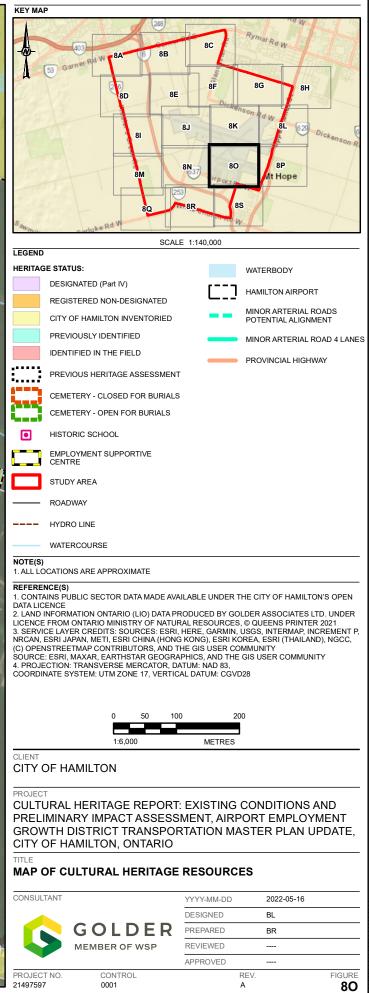


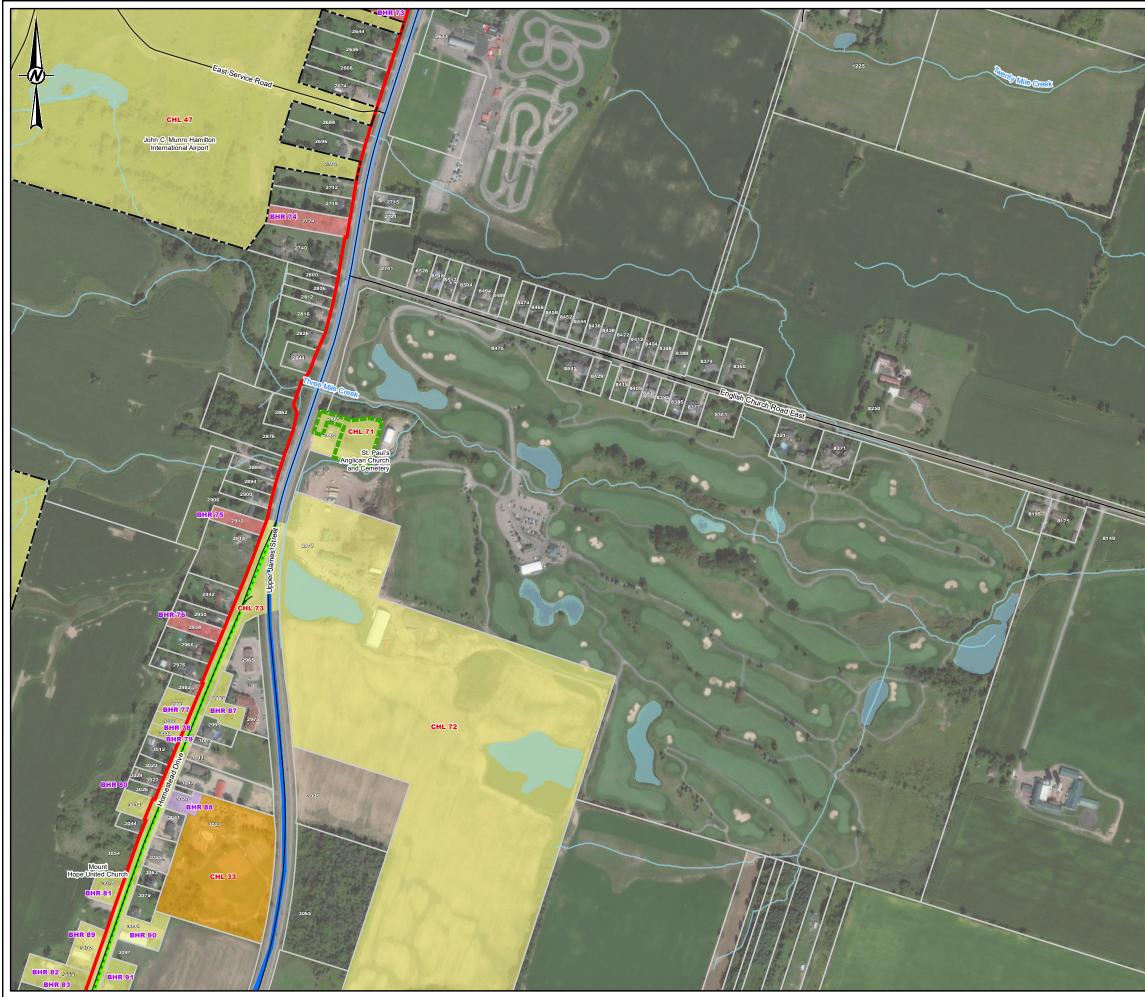


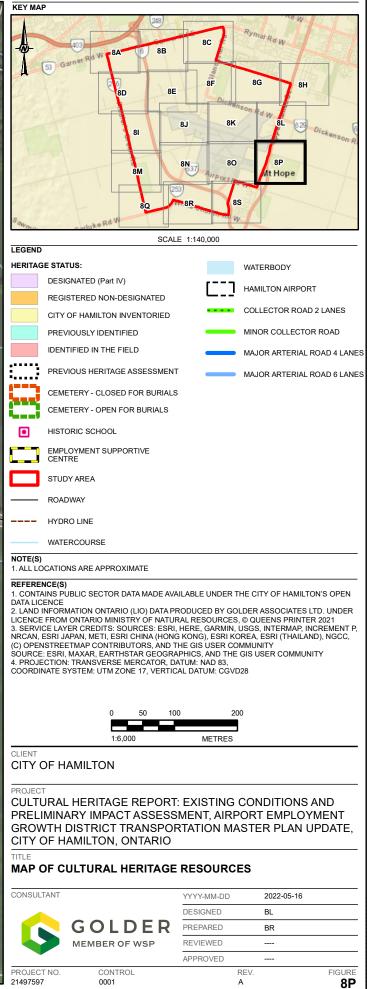
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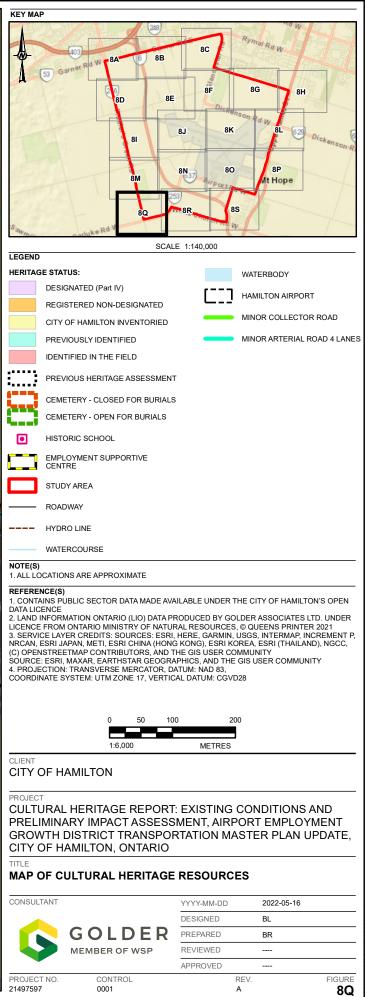




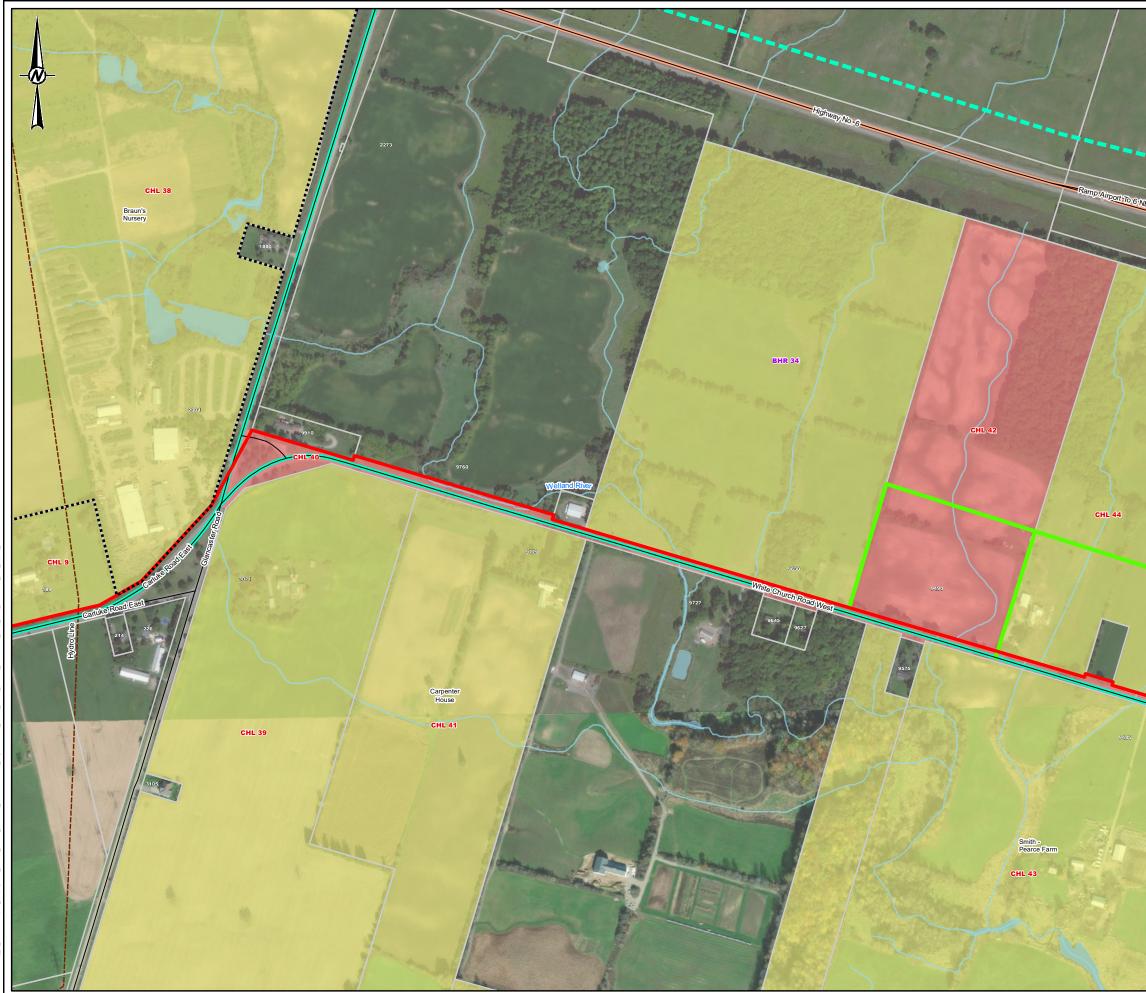
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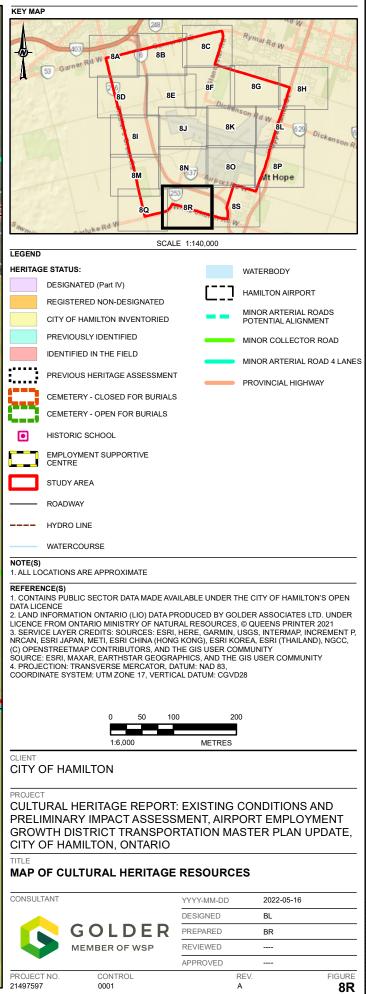
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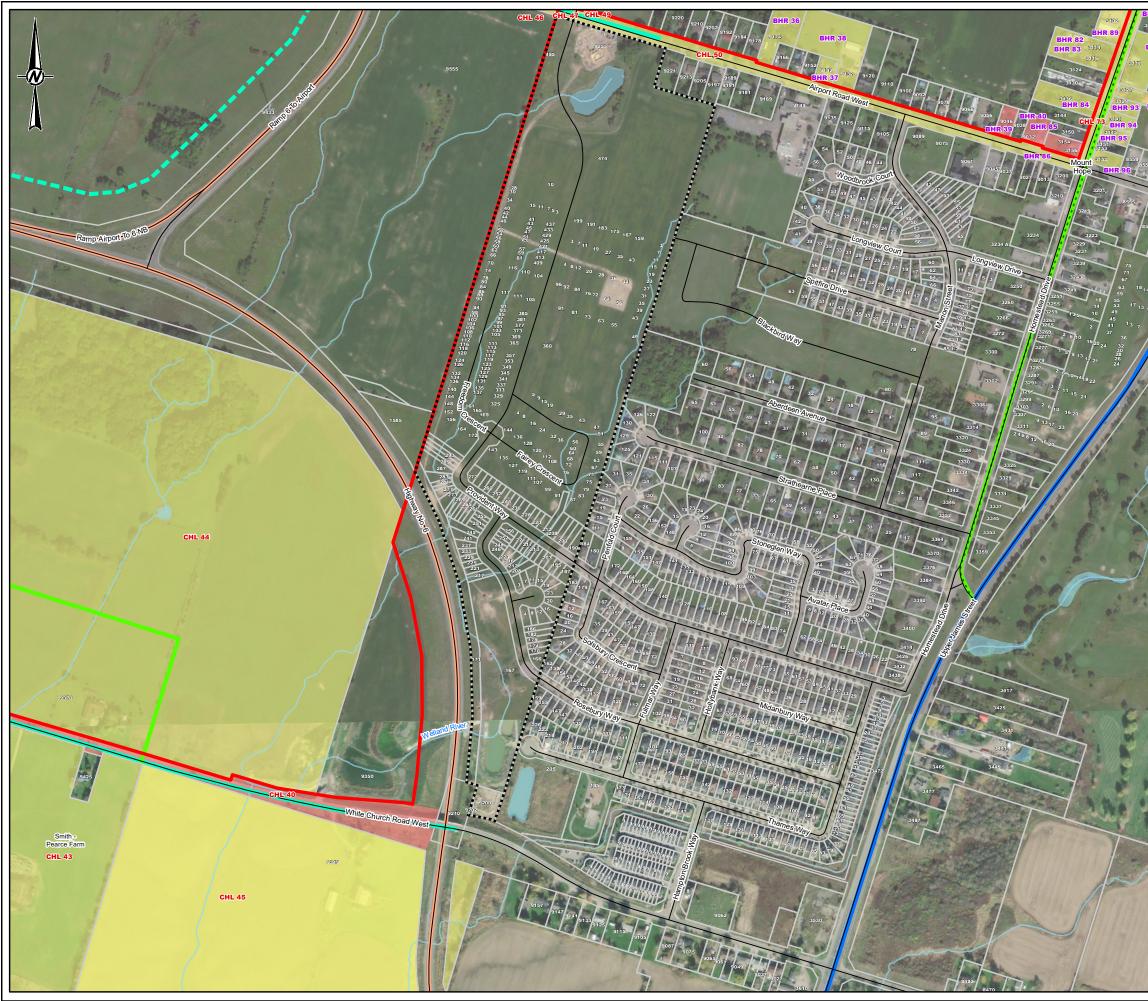
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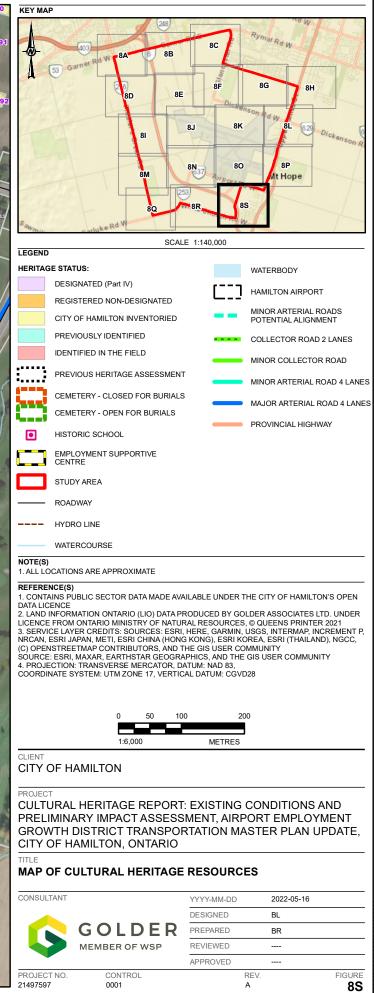




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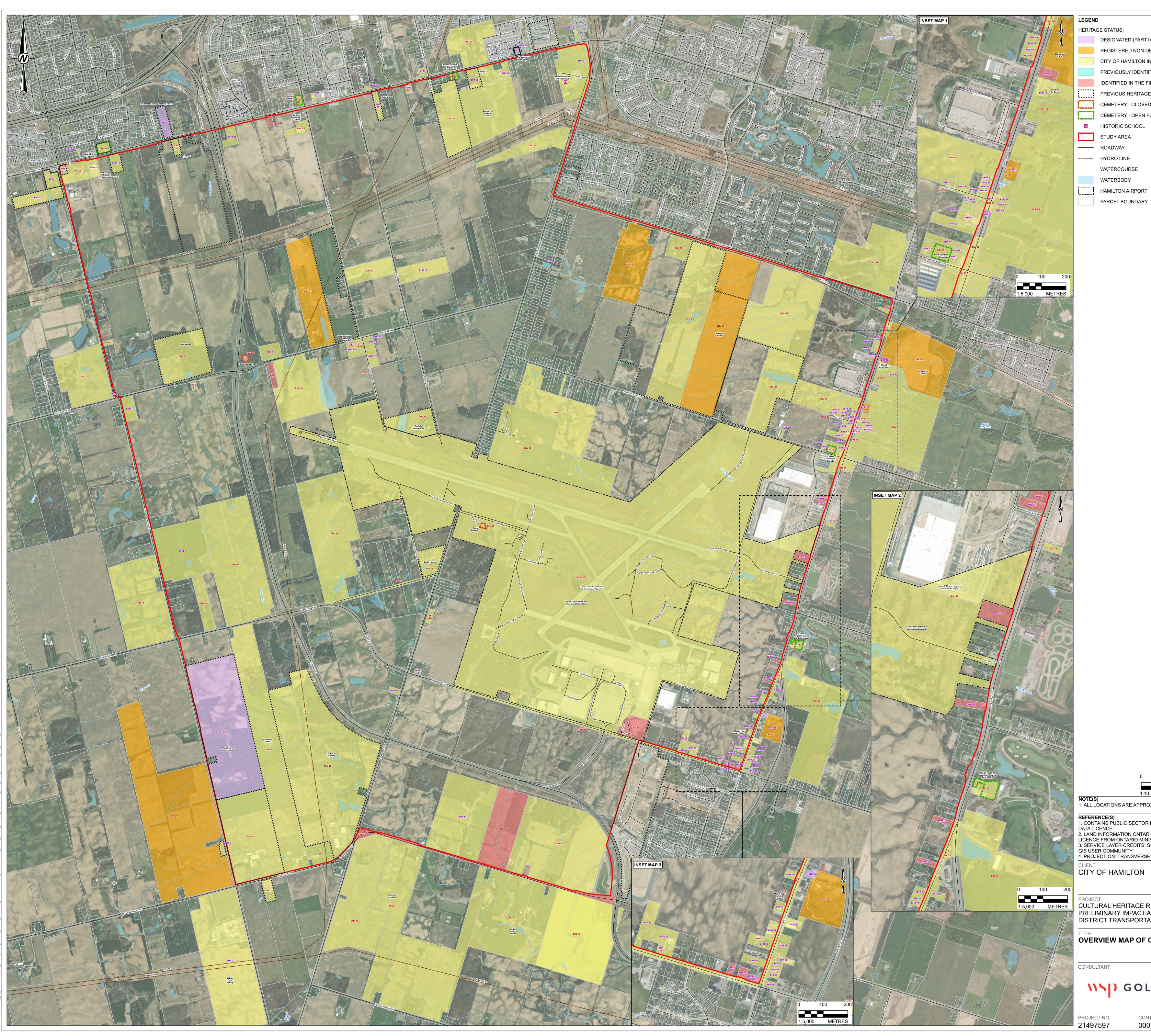
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